

FIRST AMENDED AND RESTATED
URBAN RENEWAL PLAN
RIVER STREET-MYRTLE STREET
URBAN RENEWAL PROJECT
(ANNEXATION OF THE OLD BOISE EASTSIDE STUDY AREA
AND SEVERAL MINOR PARCELS)
AND RENAMED
RIVER MYRTLE – OLD BOISE
URBAN RENEWAL PROJECT
URBAN RENEWAL AGENCY OF BOISE CITY
BOISE, IDAHO

Ordinance No. 5596
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¹ Minor revisions made in 2011 and 2015 resulted in changes to page numbers where sections appear in the text of the Urban Renewal Plan. Page numbers in the Table of Contents have been revised accordingly.

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² This item appears in the text but was inadvertently omitted from the Table of Contents in the amended and restated urban renewal plan (“the Plan”) presented to Boise City Council for adoption in 2004. This item was added to the table of contents in December 2011.

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ATTACHMENTS

- Attachment 1 Description of the Project Area and Revenue Allocation Area;
- Attachment 2 Project Area–Revenue Allocation Area Boundary Map
- Attachment 3

- 3A 2025 Downtown Redevelopment Plan;
- 3B Downtown Streetscaping Standards, updated in part and approved by the Agency pursuant to Resolution No. 1002 dated August 23, 2004;
- 3C Elements of Continuity, updated in part and approved by the Agency pursuant to Resolution No. 1002 dated August 23, 2004;
- 3D The River Street-Myrtle Street Urban Design Master Plan as adopted by the Agency pursuant to Resolution No. 677 dated October 27, 1994 and adopted by the Boise City Council pursuant to Ordinance 5596 on December 6, 2004, as revised by the Agency pursuant to Resolution No. 1002 dated August 23, 2004 and revised by the Boise City Council pursuant to Ordinance #6362 on November 30, 2004, and renamed the River Street-Myrtle Street Master Plan³;
- 3E The Old-Boise-Eastside Master Plan dated August 23, 2004, as adopted by the Agency pursuant to Resolution No. 1002 dated August 23, 2004; and adopted by the Boise City Council pursuant to Ordinance #6362 on November 30, 2004³;

- Attachment 4 Private Properties Which May Be Acquired by the Agency;
- Attachment 5 Map Depicting Current Zoning within Revenue Allocation Area and Project Area;
- Attachment 6 Economic Feasibility of the First Amended and Restated River Myrtle–Old Boise Urban Renewal District (Annexing the Old Boise Eastside Study Area and additional parcels to the River Street–Myrtle Street Urban Renewal District.

³ The original descriptions for 3D and 3E appeared in the draft River Myrtle-Old Boise Urban Renewal Plan sent by the CCDC Board of Commissioners to the Boise City Council for consideration, and only included the action taken by the CCDC Board to adopt these two master plans as attachments to the Urban Renewal Plan. These descriptions have been revised to include the action taken by the Boise City Council to adopt the River Street – Myrtle Street Master Plan and the Old Boise – Eastside Master Plan as attachments to the Urban Renewal Plan in November 2004. These revisions were made in March 2015.

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RIVER MYRTLE – OLD BOISE³
URBAN RENEWAL PROJECT

URBAN RENEWAL AGENCY OF BOISE CITY
BOISE, IDAHO

Section 100 **Introduction**

This is the First Amended and Restated Urban Renewal Plan (the "Plan") for the Amended and Restated River Myrtle–Old Boise Street Urban Renewal Project (the "Project") in the City of Boise, (the "City"), County of Ada, State of Idaho, and consists of the Text, the Description of the Project Area and Revenue Allocation Area Boundaries (Attachment 1), the Project Area–Revenue Allocation Area Boundary Map (Attachment 2), the 2025 Downtown Redevelopment Plan (Attachment 3A)⁴, Downtown Streetscaping Standards (Attachment 3B), Elements of Continuity (Attachment 3C), the River Street–Myrtle Street Master Plan (herein referred to as the River–Myrtle Master Plan from time to time for convenience) (Attachment 3D) and Old Boise–Eastside Master Plan (Attachment 3E) [expected land uses within the Urban Renewal Area are preferred land use maps in the Attachments 3D and 3E], the Private Properties which may be Acquired by Agency (Attachment 4), the Map Depicting Current Zoning Within Revenue Allocation Area and Project Area (Attachment 5), and Economic Feasibility of the First Amended and Restated River Myrtle–Old Boise Urban Renewal District (Annexing the Old Boise–Eastside Study Area and additional parcels to the River Street–Myrtle Street Urban Renewal District) (Attachment 6).

The original Urban Renewal Plan for the River Street–Myrtle Street Urban Renewal Project approved on December 6, 1994, is referred to as the “Original Plan” in this document.

The term "Project" is used herein to describe the overall activities defined in this Plan. Reference is specifically made to Idaho Code Section 50-2018(j) for the various activities contemplated by the term "Project." Such activities include both private and public development of property within the Urban Renewal Area. The term "Project" is not meant to refer to a specific activity or development scheme.

¹ This line was inadvertently omitted in the amended and restated urban renewal plan (“the Plan”) presented to Boise City Council for adoption in 2004. This omission was corrected and the line inserted in December, 2011.

² In the Plan presented to the Boise City Council for adoption, the name of the study area was punctuated Old Boise/Eastside. The punctuation has since been changed to use a hyphen rather than a slash, i.e. Old Boise–Eastside.

³ The name of the renamed urban renewal project was originally punctuated River-Myrtle/Old Boise in the Plan presented to the Boise City Council for adoption. The punctuation has since been simplified to River Myrtle–Old Boise.

⁴ The 2025 Downtown Redevelopment Plan is a planning document which remains to be finalized. It is anticipated the 2025 Downtown Redevelopment Plan will be approved separately by the Agency Board and forwarded to the City of Boise as for consideration as a planning document.

This Plan was prepared by the Urban Renewal Agency of Boise City (the "Agency"), reviewed and recommended by the Agency, pursuant to the State of Idaho Urban Renewal Law, Chapter 20, Title 50, Idaho Code (the "Law"), the Local Economic Development Act, Chapter 29, Title 50, Idaho Code (the "Act"), the Idaho Constitution, and all applicable local laws and ordinances.

The proposed redevelopment of the Project Area as described in this Amended and Restated Plan conforms to the Boise City Comprehensive Plan (hereinafter the Comprehensive Plan), adopted by the City Council on January 21, 1997 (Ordinance No. 5775), which includes the Downtown Boise Plan adopted May 25, 1993, pursuant to Ordinance No. 5464, which Downtown Boise Plan includes the Downtown Implementation Program, except where changes to the Comprehensive Plan and the Downtown Boise Plan are suggested or implied as part of the plan proposals in Attachments 3A, 3D, and 3E and so noted as such. Such changes would be subject to approval of the Boise City Council as part of updates to the Comprehensive Plan and the Downtown Boise Plan. The amendments to this Amended Plan and the planning documents as described below are intended to conform to existing master plans adopted by the City including the St. Luke's Regional Medical Center Master Plan and the State of Idaho Master Plan.

Attachment 3 contains several planning documents which generally describe the overall Project, objectives, guidelines, and implementation actions and identify certain specific public and private capital improvement projects. Because of the changing nature of the Project, these documents, by necessity, must be dynamic and flexible. The Agency anticipates that these documents will be modified as circumstances warrant. Any modification, however, shall not be deemed as an amendment of this Plan. No modification will be deemed effective if it is in conflict with this Plan. The planning documents are purposely flexible and do not constitute specific portions of the Plan. Provided, however, prior to the adoption of any proposed modification to the planning documents, the Agency shall notify the City and publish a public notice of such proposed modification at least thirty (30) days prior to the consideration of such proposed modification, thus providing the City and any other interested person or entity an opportunity to comment on said proposed modification. The Agency Board shall consider any such comments and determine whether to adopt the modification. The planning documents apply to redevelopment activity within the Project Area as described herein. In the event of any conflict between this Plan and the appended documents, the provisions of this Plan shall control.

The planning documents are identified as follows:

Attachment 1 Description of the Project Area and Revenue Allocation Area;

Attachment 2 Project Area–Revenue Allocation Area Boundary Map

Attachment 3

3A 2025 Downtown Redevelopment Plan;

3B Downtown Streetscaping Standards, updated in part and approved by the Agency pursuant to Resolution No. 1002 dated August 23, 2004;

- 3C Elements of Continuity, updated in part and approved by the Agency pursuant to Resolution No. 1002 dated August 23, 2004;
- 3D The River Street-Myrtle Street Urban Design Master Plan as adopted by the Agency pursuant to Resolution No. 677 dated October 27, 1994 and adopted by the Boise City Council pursuant to Ordinance 5596 on December 6, 2004, as revised by the Agency pursuant to Resolution No. 1002 dated August 23, 2004 and revised by the Boise City Council pursuant to Ordinance #6362 on November 30, 2004, and renamed the River Street-Myrtle Street Master Plan⁵;
- 3E The Old-Boise-Eastside Master Plan dated August 23, 2004, as adopted by the Agency pursuant to Resolution No. 1002 dated August 23, 2004; and adopted by the Boise City Council pursuant to Ordinance #6362 on November 30, 2004⁵;

Attachment 4 Private Properties Which May Be Acquired by the Agency;

Attachment 5 Map Depicting Current Zoning within Revenue Allocation Area and Project Area;

Attachment 6 Economic Feasibility of the First Amended and Restated River Myrtle–Old Boise Urban Renewal District (Annexing the Old Boise Eastside Study Area and additional parcels to the River Street–Myrtle Street Urban Renewal District.

There are three urban renewal districts in downtown Boise: Central, River Myrtle–Old Boise and Westside Downtown. Each of these urban renewal districts has a master plan or plans that address key elements such as expected land uses, built form, civic spaces, and street character for a specific part of downtown. Because of its size and history, the River Myrtle–Old Boise District has two master plans: one for the River Street–Myrtle Street area and one for the Old Boise–Eastside area. The 2025 Downtown Redevelopment Plan is a companion document to the master plans. It consolidates information from the master plans onto a series of downtown-wide maps. These maps give a more holistic view of what the eventual character, form, and structure of downtown will be. The 2025 Downtown Redevelopment Plan also contains plans for a downtown-wide network of pedestrian, bicycle, and transit connections and public parking facilities, and general design guidelines and standards for buildings, site layouts, public spaces, and streetscaping. The Downtown Streetscaping Standards and Elements of Continuity are attachments to this plan that specifically address the design of sidewalks and street furnishings. The design guidelines and standards contained in the 2025 Downtown Redevelopment Plan and its attachments apply to all of the urban renewal districts in downtown Boise and supplement the more specific guidelines and standards in the master

⁵ The original descriptions for 3D and 3E appeared in the draft River Myrtle–Old Boise Urban Renewal Plan sent by the CCDC Board of Commissioners to the Boise City Council for consideration, and only included the action taken by the CCDC Board to adopt these two master plans as attachments to the Urban Renewal Plan. These descriptions have been revised to include the action taken by the Boise City Council to adopt the River Street – Myrtle Street Master Plan and the Old Boise – Eastside Master Plan as attachments to the Urban Renewal Plan in November 2004. These revisions were made in March 2015.

plans. The 2025 Downtown Redevelopment Plan, River Street–Myrtle Street Master Plan, and Old Boise–Eastside Master Plan, taken together form the long-range plan for redevelopment of the River Myrtle–Old Boise Urban Renewal District.

Implementation of the master plans will require public co-investment to help stimulate desired private development. Typically, the public will fund enhanced public facilities like streets, sidewalks, parking garages, parks or plazas which, in turn, create an attractive setting for adjacent private investment in office, retail, housing or hotels, entertainment and convention related facilities, and civic buildings.

This Plan provides the Agency with powers, duties and obligations to implement and further the program generally formulated in this Plan for the redevelopment, rehabilitation, and revitalization of the area within the boundaries of the Project (the "Project Area"). The Agency retains all powers allowed by law. Because of the long-term nature of this Plan, and the need to retain in the Agency flexibility to respond to market and economic conditions, property owner and developer interests and opportunities from time to time presented for redevelopment, this Plan does not present a precise plan or establish specific projects for the redevelopment, rehabilitation, and revitalization of any area within the Project Area, nor does this Plan present specific proposals in an attempt to solve or alleviate the concerns and problems of the community relating to the Project Area. Instead, this Plan presents a process and a basic framework within which specific plans will be presented, specific projects will be established, and specific solutions will be proposed, and by which tools are provided to the Agency to fashion, develop, and proceed with such specific plans, projects, and solutions.

The particular projects or redevelopment projects by private entities described herein are not intended to be an exclusive or exhaustive list of potential redevelopment activity. Allowed projects are those activities which comply with the Law and the Act and meet the overall objectives of this Plan.

The purposes of the Law that will be attained through this Plan and the Plan's major goals are:⁶

- a. The elimination of environmental deficiencies in the Project Area, including, among others, obsolete and aged building types, substandard streets or rights-of-way, inadequate and deteriorated public facilities and improvements.
- b. The assembly of land into parcels suitable for efficient, integrated development with improved urban development standards, including setbacks, parking, pedestrian, and vehicular circulation in the Project Area.
- c. The potential use of the City's geothermal resource.
- d. Funding of arts programs.
- e. The replanning, redesign, and development of undeveloped areas which are

⁶ The list which follows was originally published without being lettered. Lettering was added in December 2011 to make the list consistent with the format of the Plan elsewhere.

stagnant or improperly utilized.

- f. The strengthening of the economic base of the Project Area and the community by the installation of needed site improvements to stimulate new commercial expansion, employment, and economic growth.
- g. The provision of civic buildings owned or occupied by other public entities including the City of Boise, Ada County, the Greater Boise Auditorium District, and the State of Idaho.
- h. The provision of adequate land for parks and open spaces, pedestrian walkways, and parking facilities.
- i. Improvements to the streets, rights-of-way, and other public infrastructure.
- j. The establishment and implementation of performance criteria to assure high site design standards and environmental quality and other design elements which provide unity and integrity to the entire Project.
- k. The opportunity of providing housing within the Project Area.
- l. The strengthening of the tax base by encouraging private development, thus increasing the assessed valuation of properties within the Revenue Allocation Area and the Project Area as a whole, and benefiting the various taxing districts in which the Urban Renewal Area is located.

Section 100.01 General Procedures of the Agency

The Agency is a public body, corporate and politic, as defined and described under the Law and the Act. The Agency is also governed by its bylaws as authorized by the Law and adopted by the Agency. Under the Law, the Agency is governed by the Idaho open meeting law, the Public Records Act, the Ethics in Government Act, financial reporting requirements, and the competitive bidding requirements under Idaho Code Section 50-341.

Generally, the Agency shall conduct all meetings in open session and allow meaningful public input as mandated by the issue considered or by any statutory or regulatory provision. Whenever in this Plan it is stated that the Agency may modify, change, or adopt certain policy statements or contents of this Plan not requiring a formal amendment to the Plan as required by the Law or the Act, it shall be deemed to mean a consideration by the Board of such policy or procedure, duly noticed upon the Agency meeting agenda and considered by the Agency at an open public meeting and adopted by a majority of the members present, constituting a quorum, unless any provision herein provides otherwise.

Section 101 Provisions Necessary to Meet State and Local Requirements Conformance with State of Idaho Urban Renewal Law of 1965, as Amended

- a. The laws of the State of Idaho allow for an Urban Renewal Plan to be submitted by any interested person or entity in an area designated as appropriate for an Urban Renewal Project by the Boise City Council. The River Street Area was designated by the Council by Resolution on October 12, 1993, Resolution No. 12475. The Front Street–Myrtle Street Area was designated by the Council by resolution on April 26, 1994, Resolution No. 12737. Parcels located between Capitol Boulevard, Myrtle Street, 2nd Street and the northern boundary of Julia Davis Park were designated by the Council by ordinance on December 5, 1994, Ordinance 5596.
- b. In accordance with the Idaho Urban Renewal Law of 1965 the Original Plan was submitted to the Planning Commission of the City of Boise. After consideration of the Plan, the Commission filed a Resolution with the City Council stating that the Original Plan is in conformity with the Comprehensive Plan, City of Boise.
- c. Pursuant to the Idaho Urban Renewal Law of 1965, the City Council, having published due notice thereof, a public hearing was held on this Plan. Notice of the hearing was duly published in a newspaper having general circulation. The City Council adopted this Plan on December 6, 1994, by Ordinance No. 5596.
- d. The Eastside Central Business District (Old Boise–Eastside Study Area) was designated as appropriate for an Urban Renewal Project by the Boise City Council by Resolution on September 10, 2002, Resolution No. 17216. Proposed additions to the River Street–Myrtle Street Area were designated as appropriate for an Urban Renewal Project by the Council by Resolution on July 20, 2004, Resolution R-264-04.
- e. In accordance with the Idaho Urban Renewal Law of 1965 this Plan was submitted to the Planning Commission of the City of Boise. After consideration of the Plan, the Commission filed a Resolution dated September 13, 2004 stating that this Plan is in conformity with the Boise City Comprehensive Plan (1997).
- f. Pursuant to the Idaho Urban Renewal Law of 1965, the City Council, having published due notice thereof, a public hearing was held on this Plan. Notice of the hearing was duly published in a newspaper having general circulation. The City Council adopted this Plan on November 30, 2004, by Ordinance No. 6362.

Section 102 History of the River Street–Myrtle Street Area

In April 1972 the City Council approved an urban renewal plan for the River Street Neighborhood Development Program that included an area bounded by Capitol Boulevard, Boise River, Americana, and the Union Pacific railroad property/Broad Street. This plan was never implemented and was shelved in 1973 by the Agency and the City due to lack of federal funding. A development guide for the area was prepared in 1973 and partially implemented when Community Development Block Grant funds were used for Eighth Street streetscape, the Pioneer Walkway, the greenbelt, the pedestrian bridge to Ann Morrison Park, and site acquisition for the Pioneer Square Apartments at 12th and Grand.

Preparation of the River Street Urban Renewal Plan indicated strong interest by earlier city leaders to upgrade and improve this area, but it did not result in the immediate formation of an urban renewal district or implementation of a program of improvements.

The Project Area was also included within a downtown improvement area designated by the City Council pursuant to Resolution No. 7995 adopted on July 16, 1984. The City Council found the Downtown Improvement Area as a deteriorated area by virtue of a number of blighting influences, including a substantial number of deteriorating buildings. A copy of Resolution No. 7995 is available through the City of Boise.

Redevelopment of this area began more than 20 years after the River Street Neighborhood Development Program was prepared with the approval of the River Street– Myrtle Street Urban Renewal Project and the establishment of an urban renewal district and revenue allocation area in 1994. In the intervening years, the Agency acquired land which was subsequently developed as the River Plaza apartments and office building through a Development and Disposition Agreement with a private developer, acquired fo(u)r⁷ additional properties intended for either public parking facilities, housing, or mixed-use projects, facilitated the financing and construction of the Ada County Courthouse, invested in streetscapes and public parking facilities associated with the Courthouse project, Civic Plaza Apartments, and Idaho Water Center, provided funding for streetscapes adjacent to the Boise Public Library, housing and office developments, and invested in public art and cultural facilities in the Boise Cultural District. In 2000, the Agency conducted a design competition to solicit ideas on how to transform the Pioneer Walk into a boulevard-style pedestrian and bicycle pathway and linear park between the Boise River and the downtown core and use it as a neighborhood amenity and catalyst for development along its edge. More detailed designs for the Walk are being prepared, in preparation for phased land acquisition and construction. The Agency anticipates a major investment in a lifestyle-retail, office, and entertainment development at 8th and Broad Street that will reestablish Broad Street between Capitol and 9th Street, provide a public parking garage, streetscapes and plaza space, and begin active revitalization of the area on the south side of the Front Street.

Certain parts of the River Street–Myrtle Street area show the benefits of public investment, chiefly the Courthouse Corridor and specific development sites. The challenge that remains is to extend these benefits into areas where properties have continued to deteriorate or have been cleared and redevelopment activity has yet to occur. These include parties of the Parkside neighborhood and Cultural District, much of the River Street neighborhood and along the West Connector.

In 1004, the Agency initiated preparation of an update to the River Street–Myrtle Street Urban Design Plan to address the following:

- a. Recognition that the River–Myrtle area is now part of downtown and not on its outskirts;
- b. Need to improve the Connector’s design, reduce its impact on adjacent properties, and improve pedestrian and bicycle links across it;

⁷ This word was typed as “for” instead of “four” in the Plan presented to the Boise City Council for adoption in 2004. This typographical error was corrected in December, 2011.

- c. Need to create urban centers where people can live, work and play to reduce transportation demands and combat traffic congestion and air pollution;
- d. Desire to use the River–Myrtle area as a model of sustainable development patterns;
- e. Increased interest in downtown housing;
- f. Changes in the River Street–Myrtle Street District that were unanticipated in 1994;
- g. Making the plan more complete and easier to use; and
- h. Adding the Old Boise–Eastside study area to the River Street–Myrtle Street District.

Section 103 History of the Old Boise–Eastside Area

The Old Boise–Eastside Area is located at the eastern edge of the downtown core and is surrounded geographically by four major employers and generators of activity: Idaho State government, St. Luke's Regional Medical Center, the Ada County Courthouse, and Boise City Hall. The west half is a thriving restaurant and nightclub district where most of the businesses occupy historic brick and stone buildings preserved from the early days of Boise's founding. The remainder of the area has the look and feel of a very desirable residential neighborhood with

Victorian and craftsman style houses, wide tree lawns, and beautiful mature trees.

Development of a master plan for Old Boise–Eastside was initiated in summer 2000 after property owners and other stakeholders petitioned the Agency to consider creating an urban renewal district. They were concerned that the Old Boise area would fall behind the rest of downtown in terms of public investment and improvements. They wanted to see aesthetic improvements such as street trees, historic lighting and brick sidewalks in the business district so it would become a seamless extension of the downtown core. They also cited a need for public parking facilities to support the retail and restaurant activity and allow it to expand.

The employers in the area—particularly State of Idaho, St. Luke's Regional Medical Center, and Ada County—are interested in workforce housing and the idea of a downtown shuttle or streetcar that would connect their offices and facilities with the downtown core where there is a concentration of restaurants and shopping.

Issues identified by the Agency include maintaining the long-term vitality of Old Boise–Eastside and the downtown core by increasing the supply of downtown housing in close-in locations. Old Boise is in an ideal location and has the necessary amenities to be a successful in-town neighborhood. The trend, however, is for the existing houses to be converted to or replaced by offices. One of the key objectives of the Plan is to stem this tide and use redevelopment tools to prompt housing development. As offices proliferate so do surface parking lots, which tend to sap urban vitality. Developing alternatives to these lots, either in the form of parking structures in

downtown or providing remote parking and a means of getting to and from, often requires public investment.

The Agency sees including Old Boise–Eastside in an urban renewal district as an important step in integrating it into Boise's City's and the Agency 's economic development efforts in downtown and making the same redevelopment tools available to this area as elsewhere. Including it as part of the River Street–Myrtle Street District creates a larger resource base for both areas and reduces the risk that insufficient resources will be available in any one area. Public facilities in the east end of the Project will most likely have service areas that include both the River Street–Myrtle Street District and the Old Boise–Eastside area and will prompt development in both areas, so it is advisable to capture as much of the tax increment as possible to underwrite the cost.

Section 104 Basis of the Plan

The foundation for 2025 Downtown Redevelopment Plan , the River Street–Myrtle Street Master Plan and the Old Boise–Eastside Master Plan is the City 's 1993 Downtown Boise Plan, which presents detailed goals, objectives, and policies for downtown Boise and the 1997 Boise City Comprehensive Plan, of which the Downtown Boise Plan is now a part. The Downtown Boise Plan clearly calls for:

- Downtown to remain as the regional center for corporate business, finance, arts, entertainment, higher education, and government services;
- Downtown to be a compact, urban district of high quality design and amenities;
- Promotion of urban housing;
- Promotion of alternative transportation–walking, bicycling, and transit; and
- Recruitment of new businesses and retention of existing strong businesses.

The 2025 Downtown Redevelopment Plan and the master plans building on the Downtown Boise Plan and embrace the following principles:

- Assure that downtown Boise remains the foremost urban center in the region for business, government, culture, education, and urban living.
- Promote a prosperous economy in downtown and the region by creating a place that is attractive to fundamental industries and cutting-edge companies and the workforce that they employ. Retain existing businesses and encourage their continued growth and prosperity.
- Develop downtown as a place for people offering a delightful and enriching environment for living, working, and playing. Emphasize the needs of people over vehicles.

- Create a compact, urban form with a vibrant mix of uses. Emphasize quality of design and a feeling of permanence in the built elements of downtown.
- Maintain the urban vitality of the downtown core while extending it into the larger downtown area.
- Recognize the contribution made by major institutions to the employment base and downtown economy, and work with them so they stay in downtown and are able to grow without creating impacts on adjacent areas.
- Increase density levels in the downtown in order to increase vitality, make better use of scarce and valuable downtown land, make housing more affordable, and make it possible to accommodate the market forecast for River Myrtle–Old Boise.
- Create more sustainable development patterns through the mix of land uses, better building, and site design and promoting alternative transportation modes.
- Increase the number of downtown residents, thus increasing support for retail, restaurants, arts, and entertainment and making downtown more living (lively) and safer.
- Assure that downtown continues to include thriving residential neighborhoods. Take action to prevent residential uses from disappearing and to expand the housing supply in places where a residential neighborhood is still desirable.
- Create a system of parks, plazas, and public spaces that become focal points in the various subdistricts of downtown, act as catalysts for private development and add livability to urban surroundings.
- Improve streets and sidewalks so they offer a welcoming pedestrian environment.
- Provide an adequate supply of parking while minimizing the use of land for surface parking lots so land can be put to more productive use.

Section 200 **Description of Project Area**

The boundaries of the Project Area and of the Revenue Allocation Area are described in the Attachment 1, which is attached hereto and incorporated herein by reference, and are shown on the "Project Area and Revenue Allocation Area Boundary Map," attached hereto as Attachment 2 and incorporated herein by reference. The map shows the original boundary of the River Street–Myrtle Street Urban Renewal Project and the areas added as part of the 2004 amendment process. The area contained within the original boundary was designated as appropriate for an urban renewal project

by City Council Resolution Nos. 12475 and 12737 and Ordinance 5596. The 2004 additions to the original boundary included annexation of a portion of the Old Boise–Eastside Study Area to the River Street–Myrtle Street District, designated as appropriate for an urban renewal project by City Council Resolution No. 17216, and additions to the River Street–Myrtle Street Area, designated as appropriate for an urban renewal project by Council Resolution No. 18232. The Agency has received reports prepared by Harlan W. Mann, consultant, concerning findings of deteriorated or deteriorating conditions for the original area (dated September 23, 1991, April 8, 1994, and September 19, 1994), and reports for the areas added in 2004 (dated July 13, 2001, and June 30, 2004).

Section 300 Proposed Redevelopment Actions

Section 301 General

The Agency proposes to eliminate and prevent the spread of blight and deterioration in the Project Area by undertaking some or all of the following actions:

- a. The acquisition of certain real property;
- b. The demolition or removal of certain buildings and improvements;
- c. Participation by persons or entities engaged in business or holding interests in property within the Project Area, through remaining in or reentering the Project Area;
- d. The management of any property acquired by and under the ownership and control of the Agency;
- e. The provision for relocation assistance to displaced Project occupants, as required by law;
- f. The installation, construction, or reconstruction of streets, utilities, parking facilities, recreation and park amenities, and other public improvements, including civic buildings owned or occupied by other public agencies including the City of Boise, Ada County, the State of Idaho, and the Greater Boise Auditorium District, and the operation and management of such facilities in conformance with the Law and Act;
- g. The disposition of property for uses in accordance with the Law and this Plan and the commitment by those who conveyed such property to redevelop the property in accordance with the Law and this Plan;
- h. The redevelopment of land by private enterprise or public agencies for uses in accordance with this Plan;
- i. The assembly of adequate sites for the development and construction of residential and commercial facilities;

- j. To the extent allowed by law, the lending or investing of federal funds to facilitate redevelopment; and
- k. The construction of foundations, platforms, and other like structural forms necessary for the provision or utilization of air rights and sites for buildings to be used for residential, commercial, and other uses contemplated by the Plan and to provide utilities to the development site.

In the accomplishment of these purposes and activities and in the implementation and furtherance of this Plan, the Agency is authorized to use all the powers provided in this Plan and all the powers now or hereafter permitted by law.

Section 302 Urban Renewal Plan Objectives

Urban renewal action is necessary in the Project Area to combat problems of physical blight and economic obsolescence.

The Project Area consists of approximately 321 acres adjacent to the Boise Central Business District. At the time the River Street–Myrtle Street Urban Renewal District was established, the area had a history of a slow-growing tax base primarily attributed to: inadequate street and parking improvements; inadequately located drainage facilities; poorly maintained properties; inadequate pedestrian and open areas; undeveloped properties; inconsistent and diverse property ownership; and other deteriorating factors. In the intervening years, property values have increased with some areas rising in value faster than others. Despite increasing property values, however, a significant number of properties have not been improved or redeveloped, and in fact, building and property conditions have deteriorated. As of December 2000, 40 percent of the land in the River Street– Myrtle Street District was vacant or in surface parking lots rather than being put to productive use. The Old Boise– Eastside Study Area has experienced rising property values but also has a number of properties in surface parking lots and properties where the building value is less than the land value, indicating that the properties are underutilized. Improved lots have been cleared to create parking lots because of a lack of parking facilities in the area.

The discrepancy between property values and the actual use and condition of properties in the River Myrtle–Old Boise area contrasts sharply with the strong level of economic and cultural activity in the Boise's central business district and elsewhere in the Ada County region for which downtown Boise serves as the commercial and cultural center.

Hence, the Urban Renewal Plan for the Project Area is a proposal for public improvements, to provide an improved environment for new retail, residential, and commercial developments, to eliminate unsafe conditions, to assist potential owners and developers to assemble appropriate development sites where necessary through acquisition, demolition and disposition activities, and to otherwise prevent the extension of blight and deterioration and reverse the deteriorating action of the area.

The streets to be vacated or relocated will create buildable areas for residential, retail,

commercial, office, or public use. Any such vacations or relocations must be requested from the Ada County Highway District or other agency having jurisdiction over the particular public right-of-way.

Air rights and subterranean rights may be disposed of for any permitted use within the Project Area boundaries.

Less than fee acquisition may be utilized by the Agency when and if necessary to promote redevelopment in accordance with the objectives of the Plan.

Temporary project improvements shall be provided to facilitate adequate vehicular and pedestrian circulation during construction.

All existing alleys within the Project Area may be vacated to permit development as well as encourage variety and flexibility of design within the periphery blocks, subject to City and ACHD policies.

Current parking resources are predominantly short-term customer and visitor parking and long-term employee and resident parking. Over time, individual businesses have provided their own off-street parking. Operation of parking as a private, exclusive resource has resulted in inefficient use of the resource. At any lot on any given day, reserved parking spaces are going unused while a potential parking customer looks for space elsewhere. Shared parking is a necessity. The Plan envisions the creation of a system of public parking facilities which would serve the various areas within River Myrtle–Old Boise. By relieving private property owners from having to supply parking, these facilities serve as a catalyst for private development. They also typically consolidate parking into structures and reduce the use of surface parking lots, thus freeing land for more productive use and increasing urban vitality.

A further objective of the Urban Renewal Plan is to provide for the acquisition and clearance of land to be used for other public facilities, including civic buildings owned or occupied by other public agencies including the City of Boise, Ada County, the Greater Boise Auditorium District, and the State of Idaho. Off-street parking and loading facilities will be developed to serve the new development within the Project Area. Land use in the Project Area will be modified to the extent that land and buildings currently vacant and land now devoted to scattered surface parking will be converted to residential, commercial, public and private parking, and public/semi-public uses.

In coordination with the State Historical Society and the Boise City Historical Preservation Commission, consideration will be given to the preservation of structures of historic and architectural value within, or the moving of said structures within or outside, the Project Area boundaries.

The provisions of this Plan are applicable to all public and private property in the Project Area. The provisions of the 2025 Downtown Redevelopment Plan, River Street–Myrtle Street Master Plan, and the Old Boise–Eastside Master Plan shall be interpreted and applied as objectives and goals, recognizing the need for flexibility in interpretation and implementation, while at the same time not in any way abdicating the rights and privileges of the property owners which are vested in the present and future zoning classifications of the properties. All development under an owner participation agreement shall conform to those standards specified in Section 405.02 of this Plan.

It is recognized that the Ada County Highway District has exclusive jurisdiction over all

public rights-of-way within the Project Area, except for state highways. Nothing in this Plan shall be construed to alter the powers of the Ada County Highway District pursuant to Title 40, Idaho Code.

This plan must be practical in order to succeed. Particular attention has been paid to how it can be implemented, given the changing nature of market conditions. Transforming the River-Myrtle–Old Boise Urban Renewal District into a vital, thriving part of the community requires an assertive strategy. The following list represents the key elements of that effort⁸.

- a. Initiate simultaneous projects designed to revitalize. From street planters to sidewalk improvements to significant new development, the Agency plays a key role in creating the necessary momentum to get and keep things going.
- b. Secure significant public open space. This open space will greatly increase housing and office values adjacent to it and greatly contribute to a new sense of place (“placemaking”).
- c. Transform the Pioneer Walk into a linear park and grand, boulevard-style pedestrian and bicycle link between the Boise River greenbelt and The Grove plaza, and use this link as an anchor and catalyst for development. Create public plazas in the River Street neighborhood and extend the Basque Block. Make improvements to Capitol Park, the Assay Office grounds, and C.W. Moore Park.
- d. Focus on substantially increasing the supply (of) medium to high-density housing in River Myrtle–Old Boise, with a goal of adding 2820 units of various housing types by 2025. Housing is the key to a healthy downtown, and new residents will serve as a catalyst to other redevelopment efforts.
- e. Recruit new quality hotels, with a goal of adding 730 rooms by 2025.
- f. Recruit a mixed-use, retail and entertainment center which will re-energize the downtown retail market and give downtown a regional draw. Make connections between this center and the downtown core so businesses in the core benefit from the new development rather than the center draining energy from the core. Attract 300,000 square feet of new retail development in River Myrtle–Old Boise.
- g. Prepare for the market forecast for office development in River Myrtle–Old Boise, which estimates that as much as 1.7 million square feet of office could be developed in this area by 2025. Guide the location and design of office uses so they contribute to the downtown vitality and do not cause an imbalance in the mix of office, retail, housing, hotels, and other uses needed to maintain a healthy and prosperous downtown.
- h. Make improvements to the downtown transportation system so it is capable of handling the development forecast. Develop alternative modes to reduce dependency

⁸ This list originally was numbered in the Plan presented to the City Council for adoption. The list should have been lettered to be consistent with the format of the Plan document. This correction was made in December 2011.

on automobiles, traffic congestion, and air pollution.

- i. Clear regulatory barriers. Zoning codes must support the plan, and developers must be rewarded by an expedited process.
- j. Pursue development across all land-use sectors with particular attention to increasing housing development.

Without direct public intervention, properties in the combined River Myrtle–Old Boise District could continue to be vacant or underutilized or could be developed at less than urban intensities during the next 20 to 25 years. The full potential of this area to contribute to the economic prosperity of downtown could be left untapped. The opportunity to create a model of sustainable development patterns—where sufficient densities are achieved so people can satisfy their daily needs by walking, bicycling, and transit and where transit is feasible—would be lost. Success will come through numerous public-private partnerships—no single developer will save the day. The plan and the market strategy as set out in this Plan and Attachments create the necessary flexible framework for the River Myrtle–Old Boise District to capture a share of Boise's growing population and economy and become a thriving urban place.

Section 303 Participation Opportunities and Agreement

Section 303.01 Participation Agreements

The Agency shall enter into an owner participation agreement with any existing or future owner of property, in the event the property owner seeks and/or receives assistance from the Agency in the redevelopment of the property and the Agency determines such participation is in the best interests of the Agency and the public. In that event, the Agency may allow for an existing or future owner of property to remove his property and/or structure from future Agency acquisition subject to entering into an owner participation agreement.

Each structure and building in the Project Area to be rehabilitated or to be constructed as a condition of the owner participation agreement between the Agency and the owner pursuant to this Plan will be considered to be satisfactorily rehabilitated and constructed, and the Agency will so certify, if the rehabilitated or new structure meets the following standards:

- a. Executed owner participation agreement to meet conditions described below.
- b. Any such property within the Project shall be required to conform to all applicable provisions, requirements, and regulations of this Plan, including the 2025 Downtown Redevelopment Plan and either the River Street–Myrtle Street Master Plan or the Old Boise–Eastside Master Plan, whichever is applicable. The owner participation agreement may require as a condition of financial participation by the Agency a commitment by the property owner to meet the greater objectives of the land use elements identified in the Downtown Boise Plan, Boise City Comprehensive Plan, and the Boise City zoning ordinance. Upon completion of any rehabilitation, each structure must be safe and sound in all physical respects and be refurbished and altered to bring the property to an upgraded marketable

condition which will continue throughout an estimated useful life for a minimum of twenty (20) years.

- c. All such buildings or portions of buildings which are to remain within the Project Area shall be rehabilitated in conformity with all applicable codes and ordinances of the City of Boise, including any mandated federal authority.
- d. Any new construction shall also conform to all applicable provisions, requirements, and regulations of this Plan.
- e. Any new construction shall also conform to all applicable codes and ordinances of the City of Boise.
- f. Any renovation of a historic structure may seek any applicable and appropriate waiver of the City Building Code.

In such participation agreements, participants who retain real property shall be required to join in the recordation of such documents as may be necessary to make the provisions of this Plan applicable to their properties.

In the event a participant under a participation agreement fails or refuses to rehabilitate, develop, use, and maintain its real property pursuant to this Plan and a participation agreement, the real property or any interest therein may be acquired by the Agency in accordance with Section 307 of this Plan and sold or leased for rehabilitation or development in accordance with this Plan.

Owner participation agreements may be used to implement the following objectives:

- a. Encouraging established businesses to revitalize deteriorating areas of their parcels and to incorporate elements of the 2025 Downtown Redevelopment Plan, River–Myrtle Master Plan, or Old Boise–Eastside Master Plan, whichever is applicable, such as street trees and sidewalk treatments to accelerate the enhancement of the street environment in the Plan area;
- b. Subject to the limitations of the Law and the Act, providing incentives to existing business owners to encourage continued utilization and expansion of existing permitted uses and to prevent properties from falling into disuse, a proliferation of vacant and deteriorated parcels and a reduction in downtown employment;
- c. Allowing existing nonconforming uses to continue in accordance with City regulations and to accommodate improvements and expansions allowed by City regulations;
- d. Subject to the limitations of the Act, providing incentives to improve nonconforming properties so they implement the design guidelines contained in this Plan to the extent possible and to encourage an orderly transition from nonconforming to conforming uses over the remaining planning horizon; and

- e. In all such agreements addressing phasing issues, justification, eligibility of project costs, and/or achievement of the objections of the Urban Renewal Plan, retaining for Agency its discretion in the funding level of its participation.

Section 303.02 Zoning Incentive Impact Fees and Waivers

Pursuant to the City's zoning ordinance and Downtown Boise Plan, certain zoning incentive, density waivers, and waivers of parking requirements may be available to those developers who develop properties in such a manner that those incentives and waivers are available. For example, including a residential housing component within a commercial/office development in certain districts would entitle such developer to additional density since housing is not counted against density limits.

For any development covered by an owner participation agreement or disposition and development agreement, the Agency shall have the authority, but not the obligation, to cooperate with the developer to apply for the waiver or reduction of any impact fee assessed by any other governmental entity. The Agency shall also have the authority, but not the obligation, to consider the payment of all or part of such impact fees from revenue allocation proceeds to the extent allowed by law.

The Agency shall cooperate with the City of Boise in the establishment of a transfer of development rights program as allowed by state law governing the City's rights to approve the transfer of such rights under Idaho Code Section 67-6515A, if the City chooses to pursue such a program.

Section 304 Cooperation with Public Bodies

Certain public bodies are authorized by state law to aid and cooperate, with or without consideration, in the planning, undertaking, construction, or operation of this Project. The Agency shall seek the aid and cooperation of such public bodies and shall attempt to coordinate this Plan with the activities of such public bodies in order to accomplish the purposes of redevelopment and the highest public good.

The Agency may impose on all public bodies the planning and controls contained in this Plan to ensure that present uses and any future development by public bodies will conform to the requirements of this Plan; provided, however, ACHD has exclusive jurisdiction over Ada County Highway District streets. The Agency is authorized to financially (and otherwise) assist any public entity in the cost of public land, buildings, facilities, structures, or other improvements within the Project Area.

The Agency specifically intends to cooperate to the extent allowable by law with the City of Boise and Ada County for the acquisition of property and the construction of public improvements and public buildings, including but not limited to the Ada County Courthouse, City Hall facilities, public safety facilities, and fire stations. The Agency also intends to cooperate with the Boise School District for any other appropriate public school facility. The Agency also intends to cooperate with the State of Idaho and the Greater Boise Auditorium

District for facilities which may be owned or occupied by those entities.

In the event the Agency is participating in the public development by way of financial incentive or otherwise, the public body shall enter into a participation agreement with the Agency and then shall be bound by the 2025 Downtown Redevelopment Plan and either the River–Myrtle Street Master Plan or the Old Boise–Eastside Master Plan, whichever is applicable, and other land use elements and shall conform to those standards specified in Section 405.02 of this Plan.

The Agency, by law, is not authorized to acquire real property owned by public bodies without the consent of such public bodies. The Agency, however, will seek the cooperation of all public bodies which own or intend to acquire property in the Project Area. Any public body which owns or leases property in the Project Area will be afforded all the privileges of an owner participant if such public body is willing to enter into a participation agreement with the Agency. All plans for development of property in the Project Area by a public body shall comply with the provisions of this Plan.

Section 305 Property Acquisition

Section 305.01 Real Property

Only as specifically authorized herein, the Agency may acquire, but is not required to acquire, real property located in the Project Area where it is determined that the property is needed for construction of public improvements, should be acquired to eliminate or mitigate the deteriorated or deteriorating conditions, and as otherwise allowed by law. The acquisition shall be by any means authorized by law (including, but not limited to, the Idaho Urban Renewal Law, the Local Economic Development Law, and the Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970). The Agency is authorized to acquire either the entire fee or any other interest in real property less than a fee, including structures and fixtures upon the real property, without acquiring the land upon which those structures and fixtures are located.

The Agency shall not real property to be retained by an owner pursuant to a participation agreement if the owner fully performed under the agreement.

Generally, the Agency intends to acquire any real property through voluntary or consensual gift, devise, exchange, or purchase. Such acquisition of property may be for the development of the public improvements identified in this Plan or for the assembly of properties for the redevelopment of those properties to achieve the objectives of this Plan. Such properties may include properties owned by private parties or public entities. This Plan does not anticipate the Agency's widespread use of its resources for property acquisition, except for the construction of public improvements and the ability to engage in certain demonstration projects, such as housing opportunities and other major objectives outlined in this Plan, the River Street–Myrtle Master Plan, and the Old Boise–Eastside Master Plan.

It is in the public interest and is necessary in order to eliminate the conditions requiring redevelopment and in order to execute this Plan for the power of eminent domain to be employed by the Agency to acquire real property in the Project Area which cannot be acquired by gift, devise, exchange, purchase, or any other lawful method.

The Agency shall not acquire real property on which an existing building is to be continued on its present site and in its present form and use without the consent of the owner unless: (a) such building requires structural alteration, improvement, modernization, or rehabilitation; (b) the site or lot on which the building is situated requires modification in size, shape, or use; (c) it is necessary to impose upon such property any of the controls, limitations, restrictions, and requirements of this Plan and the owner fails or refuses to execute a participation agreement in accordance with the provisions of this Plan; or (d) the site or portion thereof is necessary for public improvements.

The purpose of this section is to allow the Agency to use its eminent domain authority to acquire properties necessary for the construction of public improvements or for acquisition of those sites that are either deteriorated or deteriorating as described above.

Under the provisions of the Act, the urban renewal plan "shall be sufficiently complete to indicate such land acquisition, demolition and removal of structures, redevelopment, improvements, and rehabilitation as may be proposed to be carried out in the urban renewal area." Idaho Code § 50-2018(1).

The Agency has identified several parcels for possible acquisition for the construction of public improvements. Those parcels are contained within Attachments 4 and 5. The Agency also intends to acquire property for the purpose of developing public parking structures, public open space, and to enhance the opportunity for residential housing. At the present time, the Agency cannot specifically identify which parcels may be necessary for acquisition for the parking structures or for site assembly for private development. The Agency reserves the right to determine which properties, if any, should be acquired.

Section 305.02 Personal Property

Generally, personal property shall not be acquired. However, where necessary in the execution of this Plan, and where allowed by law, the Agency is authorized to acquire personal property in the Project Area by any lawful means, including eminent domain. For purposes of this Plan, acquisition of certain permanent fixtures or improvements upon real property shall be governed by this section. The Agency retains the right to purchase those fixtures or improvements (including buildings) for the purpose of eliminating certain deteriorated or deteriorating structures to facilitate the redevelopment of the real property upon which the buildings and structures are located. Such acquisition shall be based upon appraised value of the structures and negotiation with the owner of the structures. The Agency shall take into account, before committing to such acquisition, any environmental or other liability present or potentially present in such structures. In the event the Agency determines to acquire such property, it shall do so upon the successful negotiation of an owner participation agreement in compliance with the terms of Section 304 of this Plan. In addition, such owner shall commit to the redevelopment of the real property and to maintain the real property in a safe and clean manner. The Agency shall acquire such property by way of any acceptable conveyance.

Section 306 Property Management

During such time such property, if any, in the Project Area is owned by the Agency, such property shall be under the management and control of the Agency. Such property may be rented or leased by the Agency pending its disposition for redevelopment, and such rental or lease shall be pursuant to such policies as the Agency may adopt.

Section 307 Relocation of Persons (Including Individuals and Families), Business Concerns, and Others Displaced by the Project

If the Agency receives federal funds for real estate acquisition and relocation, the Agency shall comply with 24 C.F.R. Part 42, implementing the Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, as amended. The Agency may also undertake relocation activities for those not entitled to benefit under federal law as the Agency may deem appropriate for which funds are available. Persons leasing property held for redevelopment (i.e., those tenants who commence tenancy after acquisition by the Agency) shall not be eligible for relocation benefits. In the event the Agency's activities directly result in the displacement of families within the area, the Agency shall relocate such displaced families into descent, safe, and sanitary dwelling accommodations without undue hardship to such families. For any other activity, the Agency will comply with the provisions of the Idaho Urban Renewal Law regarding relocation.

The Agency reserves the right to extend benefits for relocation to those not otherwise entitled to relocation benefits as a matter of state law under the Act or the Law. The Agency may determine to use as a reference the relocation benefits and guidelines promulgated by the federal government, the state government, or local government, including the State Department of Transportation and the Ada County Highway District. The intent of this section is to allow the Agency sufficient flexibility to award relocation benefits on some rational basis, or by payment of some lump sum per case basis. For example Attachment 5 identifies relocation benefits of a maximum of \$10,000 per relocation case. The Agency may also consider the analysis of replacement value for the compensation awarded to either owner occupants or businesses displaced by the Agency to achieve the objectives of this Plan. The Agency may adopt relocation guidelines which would define the extent of relocation assistance in non-federally-assisted projects and which relocation assistance to the greatest extent feasible would be uniform.

For displacement of families, the Agency shall comply with, at a minimum, the standards set forth in the Law. The Agency shall also comply with all applicable state laws concerning relocation benefits.

Section 308 Demolition, Clearance, and Building and Site Preparation

Section 308.01 Demolition and Clearance

The Agency is authorized (but not required) to demolish and clear buildings, structures, and other improvements from any real property in the Project Area as necessary to carry out the purposes of this Plan.

Section 308.02 Preparation of Building Sites

The Agency is authorized (but not required) to prepare, or cause to be prepared, as building sites any real property in the Project Area owned by the Agency. In connection therewith, the Agency may cause, provide for, or undertake the installation or construction of streets, utilities, parks, pedestrian walkways, parking facilities, drainage facilities, and other public improvements necessary to carry out this Plan.

The Agency is authorized (but not required) to construct foundations, platforms, and other structural forms necessary for the provision or utilization of air rights sites for buildings to be used for residential, commercial, private, public, and other uses provided in this Plan.

The Agency is also authorized (but not required) to purchase certain site or building improvements for purpose of site preparation and development.

Section 309 Property Disposition and Development

Section 309.01 Real Property Disposition and Development

Section 309.01(a) General

For the purposes of this Plan, the Agency is authorized to sell, lease, exchange, subdivide, transfer, assign, pledge, encumber by mortgage or deed of trust, or otherwise dispose of any interest in real property under the reuse provisions set forth in Idaho Code Section 50-2011 and as otherwise allowed by law. To the extent permitted by law, the Agency is authorized to dispose of real property by negotiated lease, sale, or transfer without public bidding.

Real property acquired by the Agency may be conveyed by the Agency and, where beneficial to the Project Area, without charge to any public body as allowed by law. All real property acquired by the Agency in the Project Area shall be sold or leased to public or private persons or entities for development for the uses permitted in this Plan. All public bodies acquiring property through the Agency shall enter into a disposition and development agreement with the Agency.

All purchasers or lessees of property acquired from the Agency shall be obligated to use the property for the purposes designated in this Plan, to begin and complete development of the property within a period of time which the Agency fixes as reasonable, and to comply with other conditions which the Agency deems necessary to carry out the purposes of this Plan.

Section 309.01(b) Disposition and Development Documents

To provide adequate safeguards to ensure that the provisions of this Plan will be carried out and to prevent the recurrence of blight, all real property sold, leased, or conveyed by the Agency, as well as all property subject to participation agreements, is subject to the provisions of this Plan.

The Agency shall reserve such powers and controls in the disposition and development

documents as may be necessary to prevent transfer, retention, or use of property for speculative purposes and to ensure that development is carried out pursuant to this Plan.

Leases, deeds, contracts, agreements, and declarations of restrictions of the Agency may contain restrictions, covenants, covenants running with the land, rights of reverter, conditions subsequent, equitable servitudes, or any other provisions necessary to carry out this Plan. Where appropriate, as determined by the Agency, such documents, or portions thereof, shall be recorded in the office of the Recorder of Ada County.

All property in the Project Area is hereby subject to the restriction that there shall be no discrimination or segregation based upon race, color, creed, religion, sex, age, national origin, or ancestry in the sale, lease, sublease, transfer, use, occupancy, tenure, or enjoyment of property in the Project Area. All property sold, leased, conveyed or subject to a participation agreement shall be expressly subject by appropriate documents to the restriction that all deeds, leases, or contracts for the sale, lease, sublease, or other transfer of land in the Project Area shall contain such nondiscrimination and nonsegregation clauses as required by law.

The land and/or air rights and subterranean rights acquired by the Agency shall be disposed of subject to an agreement between the Agency and the Developers. The Developers (including owner/participants) will be required by the contractual agreement to observe provisions of this Plan and to submit a Redevelopment Schedule satisfactory to the Agency, as further defined in Section 405 of this Plan. Schedule revisions will be made only at the option of the Agency.

As required by law or as determined in the Agency's discretion to be in the best interest of the Agency and the public, the following requirements and obligations may be included in the agreement.

That the Developers, their successors, and assigns agree:

- (1) That a plan and time schedule for the proposed development shall be submitted to the Agency.
- (2) That the purchase or lease of the land and/or subterranean rights and/or air rights is for the purpose of redevelopment and not for speculation.
- (3) That the building of improvements will be commenced and completed as jointly scheduled and determined by the Agency and the Developer(s).
- (4) That there will be no discrimination against any person or group of persons because of age, race, sex, creed, color, national origin, ancestry, or handicap in the sale, lease, sublease, transfer, use, occupancy, tenure, or enjoyment of the premises or any improvements erected or to be erected thereon, therein conveyed nor will the Developer himself or any person claiming under or through him establish or permit any such practice or practices of discrimination or segregation with reference to the selection, location, number, use, or occupancy of tenants, lessees, sublessees, or vendees in the premises or any improvements therein conveyed. The

above provision will be perpetual and will be appended to the land disposed of within the Urban Renewal Project Area by the Agency.

- (5) That the site and construction plans will be submitted to the Agency for review as to conformity with the provisions and purposes of this Urban Renewal Plan and for design review purposes.
- (6) That at the discretion of the Agency a bond or other surety will be provided acceptable to the Agency to ensure performance under the contract of the sale.
- (7) That rehabilitation of any existing structure must assure that the structure is safe and sound in all physical respects and be refurbished and altered to bring the property to an upgraded marketable condition which will continue throughout an estimated useful life for a minimum of twenty (20) years.
- (8) All such buildings or portions of the buildings which are to remain within the Project Area shall be reconstructed in conformity with all applicable federal, state, and local codes and ordinances

All disposition and development documents shall also be governed by the provisions of Section 405.02 of this Plan. The Agency also reserves the right to determine the extent of its participation based upon the achievements of the objectives of this Plan.

Section 309.01(c) Development by the Agency

To the extent now or hereafter permitted by law, the Agency is authorized to pay for, develop, or construct any publicly-owned building, facility, structure, or other improvement within the Project Area for itself or for any public body or entity, which buildings, facilities, structures, or other improvements are or would be of benefit to the Project Area.

Specifically, the Agency may pay for, install, or construct the buildings, facilities, structures, and other improvements identified in the Appendix attached hereto and incorporated herein by reference and may acquire or pay for the land required therefor.

The Agency may also prepare properties for development by renovation or other means as allowed by law. The Agency may also as allowed by law assist in the development of private projects.

In addition to the public improvements authorized under Idaho Code Sections 50-2007, 50-2018, and 50-2903(9), (13), and (14) the Agency is authorized to install and construct, or to cause to be installed and constructed, within the Project Area for itself or for any public body or entity, public improvements and public utilities, including, but not limited to, the following: (1) utilities and telecommunications improvements; (2) parks, plazas, and pedestrian paths; (3) parking facilities; (4) landscaped areas; (5) street improvements and streetscaping (with prior consent of the Ada County Highway District); (6) sanitary sewers; (7) flood control facilities and storm drains; (8)

water mains; (9) courthouse facility, civic center, or other public buildings; (10) police and fire facilities; (11) geothermal system improvements; (12) cultural investments; and (1-1-3) transit transportation facilities, vehicles, and infrastructure.

The Agency seeks to develop a major new urban open space along the Pioneer Corridor. The design proposed for this open space is described in the River Myrtle–Old Boise Master Plan.

In partnership with other agencies such as Boise City, Ada County Highway District, Idaho Transportation Department, Boise State University, Valley Ride, and Community Planning Association of Southwest Idaho, the Agency seeks to develop a downtown circulator that would link together the various subdistricts and activity centers in downtown and encourage people to use of transit to meet daily needs rather than resorting to driving.

Any public facility ultimately owned by the Agency shall be operated and managed in such a manner to preserve the public purpose nature of the facility. Any lease agreement with a private entity or management contract agreement shall include all necessary provisions sufficient to protect the public interest and public purpose.

The Agency seeks to create a network of special streets, streetscapes, parks, plazas, and urban open spaces within the Project Area. Open spaces may include a water feature that is compatible with other street furnishings. Ample seating would be provided along any promenade. Enhanced lighting would be provided for safety. Open spaces would be programmed, designed, and promoted to accommodate active day, night, and seasonal uses. Intersections where one enters or leaves the CBD serve as potential nodes for enhanced design treatment. These features contribute to the public realm. When completed, the network of civic places and the connections between them will extend the amenities of the core to the Project Area.

Section 309.01(d) Development Plans

All development plans (whether public or private) prepared, pursuant to disposition and development or owner participation agreements, shall be submitted to the Agency for approval and design review.

All development plans (whether public or private) prepared pursuant to a disposition and development agreement or owner participation agreement shall be submitted to the Agency for approval and design review. All development under a disposition and development agreement or owner participation agreement must also conform to those standards specified in Section 405.02 of this Plan.

Section 310 Personal Property Disposition

For the purposes of this Plan, the Agency is authorized to lease, sell, exchange, transfer, assign, pledge, encumber, or otherwise dispose of personal property which is acquired by the Agency.

Section 311 Rehabilitation and Conservation

The Agency is authorized to rehabilitate, renovate, and conserve, or to cause to be rehabilitated, renovated, and conserved, any building or structure in the Project Area owned by the Agency for preparation of redevelopment and disposition. The Agency is also authorized and directed to advise, encourage, and assist in the rehabilitation and conservation of property in the Project Area not owned by the Agency. The Agency is also authorized to acquire, restore, rehabilitate, move, and conserve buildings of historic or architectural significance.

As necessary in carrying out this Plan, the Agency is authorized to move, or to cause to be moved, any standard structure or building or any structure or building which can be rehabilitated to a location within or outside the Project Area.

Section 312 Participation with Private or Public Development

Under the Idaho Urban Renewal Law and the Act, the Agency has the authority to lend or invest funds obtained from the federal government for the purposes of the Urban Renewal law if allowable under federal laws or regulations. The federal funds that may be available to the Agency are governed by regulations promulgated by the Department of Housing and Urban Development for the Community Development Block Grant Program.

Under those regulations the Agency may participate with the private sector in the development and financing of those private projects which will attain certain federal objectives.

The Agency may, therefore, use the federal funds for the provision of assistance to private for profit business, including, but not limited to, grants, loans, loan guarantees, interest supplements, technical assistance, and other forms of support, for any other activity necessary or appropriate to carry out an economic development project.

As allowed by law, the Agency may also use funds from any other sources for any purpose set forth under the Law or Act.

The Agency may enter into contracts, leases, and agreements with the City, or other public body or private entity, pursuant to this section, and the obligation of the Agency under such contract, lease, or agreement shall constitute an indebtedness of the Agency as described in Idaho Code Section 50-2909 which may be made payable out of the taxes levied in the Project Area and allocated to the Agency under subdivision 2(b) of Section 50-2908 of the Local Economic Development Act and Section 504 to this Plan or out of any other available funds.

Section 313 Conforming Owners

The Agency may, at the Agency's sole and absolute discretion, determine that certain real property within the Project Area presently meets the requirements of this Plan, and the owner of such property will be permitted to remain as a conforming owner without a participation agreement with the Agency, provided such owner continues to operate, use, and maintain the real property within the requirements of this Plan.

Section 314 Arts Funding

The Agency may provide funding for arts projects, including placement and maintenance of public art, integration of art with project design, and provision for performing arts facilities and equipment. Potential arts projects will be evaluated on a case-by-case basis prior to funding by the Agency. A fundamental criterion for providing funding will be the degree to which a particular art project contributes to revitalization of the downtown Boise and the achievement of the goals and objectives in the 2025 Downtown Redevelopment Plan and either the River–Myrtle Master Plan or the Old Boise/Eastside Master Plan, whichever is applicable. The Agency may use as a guideline that one percent (1.0%) of the construction cost to be allocated to arts projects, subject to budget considerations.

Projects the Agency funds for private projects developed under either an owner participation agreement or a disposition and development agreement, the Agency and owner/developer shall consider an appropriate arts component. If an arts component is agreed upon, the Agency and owner/developer shall seek input from the Arts Commission on the selection of the artist and work of art. For other projects the Agency shall encourage developer or owners to contact the Boise Arts Commission to consider a public arts component.

The Agency Board of Commissioners shall make selections of the works of art with the advice and assistance of the Boise City Arts Commission. Selected art projects may be an integral part of the public improvement/facility, attached to the structure, detached within or outside of the structure, or exhibited in other areas of the Project open to the general public.

Whenever possible, Agency arts funding will be used to leverage additional contributions from developers, other private sources, and public or quasi-public entities.

Section 315 Geothermal Resource

The Agency shall include in any owner participation agreement or disposition and development agreement entered into under the provisions of this Plan the following text pertaining to consideration of the use of the City's geothermal resource:

The Developer/Owner Participant shall investigate the feasibility of using the City's geothermal resource for heating the development. The Developer/Owner Participant shall contact and discuss the use of this resource with the City Public Works Department. In the event the Developer/Owner Participant determines not to avail itself of this resource, which determination shall be in the Developer/Owner Participant's sole discretion, the Developer/Owner Participant shall provide the Agency with documentation explaining the reason for not using this resource and evidence of contact with the City Public Works Department.

Section 400 Uses Permitted in the Project Area

Section 401 Redevelopment Plan Map and Development Strategy

The Description of the Project Area and Revenue Allocation Area Boundary and Project Area-Revenue Allocation Area Boundary Map, attached hereto as Attachments 1 and 2 and incorporated by reference, describe the location of the Project Area boundaries. The proposed land uses to be permitted in the Project for all land, public and private, are depicted in Attachments 3, 3A, 3D, and 3E.

Section 402 Designated Land Uses

Section 402.01 Land Use Classifications

The land use classifications for the Project Area are as shown and depicted in Attachment 5 and include general commercial (C-2), service commercial (C-3), residential- office (R-0), and central business (C-5), all as more specifically defined in chapter 4, title 11, Boise City Code. The Agency also recognizes those land uses permitted by conditional uses under each zoning classification, subject to the conditions imposed by the City of Boise pursuant to the condition use process. Any changes proposed to any zoning classifications or zoning regulations are described in Chapters 6, 7, and 8 in the River–Myrtle Master Plan and in Chapters 6 and 7 in the Old Boise–Eastside Master Plan.

Section 402.02 Preferred Land Use Framework Plan

The River Street–Myrtle Street Master Plan and the Old Boise–Eastside Master Plan, taken together, describe the preferred pattern for the development of land uses in order to achieve the overall vision for the combined River-Myrtle/Old Boise area. Proposed uses correspond to the following land use categories developed during the planning process for each area:

For the River–Myrtle planning area:

- Ground Level Retail
- Service Commercial
- Office Emphasis
- Residential Emphasis
- Urban Village
- Institutional Emphasis
- Mixed Use
- Civic Spaces

For the Old Boise–Eastside planning area:

- Retail Emphasis
- Office Emphasis
- Residential Emphasis

- Government Emphasis
- Medical Emphasis
- Mixed Use
- Civic Spaces

These land use categories are defined in the text of each master plan. A composite map showing the preferred development concept and pattern of land uses for the combined River Myrtle–Old Boise Urban Renewal District appears in the 2025 Downtown Redevelopment Plan.

Section 403 Other Land Uses

Section 403.01 Public Rights-of-Way

Additional public streets, alleys, and easements may be created in the Project Area as needed for proper development. Existing streets, alleys, and easements may be abandoned, closed, expanded or modified as necessary for proper development of the Project, in accordance with any applicable policies and standards of the City of Boise or Ada County Highway District regarding changes to dedicated rights-of-way.

Any changes in the existing interior or exterior street layout shall be in accordance with the objectives of this Plan and the design standards of the City, Ada County Highway Department, or Idaho Department of Transportation as may be applicable.

Any changes in the existing interior or exterior street layout shall be in accordance with the objectives of this Plan and the design standards of the City and ACHD; shall be effectuated in the manner prescribed by state and local law; and shall be guided by the following criteria:

- a. A balancing of the needs of proposed and potential new developments for pedestrian and vehicular access, transit facilities, vehicular parking and delivery loading docks with the similar needs of any existing developments permitted to remain. Such balancing shall take into consideration the rights of existing owners and tenants under the rules for owner and tenant participation adopted by the Agency for the Project and any participation agreements executed thereunder;
- b. The requirements imposed by such factors as topography, traffic safety, and aesthetics;
- c. The potential need to serve not only the Project Area and new or existing developments, but to also serve areas outside the Project by providing convenient and efficient vehicular access and movement; and
- d. The objective stated in the 2025 Downtown Redevelopment Plan, River–Myrtle Master Plan and Old Boise–Eastside Master Plan of restoring the traditional downtown street grid wherever possible, taking into consideration property ownerships and property rights of owners. Any re-establishment of public rights-

of-way for streets shall be in accordance with the policies and design standards and with the approval of the Ada County Highway District.

The public rights-of-way may be used for vehicular and/or pedestrian traffic, as well as for public improvements, public and private utilities, and activities typically found in public rights-of-way.

Section 403.02 Other Public, Semi-Public, Institutional, and Nonprofit Uses

The Agency is also authorized to permit the maintenance, establishment or enlargement of public, semi-public, institutional, or nonprofit uses. All such uses shall, to the extent possible, conform to the provisions of this Plan applicable to the uses in the specific area involved. The Agency may impose such other reasonable requirements and restrictions as may be necessary to protect the development and use of the Project Area.

Section 403.03 Interim Uses

Pending the ultimate development of land by developers and participants, the Agency is authorized to use or permit the use of any land in the Project Area for interim uses that are not in conformity with the uses permitted in this Plan. However, any interim use must comply with applicable Boise City Code and must have a specific expiration date.

Section 403.04 Nonconforming Uses

The Agency may permit an existing use to remain in an existing building and site usage in good condition, which use does not conform to the provisions of this Plan, provided that such use is generally compatible with existing and proposed developments and uses in the Project Area. The owner of such a property must be willing to enter into a participation agreement and agree to the imposition of such reasonable restrictions as may be necessary to protect the development and use of the Project Area.

The Agency may authorize additions, alterations, repairs, or other improvements in the Project Area for uses which do not conform to the provisions of this Plan where such improvements are within a portion of the Project where, in the determination of the Agency, such improvements would be compatible with surrounding Project uses and development.

All nonconforming uses shall also comply with the Boise City Code.

Section 404 General Controls and Limitations

All real property in the Project Area is subject to the controls and requirements of this Plan. No real property shall be developed, rehabilitated, or otherwise changed after the date of the adoption of this Plan except in conformance with the provisions of this Plan, except as provided in Section 405.

Section 404.01 Construction

All construction in the Project Area shall comply with all applicable state and local laws and codes in effect from time to time. In addition to applicable codes, ordinances, or other requirements governing development in the Project Area, additional specific performance and development standards may be adopted by the Agency to control and direct redevelopment activities in the Project Area in the event of a disposition and development agreement or owner participation agreement.

Section 404.02 Rehabilitation and Retention of Properties

Any existing structure within the Project Area, subject to either a disposition and development agreement or owner participation agreement, approved by the Agency for retention and rehabilitation, shall be repaired, altered, reconstructed, or rehabilitated in such a manner that it will be safe and sound in all physical respects and be attractive in appearance and not detrimental to the surrounding uses.

Section 404.03 Limitation on Type, Size, Height of Building

Except as set forth in other sections of this Plan, including Section 405 of this Plan, the type, size, and height of buildings shall be as limited by applicable federal, state, and local statutes, ordinances, and regulations.

Section 404.04 Open Spaces, Landscaping, Light, Air and Privacy

The issues of open space, landscaping, light, air and privacy shall be governed by applicable local ordinances.

Section 404.05 Signs

All signs shall conform to City sign ordinances as they now exist or are hereafter amended.

Section 404.06 Utilities

The Agency shall require that all utilities be placed underground whenever physically and economically feasible.

Section 404.07 Incompatible Uses

No use or structure which by reason of appearance, traffic, smoke, glare, noise, odor, or similar factors which would be incompatible with the surrounding areas or structures shall be permitted in any part of the Project Area.

Section 404.08 Nondiscrimination and Nonsegregation

There shall be no discrimination or segregation based upon race, color, creed, religion, sex,

marital status, national origin, ancestry, age or disability permitted in the sale, lease sublease, transfer, use, occupancy, tenure, or enjoyment of property in the Project Area.

Section 404.09 Subdivision of Parcels

Any parcel in the Project Area shall be subdivided only in compliance with the City Subdivision Ordinance.

Section 404.10 Variations

Under exceptional circumstances, the Agency is authorized to permit a variation from the limits, restrictions, and controls established by this Plan. In order to permit such variation, the Agency must determine that:

- a. The application of certain provisions of this Plan would result in practical difficulties or unnecessary hardships inconsistent with the general purpose and intent of this Plan;
- b. There are exceptional circumstances or conditions applicable to the property or to the intended development of the property which do not apply generally to other properties having the same standards, restrictions, and controls;
- c. Permitting a variation will not be materially detrimental to the public welfare or injurious to property or improvements in the area; and
- d. Permitting a variation will not be contrary to the objectives of this Plan or the Comprehensive Plan.

No variation shall be granted which changes a basic land use or which permits other than a minor departure from the provisions of this Plan, without amendment of this Plan. In permitting any such variation, the Agency shall impose such conditions as are necessary to protect the public peace, health, safety, or welfare and to assure compliance with the purposes of this Plan. Any variation permitted by the Agency hereunder shall not supersede any other approval required under City codes and ordinances.

Section 404.11 Off-Street Loading

Any development and improvements shall provide for off-street loading as required by the City ordinances as they now exist or are hereafter amended.

Section 404.12 Off-Street Parking

All new construction in the area shall provide off-street parking as required by the City ordinances as they now exist or are hereafter amended, recognizing the economics of a shared parking regimen and the parking objectives set forth in the 2025 Downtown Redevelopment Plan and the River–Myrtle Master Plan or the Old Boise–Eastside Master Plan, whichever is applicable.

Section 405 Design for Development

Section 405.01 Design Guidelines for Development

No new improvement shall be constructed, and no existing improvements shall be substantially modified, altered, repaired, or rehabilitated except in accordance with this Plan; provided, however, the Ada County Highway District shall not be subject to the jurisdiction of the Agency for modifications of public rights-of-way. The provisions of this Plan and the design and development guidelines contained in the 2025 Downtown Redevelopment, and either the River Street–Myrtle Street Master Plan or the Old Boise–Eastside Master Plan whichever is applicable, apply to all public and private property in the Project Area recognizing the need for flexibility in interpretation and implementation of the Plan and the urban design plan objectives and development guidelines from the appropriate plans noted in this Section 405.01 and the ability of the Agency to grant variations to the Plan, the 2025 Downtown Redevelopment Plan, the River–Myrtle Master Plan, and the Old Boise–Eastside Master Plan as allowed by Section 404.10 of this Plan.

The land use elements of the Urban Design Plan are encouraged but not specifically required for development within the Project Area unless an owner participation agreement or disposition and development agreement is entered into between the property owner or developer and the Agency.

All development under this section shall also comply with all applicable City zoning and building ordinances, including the 2025 Downtown Redevelopment Plan, the River-Myrtle Master Plan, and the Old Boise/Eastside Master Plan as may be adopted and implemented by the City of Boise.

Section 405.02 Design Guidelines for Development under a Disposition and Development Agreement or Owner Participation Agreement

Under an owner participation agreement or a disposition and development agreement the design guidelines and land use elements of the 2025 Downtown Redevelopment Plan and either the River–Myrtle Master Plan or the Old Boise–Eastside Master Plan, whichever is applicable, shall be achieved to the greatest extent feasible, though the Agency retains the authority to grant minor variations under Section 404.10 of this Plan and subject to a negotiated agreement between the Agency and the developer or property owner.

Under those agreements, the architectural, landscape, and site plans shall be submitted to the Agency and approved in writing by the Agency. In such agreements, the Agency may impose additional design controls. One of the objectives of this Plan is to create an attractive pedestrian environment in the Project Area. Therefore, such plans shall give consideration to good design, and pedestrian amenities to enhance the aesthetic quality of the Project Area. These additional design standards or controls will be implemented through the provisions of any disposition and development agreement or owner participation agreement or by appropriate covenants appended to the land and instruments of conveyance executed pursuant thereto. These controls are in addition to any standard and provisions of any applicable City building or

zoning ordinances; provided, however, each and every development shall comply with all applicable City zoning and building ordinances.

Within the limits, restrictions, and controls established in this Plan, the Agency is authorized to establish heights of buildings, land coverage, setback requirements, design criteria, traffic circulation, traffic access, and other development and design controls necessary for proper development of both private and public areas within the Project Area. Any development must also comply with the City of Boise's zoning ordinance regarding heights, setbacks, and other like standards.

In the case of property which is the subject of a disposition and development or participation agreement with the Agency, no new improvement shall be constructed, and no existing improvement shall be substantially modified, altered, repaired, or rehabilitated, except in accordance with this Plan. Under those agreements the architectural, landscape, and site plans shall be submitted to the Agency and approved in writing by the Agency. One of the objectives of this Plan is to create an attractive and pleasant environment in the Project Area. Therefore, such plans shall give consideration to good design, open space, and other amenities to enhance the aesthetic quality of the Project Area. The Agency shall not approve any plans that do not comply with this Plan. The Agency reserves the right to impose such design standards on an ad hoc, case-by-case basis through the approval process of the Owner Participation Agreement or disposition and development agreement. Any change to such approved design must be consented to by the Agency, and such consent may be conditioned upon reduction of Agency's financial participation toward the Project.

In the event the Agency adopts design standards or controls, those provisions will thereafter apply to each site or portion thereof in the Project Area. Those controls and standards will be implemented through the provisions of any disposition and development agreement or owner participation agreement or by appropriate covenants appended to the land and instruments of conveyance executed pursuant thereto. These controls are in addition to any standards and provisions of any applicable City building or zoning ordinances; provided, however, each and every development shall comply with all applicable City zoning and building ordinance.

Section 405.03 City Design Review Committee

By submitting this Plan and attachments for approval by the City Council, the Agency requests the City formally adopt the 2025 Downtown Redevelopment Plan, the River Street–Myrtle Street Urban Master Plan, and the Old Boise–Eastside Master Plan as additional design review guidelines for the Project Area.

Section 500 Methods of Financing the Project

Section 501 General Description of the Proposed Financing Method

The Agency is authorized to finance this Project with financial assistance from the City, State of Idaho, federal government, interest income, Agency bonds, donations, loans from private financial institutions, the lease or sale of Agency-owned property, lease revenue conduit and

financing, public parking revenue, revenue allocation funds as allowed by the Act, or any other available source, public or private, including assistance from any taxing district or any public entity.

The Agency is also authorized to obtain advances, borrow funds, and create indebtedness in carrying out this Plan. The principal and interest on such advances, funds, and indebtedness may be paid from any other funds available to the Agency. The City or other public entities, as it is able, may also supply additional assistance through loans and grants for various public facilities.

The City or any other public agency may expend money to assist the Agency in carrying out this Project.

Section 502 Revenue Bond Funds

As allowed by law and subject to such restrictions as are imposed by law, the Agency is authorized to issue bonds from time to time, if it deems appropriate to do so, in order to finance all or any part of the Project. Neither the members of the Agency nor any persons executing the bonds shall be liable personally on the bonds by reason of their issuance.

Section 503 Other Loans and Grants

Any other loans, grants, guarantees, or financial assistance from the United States, the State of Idaho, or any other public or private source will be utilized if available.

Section 504 Revenue Allocation Financing Provisions

The Agency hereby adopts revenue allocation financing provisions as authorized by Chapter 29, Title 50, Idaho Code (the "Act"), effective retroactively to January 1, 1994, for the original area and January 1, 2004, for the Old Boise/Eastside area and the adjacent River–Myrtle parcels. These revenue allocation provisions shall apply to all taxing districts in which the Revenue Allocation Area is located and described on Attachments 1 and 2 to this Plan. The Agency shall take all actions necessary or convenient to implement these revenue allocation financing provisions. The Agency specifically finds that the equalized assessed valuation of property within the Revenue Allocation Area is likely to increase as a result of the initiation of the Urban Renewal Project.

The Agency, acting by one or more resolutions adopted by its Board of Commissioners, is hereby authorized to apply all or any portion of the revenues allocated to the Agency pursuant to the Act to pay such costs as are incurred or to pledge all or any portion of such revenues to the repayment of any moneys borrowed, indebtedness incurred, or bonds issued by the Agency to finance or to refinance the Project Costs (as defined in Idaho Code Section 50-2903[14]) of one or more urban renewal projects.

Upon enactment of an ordinance by the governing body of the City of Boise, Idaho, finally adopting these revenue allocation financing provisions and defining the Revenue Allocation Area described herein as part of the Plan, there shall hereby be created a special fund of the Agency into which the County Treasurer shall deposit allocated revenues as provided in Idaho Code Section 50-2908. The Agency shall use such funds solely in accordance with Idaho Code Section 50-2909 and solely for the purpose of providing funds to pay the Project Costs, including any incidental costs, of

such urban renewal projects as the Agency may determine by resolution or resolutions of its Board of Commissioners.

A statement listing proposed public improvements and facilities, an economic feasibility study, estimated project costs, fiscal impact upon other taxing districts, and methods of financing project costs required by Idaho Code Section 50-2905 is included in Attachment 6⁹ to this Plan. This statement necessarily incorporates estimates and projections based on the Agency's present knowledge and expectations. The Agency is hereby authorized to modify the presently anticipated urban renewal projects and use of revenue allocation financing of the related Project Costs if the Board of Commissioners of the Agency deems such modification necessary or convenient to effectuate the general objectives of the Plan.

The Agency may also appropriate funds consisting of revenue allocation proceeds on an annual basis without the issuance of bonds. The Agency has also provided for obtaining advances or loans from the City, other public entity or private entity in order to immediately commence construction of certain of the public improvements. Revenues will continue to be allocated to the Agency until the improvements identified in Attachment 6 are completely constructed or until any obligation to the City or other public entity or private entity are fulfilled. Attachment 6 incorporates estimates and projections based on the Agency's present knowledge and expectations concerning the length of time to complete the improvements. The activity may take longer depending on the significance and timeliness of development. Alternatively the activity may be completed earlier if revenue allocation proceeds are greater or the Agency obtains additional funds.

The revenue allocation proceeds are hereby irrevocably pledged for the payment of the principal and interest on the advance of monies or making of loans or the incurring of any indebtedness such as bonds, notes, and other obligations (whether funded, refunded, assumed, or otherwise) by the Agency to finance or refinance the Project in whole or in part.

The Agency is authorized to make such pledges as to specific advances, loans, and indebtedness as appropriate in carrying out the Project.

Section 504.01 Keyser–Marston Feasibility Study

Attachment 6 consists of the Economic Feasibility Study (“Study”) of the First Amended and Restated River–Myrtle Urban Renewal District (annexing the Old Boise–Eastside Area and additional parcels to the River Street–Myrtle Street Urban Renewal District. The Study constitutes the financial analysis required by the Act.

Section 504.02 Assumptions and Conditions/Economic Feasibility Statement

The information contained in Attachment 6 assumes certain completed and projected actions. Under the provisions of the Act, the revenue allocation shall continue through the duration period of the Plan. First, all debt is projected to be repaid no later than the duration period of the Plan.

⁹ This reference was inadvertently listed as Attachment 5 in the Plan presented to the Boise City Council for adoption in 2004, but should have been listed as Attachment 6. This error was corrected in December, 2011.

Second, the total amount of bonded indebtedness and the amount of revenue generated by revenue allocation is dependent upon the extent and timing of private development. Should all of the development take place as projected, bonded indebtedness could be extinguished earlier, dependent upon the bond sale documents or other legal obligations. Should private development take longer to materialize, or should the private development be substantially less than projected, then the amount of revenue generated will be substantially reduced and bonds may continue for their full term.

The Urban Renewal Plan and attachments incorporate estimates and projections based on the Agency's present knowledge and expectations. The Agency may modify the project if the Board of Commissioners deems such modifications necessary to effectuate the Plan. The Plan proposes certain public improvements, including utility improvements, streetscapes, street improvements, property acquisition, and relocation costs, which will facilitate development in the Revenue Allocation Area.

Section 504.03 Ten Percent Limitation

Under the Act the base assessed valuation for all revenue allocation areas cannot exceed gross/net ten percent (10%) of the current assessed valuation for the entire City. The base assessment roll, Ret-including utilities and less any homeowner's exemption, for the original revenue allocation area for the Boise Central District is \$44,626,400 as of January 1, 1987, is as adjusted. The total assessed value for the City as of January 1, 1994, including utilities less homeowner's exemption for the original River Street–Myrtle Street Area, is \$109,119,200 as adjusted. The base assessment roll for the Old Boise Eastside Area and adjacent parcels less the homeowner's exemption is \$92,817,500. The base assessment roll including utilities less the homeowner's exemption for the Westside Area as of January 1, 2001, is \$291,274,400 as adjusted. The base assessment roll, including utilities and less any homeowner's exemption for the Boise Central District, the River Street Area, the Old Boise Area, the adjacent area, and the Westside Area is \$537,837,500 as adjusted. The total assessed value for the City of Boise as of January 1, 2004, less homeowner's exemptions, is \$12,935,853,147. The base assessment roll for all three Revenue Allocation Areas, as amended, does not exceed 10 percent of the assessed value for the City of Boise.¹⁰

Section 504.04 Financial Limitation

The Study identifies several capital improvement projects. Use of any particular financing source for any particular purpose is not assured or identified. Use of the funding source shall be conditioned on any limiting authority. For example, the Study identifies Facade Easements and Facade Improvements as capital projects. Use of revenue allocation funds for that purpose will be limited by the authority of the Act. If revenue allocation funds are unavailable, then the Agency will need to use a different funding source for that improvement.

The amount of funds available to the Agency from revenue allocation financing is directly related to the assessed value of new improvements within the revenue allocation area. Under the Act, the Agency is allowed the revenue allocation generated from inflationary increases and New Development Value (Table 4 of the Study). The Study has assumed a two percent (2%) inflationary

¹⁰ These assessed value figures have been obtained from the Ada County Assessor's Office and may be subject to clarification.

increase.

The Study, with the various estimates and projections, constitutes an economic feasibility study. Costs and revenues are analyzed and the analysis shows the need for public capital funds during the project. Multiple financing sources including proposed revenue allocation notes and bonds, annual revenue allocations, developer contributions, and other funds are shown. This Study identifies the kind, number, and location of all proposed public works or improvements, a detailed list of estimated project costs, a description of methods of financing illustrating project costs, and the time when related costs or monetary obligations are to be incurred. See Idaho Code § 50-2905. Based on these funding sources, the conclusion is that the project is feasible.

The information contained in the Study assumes certain projected actions. First, the Agency has projected several bond terms and note issues. The bond term will be finally determined by the marketability of the notes. Under the provisions of the Act, the revenue allocation may continue until the debt is satisfied for the remaining period of the Plan. Second, the total amount of indebtedness and the amount of revenue generated by revenue allocation is dependent upon the extent and time of private development. Should all of the development take place as projected, indebtedness would be extinguished earlier, dependent upon the bond sale documents and legal obligations therein. Should private development take longer to materialize or should the private development be substantially less than projected, then the amount of revenue generated will be substantially reduced and bonds may continue for their full term.

The proposed timing for the public improvements may very well have to be modified depending upon the availability of some of the funds and the Agency's ability to sell an initial issue of notes or bonds.

The Plan has shown that the equalized valuation of the Revenue Allocation Area defined in the Urban Renewal Plan is likely to increase as a result of the initiation and completion of urban renewal projects pursuant to the Urban Renewal Plan.

Section 505 Rebate of Revenue Allocation Funds

In any year during which the Agency receives revenue allocation proceeds, the Agency, as allowed by law, is authorized (but not required) to return or rebate to the other taxing entities identified in Attachment 6 of this Plan any revenue allocation funds previously pledged or committed for the purposes identified in the Plan. Under the Act, the Agency must first apply all such revenues for the payment of the projected costs of the urban renewal project identified and repayment of principal and interest on any moneys borrowed, indebtedness incurred, or bonds issued by the Agency and maintain any required reserve for payments of such obligation or indebtedness. Only to the extent revenues of the Agency exceed these obligations shall the Agency consider any rebate or return of revenue allocation funds to the other taxing entities.

The Agency shall rebate such funds in a manner that corresponds to each taxing entity's relative share of the revenue allocation proceeds or on the basis of extraordinary service requirements generated by the Project. All other taxing entities shall first receive any such rebate before such rebate shall be disbursed to the City.

Attachment 6 describes the Agency's financing plan for the Project. The Project will be financed, in part, through tax increment financing, using revenue allocation funds as allowed by the Act. The Agency anticipates that on an annual basis, tax increment, and other funds may be sufficient to satisfy the obligations incurred by the Agency, even though the entire amount of revenue allocation funds must be pledged for the term of any bonds or other debts incurred by the Agency. Therefore, on an annual basis, the Agency will consider the rebate of funds, which funds, may not be revenue allocation funds, but other funds available to the Agency (*e.g.*, net parking revenues).

Section 506 Participation with Local Improvement Districts

Under the Idaho Local Improvement District Code, Chapter 17, Title 50, Idaho Code, the City has the authority to establish local improvement districts for various public facilities, including, but not limited to, streets, curbs, gutters, sidewalks, drains, landscaping, and other like facilities. To the extent allowed by the Law and the Act, the Agency reserves the authority to participate in the funding of local improvement district facilities. This participation may include either direct funding to reduce the overall cost of the LID or to participate as an assessed entity to finance the LTD project.

Section 507 Issuance of Debt and Debt Limitation

Any debt incurred by the Agency as allowed by the Law and Act shall be secured by revenues identified in the debt resolution or revenue allocation funds as allowed by the Act. All such debt shall be repaid within the duration of this Plan.

Section 508 Impact on Other Taxing Districts and Levy Rate

A specific delineation of tax dollars generated by revenue allocation upon each taxing district has not been prepared. The overall impact of the revenue allocation project is shown in the Study. Since the passage of House Bill 156 in 1995, taxing entities are constrained in establishing levy rates by a function of the amount each budget of each taxing district can increase on an annual basis. The amounts set forth in the Study, Net Income, Table 2, would constitute the amounts distributed to the other taxing entities from the Revenue Allocation Area if there were no urban renewal project. Each individual district's share of that amount would be determined by its particular levy rate as compared to the other districts in any given year. Therefore, the impact of revenue allocation is more of a product of the imposition of House Bill 156. In addition, without the revenue allocation district and its ability to pay for public improvements and public facilities, fewer substantial improvements within the revenue allocation area would be expected in the next five to ten years, hence there would be lower increases in assessed valuation to be used by the other taxing entities.

Additionally, the Study has taken the existing 2001 levy rate of .014 and imposed such levy rate for the term of the Plan. One result of House Bill 156 is the likely reduction of the levy rate as assessed values increase for property within each taxing entity's jurisdiction. If the overall levy rate is less than .014, the Agency shall receive fewer funds from revenue allocation.

Section 509

Lease Revenue/Conduit Financing

Under the Law, the Agency is authorized to issue revenue bonds to finance certain public improvements identified in the Urban Renewal Plan. One example of a revenue bond is the financing of the new Ada County Courthouse Building. Under that type of financing, the County is paying the Agency a lease payment annually which provides certain funds to the Agency to retire the bond debt. Another variation of this type of financing is sometimes referred to as conduit financing, which provides a mechanism where the Agency uses its bonding authority for the project, with the end user making payments to the Agency to retire the bond debt. These sources of revenues are not related to revenue allocation funds and may not be particularly noted in the Study, because of the "pass through" aspects of the financing. Under the Act, the economic feasibility study focuses on the revenue allocation aspects of the Agency's financial model.

These financing models typically are for a longer period of time than the 24-year period set forth in the Act. However, these financing models do not involve revenue allocation funds, but rather funds from the end users which provide a funding source for the Agency to continue to own and operate the facility beyond the term of the Plan as allowed by Idaho Code Section 50-2905(7) as those resources involve funds not related to revenue allocation funds.

Section 510

Capital Improvement Plan/Distribution of Funds

As set forth in the River Myrtle Master Plan and the Old Boise/Eastside Master Plan, the combined plan area involves several different "subareas" with somewhat different needs. However, the Agency's funding sources through revenue allocation financing are generated on a broad, area-wide basis. Nonetheless, the Agency intends to distribute funds and resources, as financially feasible to benefit all of the various subareas. In order to achieve this goal, the Agency expects to adopt a capital improvement plan (CIP), which would identify, for successive five-year periods, the Agency's goals for improvements within the several subareas. To the extent financially feasible, one goal for the CIP is that each subarea contribute to its own improvement enhancements, through the revenue allocation or other resources generated from that subarea. Such resources would not be limited to revenue allocation funds, but would include grants, loans, local improvement district funding, business improvement district funding, and other like resources. The Agency intends to foster such programs and to coordinate other community-wide resources and organizations which may provide assistance to the subarea community, or individual property owners.

The Agency retains the discretion to achieve broader objectives throughout the entire Project area as more particularly described in this Plan and the 2025 Downtown Redevelopment Plan, the River–Myrtle Master Plan, and the Old Boise–Eastside Master Plan.

Additional details on the process to establish the Capital Improvement Plan are contained within the River–Myrtle Master Plan and Old Boise–Eastside Master Plan.

Section 600

Actions by the City

The City shall aid and cooperate with the Agency in carrying out this Plan and shall take all

actions necessary to ensure the continued fulfillment of the purposes of this Plan and to prevent the recurrence or spread in the area of conditions causing deterioration. Actions by the City shall include, but not be limited to, the following:

- a. Institution and completion of proceedings necessary for changes and improvements in private and publicly-owned property, rights-of-way, or public utilities within or affecting the Project Area.
- b. Revision of zoning (if necessary) within the Project Area to permit the land uses and redevelopment authorized by this Plan.
- c. Provision for administrative enforcement of this Plan by the City after development. The City and the Agency may develop and provide for enforcement of a program for continued maintenance by owners of all real property, both public and private, within the Project Area throughout the duration of this Plan.
- d. Preservation of historical sites.
- e. Performance of the above actions and of all other functions and services relating to public peace, health, safety, and physical development normally rendered in accordance with a schedule which will permit the redevelopment of the Project Area to be commenced and carried to completion without unnecessary delays.
- f. Institution and completion of proceedings necessary for the consideration of a local improvement district or business improvement district under Chapters 17 and 26, Title 50, Idaho Code, if requested.
- g. The undertaking and completing of any other proceedings necessary to carry out the Project.
- h. Administration of Community Development Block Grant and other state and federal grant funds that may be made available for the Project.
- 1. Appropriate agreements with the Agency for administration, supporting services, funding sources, and the like.
- J. The waiver of any hookup or installation fee for sewer, water, or other utility services for any facility owned by any public agency, including the Agency, if the City deems such waiver necessary.
- k. The waiver of any impact fee assessed by the City, if the City deems such waiver necessary.
- I. Imposition, wherever necessary (by conditional use permits or other means as appropriate), of controls within the limits of this Plan upon parcels in the Project Area to ensure the proper development and use.

The foregoing actions which may be taken by the City do not constitute any commitment for financial outlays by the City.

Section 700 **Enforcement**

The administration and enforcement of this Plan, including the preparation and execution of any documents implementing this Plan, shall be performed by the Agency and/or the City.

The provisions of this Plan or other documents entered into pursuant to this Plan may also be enforced by court litigation instituted by either the Agency or the City. Such remedies may include, but are not limited to, specific performance, damages, reentry, injunctions, or any other remedies appropriate to the purposes of this Plan. In addition, any recorded provisions which are expressly for the benefit of owners of property in the Project Area may be enforced by such owners.

Section 800 **Duration of This Plan**

Except for the nondiscrimination and nonsegregation provisions which shall run in perpetuity, the provisions of this Plan shall be effective and the provisions of other documents formulated pursuant to this Plan shall be effective for thirty (30) years from the date of adoption of the original Plan by the City Council in 1994, which period shall expire on December 31, 2024, except for any revenue allocation proceeds received in calendar year 2025.

This Plan shall terminate no later than December 31, 2024, except for revenues which may be received in 2025. The Plan shall terminate either on January 1, 2024, or if the Agency determines an earlier termination date, it shall terminate in accordance with the following:

- a. When the revenue allocation area plan budget estimates that all financial obligations have been provided for, the principal of and interest on such moneys, indebtedness, and bonds have been paid in full or when deposits in the special fund or funds created under this chapter are sufficient to pay such principal and interest as they come due, and to fund reserves, if any, or any other obligations of the Agency funded through revenue allocation proceeds shall be satisfied and the Agency has determined no additional project costs need be funded through revenue allocation financing, the allocation of revenues under Section 50-2908, Idaho Code, shall thereupon cease; any moneys in such fund or funds in excess of the amount necessary to pay such principal and interest shall be distributed to the affected taxing districts in which the revenue allocation area is located in the same manner and proportion as the most recent distribution to the affected taxing districts of the taxes on the taxable property located within the revenue allocation area; and the powers granted to the urban renewal agency under Section 50-2909, Idaho Code, shall thereupon terminate.
- b. In determining the termination date, the Plan shall recognize that the Agency shall receive allocation of revenues in the calendar year following the last year of the

revenue allocation provisions described in the urban renewal plan.

- c. For the fiscal year that immediately predates the termination date, the Agency shall adopt and publish a budget specifically for the projected revenues and expenses of the Plan and make a determination as to whether the revenue allocation area can be terminated before January 1 of the termination year pursuant to the terms of Section 50-2909(4), Idaho Code. In the event that the Agency determines that current tax year revenues are sufficient to cover all estimated expenses for the current year and all future years, by September 1, the Agency shall adopt a resolution advising and notifying the local governing body, the county auditor, and the State Tax Commission, recommending the adoption of an ordinance for termination (of)¹¹ the revenue allocation area by December 31 of the current year, and declaring a surplus to be distributed as described in Section 50-2909, Idaho Code, should a surplus be determined to exist. The Agency shall cause the ordinance to be filed with the office of the county recorder and the Idaho State Tax Commission as provided in Section 63-215, Idaho Code.

As allowed by Idaho Code Section 50-2905(7), the Agency may retain assets or revenues generated from such assets as loans the Agency shall have or resources other than revenue allocation funds to operate and manage such assets. The Agency likely will retain ownership of the several parking garages which may be constructed in the Project area, as parking revenues may be sufficient to provide the resources necessary for the Agency to retain those assets. Similarly, facilities like the Ada County Courthouse building which provide a lease income stream to the Agency for full retirement of the facility debt, which will allow the Agency to meet debt service obligations and provide for the continued operation and management of the facility.

For those assets which do not provide such resources or revenues, the Agency will likely convey such assets to the City of Boise or the Ada County Highway District, depending on the nature of the asset.

Section 900 Procedure for Amendment

The Urban Renewal Plan may be further modified at any time by the Agency and in compliance with the Law and the Act. Where the proposed modification will substantially change the Plan, the modifications must be approved by the City Council in the same manner as the original Plan and the amended and restated Plan. Substantial changes for City Council approval purposes shall be regarded as revisions in project boundaries, land uses permitted, land acquisition, and other changes which will violate the objectives of this Plan. Any amendment or modification shall be subject to such rights at law or equity any individual or entity who has entered into a disposition or development agreement or owner participation agreement with the Agency or its successor or successors in interest may be entitled to assert.

¹¹ This word was typed as “fo” instead of “of” in the Plan presented to the Boise City Council for adoption in 2004. This typographical error was corrected in December, 2011.

Section 1000 **Severability**

If any one or more of the provisions contained in this Plan to be performed on the part of the Agency shall be declared by any court of competent jurisdiction to be contrary to law, then such provision or provisions shall be null and void and shall be deemed separable from the remaining provisions in this Plan and shall in no way affect the validity of the other provisions of this Plan.

Section 1100 **Performance Review**

Under the Idaho Urban Renewal Law, the Agency is required to file with the City, on or before March 31 of each year, a report of the Agency's activities for the preceding calendar year, which report shall include a complete financial statement setting forth its assets, liabilities, income, and operating expenses as of the end of such calendar year. In addition to this report, the Agency shall file with the City on an annual basis an official summary adopted by the Agency Board describing in detail the Agency's performance during the preceding year. The annual report on Agency performance may be filed with the City as part of the annual report required under the Law.

8/27/04

Issued with corrections – December 2011 (Revisions identified and explained in footnotes.)

Issued with corrections – March 2015 (Revisions identified and explained in footnotes.)

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ATTACHMENTS

Attachment 1 Description of the Project Area and Revenue Allocation Area;

Attachment 2 Project Area–Revenue Allocation Area Boundary Map

Attachment 3

- 3A 2025 Downtown Redevelopment Plan;
- 3B Downtown Streetscaping Standards, updated in part and approved by the Agency pursuant to Resolution No. 1002 dated August 23, 2004;
- 3C Elements of Continuity, updated in part and approved by the Agency pursuant to Resolution No. 1002 dated August 23, 2004;
- 3D The River Street-Myrtle Street Urban Design Master Plan as adopted by the Agency pursuant to Resolution No. 677 dated October 27, 1994 and adopted by the Boise City Council pursuant to Ordinance 5596 on December 6, 2004, as revised by the Agency pursuant to Resolution No. 1002 dated August 23, 2004 and revised by the Boise City Council pursuant to Ordinance #6362 on November 30, 2004, and renamed the River Street-Myrtle Street Master Plan¹²;
- 3E The Old-Boise-Eastside Master Plan dated August 23, 2004, as adopted by the Agency pursuant to Resolution No. 1002 dated August 23, 2004; and adopted by the Boise City Council pursuant to Ordinance #6362 on November 30, 2004¹²;

Attachment 4 Private Properties Which May Be Acquired by the Agency;

Attachment 5 Map Depicting Current Zoning within Revenue Allocation Area and Project Area;

Attachment 6 Economic Feasibility of the First Amended and Restated River Myrtle–Old Boise Urban Renewal District (Annexing the Old Boise Eastside Study Area and additional parcels to the River Street–Myrtle Street Urban Renewal District.

¹² The original descriptions for 3D and 3E appeared in the draft River Myrtle-Old Boise Urban Renewal Plan sent by the CCDC Board of Commissioners to the Boise City Council for consideration, and only included the action taken by the CCDC Board to adopt these two master plans as attachments to the Urban Renewal Plan. These descriptions have been revised to include the action taken by the Boise City Council to adopt the River Street – Myrtle Street Master Plan and the Old Boise – Eastside Master Plan as attachments to the Urban Renewal Plan in November 2004. These revisions were made in March 2015.

RIVER MYRTLE - OLD BOISE URBAN RENEWAL PLAN¹

ATTACHMENTS 1 & 2: LEGAL DESCRIPTION

The boundary description of an urban renewal district and revenue allocation area is done in two parts: a metes and bounds description and a map. Idaho Code requires that both of these items be prepared by a licensed surveyor. The legal description for the River-Myrtle/Old Boise Urban Renewal District is being prepared by Quadrant Consulting, and it will be completed in time for the City Council hearing on the urban renewal district. Legal counsel has advised us that a map showing the proposed boundary is sufficient for the CCDC Board's consideration of this issue.

P:\Urban Renewal Districts\UR Districts\River Myrtle-Old Boise - 2004\Urban Renewal Plan - Prep\URP-attach 1-2 LEGAL
DESCRIP REV 3-21-15.doc

¹Name of urban renewal district as it appears on this page has been changed from the original "River-Myrtle/Old Boise Urban Renewal District" to the simplified version now in common usage "River Myrtle-Old Boise Urban Renewal District". Revision made in March 2015. Path to filename in 2004 originally appeared at bottom of page. This path has been replaced by current path.

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AMENDED
PROJECT AREA AND REVENUE ALLOCATION BOUNDARY OF THE
RIVER STREET- MYRTLE STREET URBAN RENEWAL PROJECT
BOISE, IDAHO

A tract of land consisting of approximately 340 acres being portions of subdivided and un-subdivided land situated in portions of Sections 3, 4, 9, 10 and 11 of Township 3 North, Range 2 East, Boise Meridian, Boise City, Ada County, Idaho. Said tract being more particularly described by record information as follows:

Commencing at the Section Corner common to Sections 3, 4, 9 and 10, of Township 3 North, Range 2 East, Boise Meridian thence North 00°21'00" East 212.53 feet along the Section line common to said Sections 3 and 4 to a point on Northeasterly Right-Of-Way of Grove Street and the Westside Urban Renewal District Boundary and the **POINT OF BEGINNING**; thence tracing said Westside District Boundary the following courses,

South 55°18'00" East 2198.45 feet along said Northeasterly Right-of-Way of Grove Street to the Northwesternly Right-Of-Way of North 10th Street, thence
South 34°42'00" West 340.00 feet along said Northwesternly Right-Of-Way to the Northeasterly Right-Of-Way of Front Street, thence
South 55°18'00" East 380.00 feet along said Northeasterly Right-Of-Way to the Northwesternly Right-Of-Way of North 9th Street and the Central Urban Renewal District boundary, thence leaving said Westside District boundary and tracing said Central District boundary the following courses,
South 35°11'57" West 210.00 feet along said Northwesternly Right-Of-Way of North 9th Street to the Northwesternly extension of the Southwesterly line of Lots 1 and 12 of Block 1 of the Davis Addition to Boise City, recorded at Book 1, Page 2 of Plats, thence along said extension and the Southwesterly line of Lots 1 and 12 of Block 1 and the Southwesterly line of Lots 1 and 12 of Block 7 of Davis Addition, recorded at Book 2, Page 93 of Plats,
South 55°17'58" East 841.83 feet along said Southwesterly line and its Southeasterly extension to the Southeasterly Right-Of-Way of North Capitol Boulevard, thence
North 34°42'00" East 810.00 feet along said Southeasterly Right-Of-Way to the Southwesterly Right-Of-Way of West Main Street, thence
South 55°18'00" East 380.00 feet along said Southwesterly Right-Of-Way to the Southeasterly Right-Of-Way of North 6th Street, thence along said Southeasterly line
North 34°42'00" East 420.00 feet to the Northeasterly Right-Of-Way of West Idaho Street, thence along said Northeasterly Right-Of-Way
North 55°18'00" West 380.00 feet to said Southeasterly Right-Of-Way of North Capitol Boulevard, thence along said Southeasterly Right-Of-Way
North 34°42'00" East 340.00 feet to the Northeasterly Right-Of-Way of West Bannock Street, thence along said Northeasterly Right-Of-Way
North 55°18'00" West 380.00 feet to a point on the Southeasterly Right-Of-Way of North 8th Street, said point being common to said Central Urban Renewal District and the Westside Urban Renewal District boundary, thence leaving said Central District boundary and

following said Westside District boundary,
North 34°42'00" East 340.00 feet along said Southeasterly Right-Of-Way of North 8th Street to the
Northeasterly Right-Of-Way of West Jefferson Street, thence
South 55°18'00" East 1900.00 feet to the Southeasterly Right-Of-Way of North 3rd Street, thence
along said Southeasterly Right-Of-Way
South 34°42'00" West 340.00 feet to the Northeasterly Right-Of-Way of West Bannock Street,
thence along said Northeasterly Right-Of-Way
South 55°18'00" East 380.00 feet to the Southeasterly Right-Of-Way of North 2nd Street, thence
along said Southeasterly Right-Of-Way
South 34°42'00" West 680.00 feet to the Northeasterly Right-of-Way of West Main Street, thence
along said Northeasterly Right-Of-Way
South 55°18'00" East 300.00 feet to the Southeasterly corner of Block 36 of said Boise City
Original Townsite, thence
South 46°22'57" East 80.44 feet to the intersection of said Northeasterly Right-of-way of West
Main Street and the Southeasterly Right-Of-Way of North 1st Street, thence along said
Northeast Right-Of-Way of West Main and its extension the following two courses:

South 69°18'27" East 140.79 feet, thence
South 78°28'36" East 473.00 feet, thence leaving said extension

South 41°43'59" East 85.65 feet to the intersection of the Southwest Right-Of-Way of Warm
Springs Avenue and the Easterly Right-Of-Way of South Broadway Avenue, thence
following said Easterly Right-Of-Way the following courses:

South 48°27'33" West 60.96 feet, thence
South 42°03'25" West 55.90 feet, thence
South 33°00'01" West 55.24 feet, thence
South 25°18'49" West 37.50 feet, thence
South 10°45'12" West 77.34 feet, thence
South 09°00'53" West 16.60 feet, thence
South 03°59'44" West 38.12 feet, thence
South 00°31'07" East 37.69 feet, thence
South 81°06'56" West 12.89 feet, thence
South 00°31'58" West 327.06 feet, thence
North 88°22'04" East 7.95 feet, thence
South 57°20'44" East 10.78 feet, thence
South 00°43'59" East 365.82 feet, thence
South 26°30'01" East 22.96 feet, thence
South 01°22'40" East 117.81 feet, thence
South 00°09'38" East 22.21 feet, thence
South 89°50'22" West 4.00 feet, thence
South 00°09'38" East 163.22 feet, thence
South 00°22'06" West 532.62 feet to the extension of the Southwesterly Right-Of-Way of West
Myrtle Street, thence leaving said Easterly Right-Of-Way of South Broadway Avenue,

North 55°17'22" West 1985.15 feet along said Southwesterly Right-Of-Way to the Southeasterly Right-Of-Way of South 2nd Street, thence
South 34°36'47" West 79.94 feet said along Southeasterly Right-Of-Way to the Southwesterly line of Central Subdivision, Book 1 at Page 7A of Plats, thence
North 55°17'12" West 836.09 feet along said Southwesterly line to a point on the Southeasterly line of a parcel of land described in Instrument No. 8915641, thence along the said Southeasterly line and the Southwesterly lines of said Instrument No. 8915641 and Instruments No. 8845056, 9006162 and 8855098, Ada County records, the following courses:

South 34°43'09" West 56.00 feet, thence
North 55°15'39" West 172.96 feet, thence
South 34°38'38" West 19.13 feet, thence
North 55°14'23" West 142.14 feet to the Southeasterly Right-Of-Way of South 5th Street, thence
South 34°53'53" West 187.60 feet along said Southeasterly Right-Of-Way to the Southeasterly extension of the Southwest Right-Of-Way of West Fulton Street, thence
North 55°20'40" West 760.33 feet along said Southeasterly extension and said Southwest Right-Of-Way to the Southeasterly Right-Of-Way of South Capitol Boulevard, thence along said Southeasterly Right-Of-Way the following courses
South 34°36'31" West 655.20 feet, thence
South 57°12'28" East 10.47 feet, thence
South 34°46'31" West 288.61 feet, thence leaving said Southeasterly Right-Of-Way
North 34°28'07" West 106.99 feet to the Southeast corner of the Log Cabin/Greenbelt Parcel as described by a "Waiver of Reversion Right and Consent to Lease" on file with the Boise Parks Department, thence
North 42°19'27" West 297.40 feet along the Southerly line of said Log Cabin/Greenbelt Parcel to the Southwest Corner of said Parcel, thence
North 64°11'30" West 163.14 feet to the Southwesterly Right-Of-Way of South 9th Street, thence along said Southwesterly Right-Of-Way the following courses:

North 10°11'51" East 110.22 feet, thence
North 02°09'01" East 109.91 feet, thence
North 40°50'40" West 8.36 feet, thence
North 87°34'51" West 9.88 feet, thence
North 02°09'18" East 42.67 feet, thence
North 21°54'06" East 10.70 feet, thence
North 79°08'10" East 6.50 feet, thence
North 02°08'53" East 82.67 feet, thence
North 13°56'39" East 27.56 feet, thence
North 02°09'01" East 50.39 feet to the Southwesterly Right-Of-Way of West River Street, thence along said Southwesterly Right-Of-Way the following courses:

38.34 feet along a non-tangent curve to the left, said curve having a radius of 45.32 feet, a delta angle of 48°28'32" and a chord bearing and distance of North 56°30'31" West 37.21 feet, thence

13.99 feet along a curve to the left, said curve having a radius of 114.50 feet, a delta angle of 7°00'00" and a chord bearing and distance of North 84°14'43" West 13.98 feet, thence

197.48 feet along a curve to the right, said curve having a radius of 380.19 feet, a delta angle of 29°45'40" and a chord bearing and distance of North 70°03'08" West 195.27 feet, thence

North 55°16'39" West 522.74 feet, thence

North 54°25'27" West 80.21 feet, thence

North 55°20'21" West 272.03 feet, thence

North 61°00'23" West 50.67 feet, thence

North 55°20'21" West 50.76 feet, thence

136.12 feet along a curve to the right, said curve having a radius of 265.00 feet, a delta angle of 29°25'51" and a chord bearing and distance of North 42°22'21" West 134.63 feet, thence

North 29°24'21" West 9.77 feet, thence leaving said Southwesterly line of West River Street and following the Southeasterly Right-Of-Way of South Pioneer Avenue the following courses:

6.80 feet along a curve to the left, said curve having a radius of 4.33 feet, a delta angle of 89°56'01" and a chord bearing and distance of North 74°24'21" West 6.12 feet, thence

23.57 feet along a curve to the right, said curve having a radius of 56.00 feet, a delta angle of 24°07'09" and a chord bearing and distance of South 72°39'09" West 23.40 feet, thence

South 84°42'39" West 20.43 feet, thence

23.25 feet along a curve to the left, said curve having a radius of 65.00 feet, a delta angle of 20°29'51" and a chord bearing and distance of South 74°27'39" West 23.13 feet, thence

South 64°12'39" West 177.58 feet to a point on said Southeasterly Right-Of-Way which is common to Lot 8 and Lot 9 of Forest River No. 3 Subdivision, Book 54 of Plats at Pages 4865-4867, thence along the line common to said Lots the following courses:

South 25°47'21" East 55.00 feet, thence

South 64°12'39" West 215.42 feet, thence

North 66°14'21" West 41.78 feet, thence

South 23°43'58" West 137.00 feet to a point being the most Southerly corner of said Lot 8 and the most Westerly Corner of said Lot 9, said point also being on the Northeasterly line of a parcel of land described in Instruments No. 888062 and 7612858, records of Ada County, thence tracing said parcel the following courses:

South 65°59'58" East 527.02 feet, thence

South 19°27'12" West 45.00 feet, thence

South 70°32'48" East 50.00 feet, thence

South 64°05'48" East 22.71 feet to the line common to said Sections 9 and 10, thence

South 00°31'12" West 40.00 feet along said line, thence leaving said line and following the Northerly Bank of the Boise River the following courses:

North 66°53'01" West 645.60 feet, thence

North 85°34'48" West 58.94 feet, thence

North 60°54'48" West 150.90 feet, thence

North 78°54'48" West 55.40 feet, thence

North 50°54'48" West 69.00 feet, thence

North 58°09'49" West 302.69 feet, thence

North 51°14'48" West 200.00 feet, thence

North 66°14'48" West 51.90 feet, thence

North 55°54'48" West 113.94 feet, thence

North 25°47'48" West 22.40 feet, thence

North 44°28'46" West 42.51 feet to the centerline of a vacated portion of South 13th Street, thence leaving said Northerly River Bank

North 63°54'56" East 111.12 feet along said centerline to the Southwesterly Right-Of-Way of West Shoreline Drive, thence

South 27°41'04" East 40.00 feet along said Southwesterly Right-Of-Way, thence leaving said Right-Of-Way continuing along the Southerly lines of Forest River No. 1, No. 2 and No. 3 Subdivision the following course:

South 47°51'02" East 363.97 feet, thence

South 66°00'32" East 195.16 feet to the Southwesterly corner of Lot 6, Block 1 of Forest River No. 2 Subdivision. Book 53 of Plats at Pages 4634-4635, thence leaving said parcel (Instruments No. No. 888062 and 7612858), and following the Northwesterly lines of said Lot 6 and Lot 5 of said Block 1 the following courses:

North 23°59'55" East 59.23 feet, thence

North 64°12'39" East 150.39 feet, thence

North 25°47'21" West 32.00 feet, thence

North 64°12'39" East 94.70 feet, thence

North 25°47'21" West 88.00 feet, thence

North 64°12'39" East 203.33 feet to the Easternmost corner of a parcel of land described in Instrument No. 9105774, thence along the Northeasterly line of said parcel and the Northwesterly extension thereof

North 46°01'12" West 433.57 feet to the Northwesterly Right-Of-Way of South 13th Street, thence

North 64°05'23" East 332.86 feet along said Northwesterly Right-Of-Way to the Easternmost corner of Lot 6, Block 3 of the Riverside Addition to Boise, Book 1 of Plats at Page 13, thence

North 25°54'55" West 142.03 feet along the Northeasterly line of said Lot 6 to the Northernmost corner of said Lot 6, thence

South 64°04'52" West 150.00 feet along the Northwesterly line of Lots 6 through 1 of said Block 3 to the Westernmost corner of said Lot 1, thence

North 25°55'03" West 16.00 feet to the Southerly corner of Lot 24, Block 3 of said Riverside Addition, thence along the Southeasterly boundary of the Peckstein Addition to the City of Boise, Book 7 of Plats at Page 314 the following courses:

South 64°07'20" West 62.23 feet, thence

North 26°04'35" West 75.35 feet, thence

South 51°22'10" West 148.70 feet to the Northeasterly line of a parcel of land described in Instrument No. 8949797, records of Ada County, thence tracing said parcel the following courses:

South 64°12'12" West 12.13 feet, thence

South 26°14'20" East 50.80 feet, thence

South 64°07'56" West 231.00 feet, thence

North 25°53'15" West 150.15 feet to the Westernmost corner of said parcel, said corner also being on the Southeasterly Right-Of-Way of South 14th Street, thence

South 64°29'33" West 284.85 feet along said Southeasterly Right-Of-Way to the Southwesterly Right-Of-Way of West Shoreline Drive, thence along said Southwesterly Right-Of-Way the following courses:

North 25°47'41" West 87.63 feet, thence

North 06°13'38" West 87.57 feet, thence

North 16°39'40" West 154.91 feet, thence

North 25°43'16" West 389.36 feet to the Southeasterly Right-Of-Way of North Americana Boulevard, thence

North 31°25'49" West 92.09 feet to the Northwesterly Right-Of-Way of said North Americana Boulevard, thence along said Northwesterly Right-Of-Way the following courses:

North 20°36'39" East 43.42 feet, thence

North 48°00'12" East 81.55 feet, thence

South 79°04'18" East 39.05 feet, thence

223.77 feet along a curve to the right, said curve having a radius of 1453.37 feet, a delta angle of 08°49'18" and a chord bearing and distance of North 59°51'19" East 223.55 feet, thence

South 25°44'18" East 10.00 feet, thence

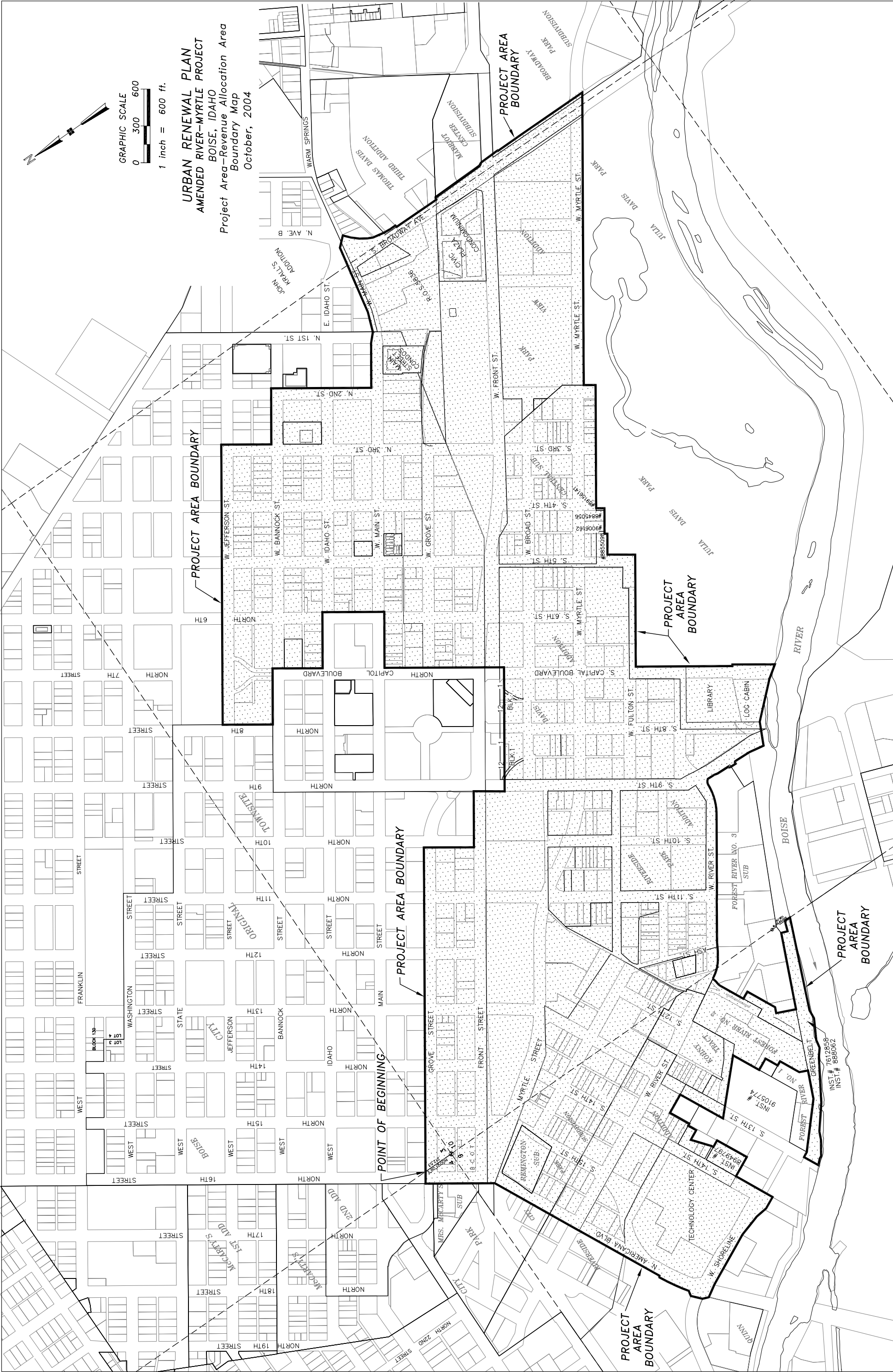
North 64°15'42" East 624.15 feet to the Southwesterly Right-Of-Way of West River Street, thence continuing along said Northwesterly Right-Of-Way of North Americana Boulevard

North 64°22'00" East 656.83 feet to the intersection of the Northeasterly extension of said Northwesterly Right-Of-Way of North Americana and the Southwesterly extension of the Northwesterly Right-Of-Way of North 16th Street, thence

North 34°37'20" East 388.82 feet along the said Southwesterly extension and Northwesterly Right-Of-Way of North 16th Street to a point on the Southwest Right-Of-Way of Grove Street, said point also being on the Westside Urban Renewal District Boundary, thence tracing said Westside District boundary the following courses

North 34°37'20" East 80.00 feet to the Northeasterly Right-Of-Way of Grove Street, thence

South 55°18'00" East 81.55 feet along said Northeasterly Right-Of-Way to the **POINT OF BEGINNING**.



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RIVER MYRTLE - OLD BOISE URBAN RENEWAL PLAN¹

ATTACHMENT 3: MASTER PLANS

Attachment 3 to the River-Myrtle/Old Boise Urban Renewal Plan consists of the following documents:

- 3A: 2025 Downtown Boise Redevelopment Plan**
The 2025 Downtown Boise Redevelopment Plan is not completed as of August 23, 2004. This place in attachment 3 to the urban renewal plan is held for the document to be completed and adopted at a later date.
- 3B: Elements of Continuity** *(3C in the Urban Renewal Plan)*²
- 3C: Streetscaping Standards** *(3B in the Urban Renewal Plan)*²
- 3D: River Street-Myrtle Street Master Plan**
- 3E: Old Boise/Eastside Master Plan**

P:\Urban Renewal Districts\UR Districts\River Myrtle-Old Boise - 2004\Urban Renewal Plan - Prep\URP-
attch 3A-E MASTER PLANS REV 3-21-15.doc

¹ Name of urban renewal district as it appears on this page has been changed from the original “River-Myrtle/Old Boise Urban Renewal District” to the simplified version now in common usage “River Myrtle-Old Boise Urban Renewal District”. Revision made in March 2015. Path to filename in 2004 originally appeared at bottom of page. This path has been replaced by current path.

² The list of attachments that appears on pages 2-3 and on page 44 of the River Myrtle-Old Boise Urban Renewal Plan identifies the Streetscape Standards as Attachment 3B and the Elements of Continuity as Attachment 3C. The list on this face page has the order of these two attachments reversed and identifies the Streetscaping Standards as Attachment 3C and Elements of Continuity as Attachment 3B. The correct references are on pages 2-3 and page 44 of the Urban Renewal Plan. They appear in the correct order after this face page.

It is further noted that the Streetscape Standards and the Elements of Continuity have been revised since the approval of the River Myrtle-Old Boise Urban Renewal Plan. For the most current versions of these documents, contact Capital City Development Corporation’s office. (These notes were added in March 2015.)

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ATTACHMENT 3A

2025 DOWNTOWN REDEVELOPMENT PLAN

This document is not completed. CCDC Staff are working on a draft of this plan to bring to the CCDC Board of Commissioners and the City Council in the coming months.

The 2025 Downtown Boise Redevelopment Plan is an aggregation of the planning and urban design principles consistent among the plans for the three districts. It is identified as an attachment to the Urban Renewal Plan. Agency legal counsel has advised us that the Urban Renewal Plan can stand on its own without the 2025 Downtown Boise Redevelopment Plan.

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Appendix B: Streetscape Standards

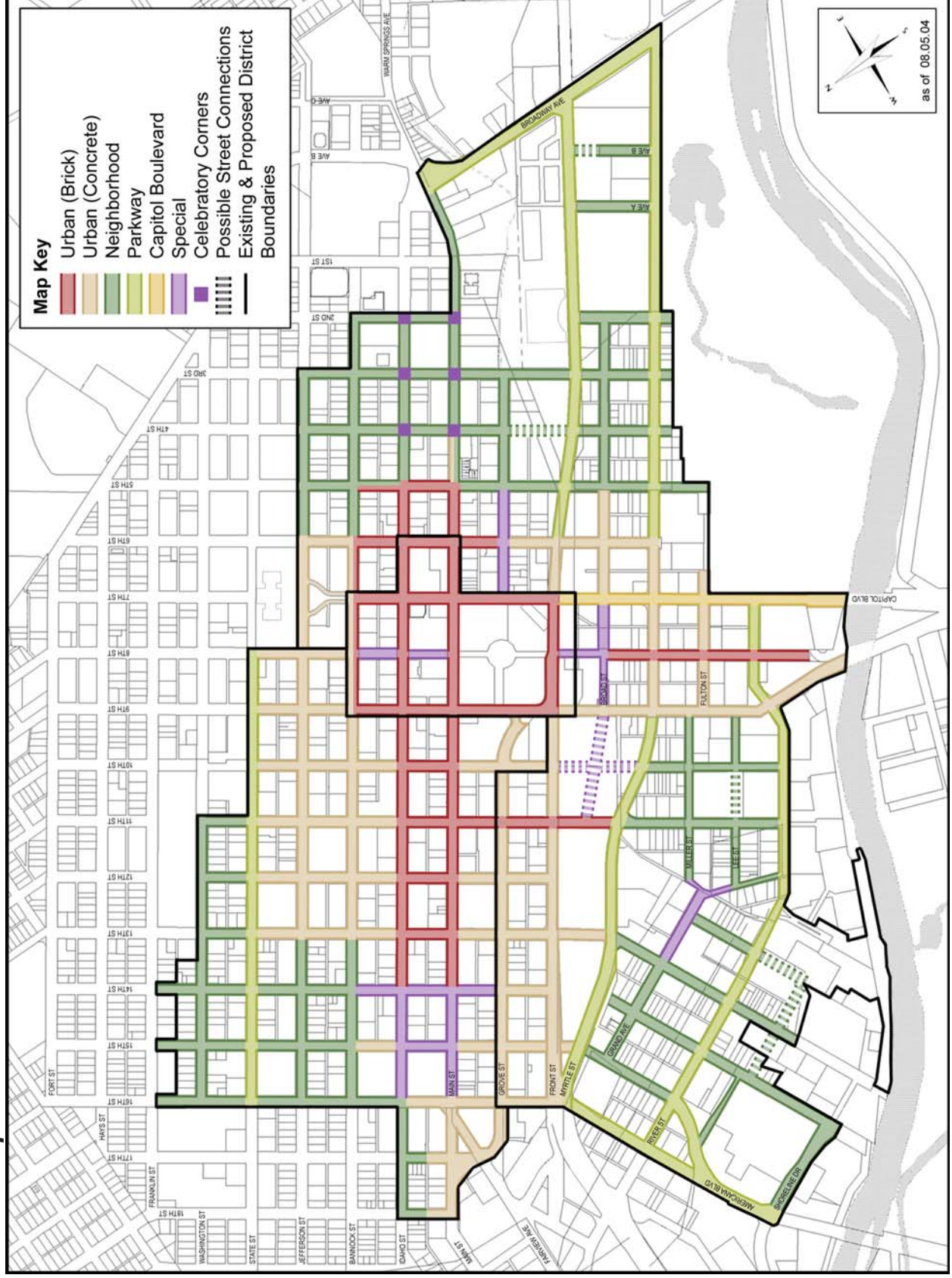


How street rights-of-way are constructed and improved is critical. Streets and sidewalks are key to the livability, vitality and identity of downtown. Well designed streets and sidewalks add value and act as a catalyst to the development of private property. They provide the setting for land use. The capacity of the street and sidewalk to carry car and pedestrian traffic determines, in part, the land uses appropriate for that street. Sidewalk width, street lights and other amenities affect pedestrian activity and aesthetic quality and thus help to determine how adjoining private land is developed and used. The improvements made to streets and sidewalks can be applied deliberately and strategically to influence land use.

The streetscape is that part of the street right-of-way between the face of the curb and the building. Since the mid 1980s much effort has been made to invest in Downtown Boise's streetscapes. These highly visible improvements have been instrumental to the success of the downtown core. The streetscapes have provided a setting for community events, for the development of adjoining private property, and for the everyday interaction of visitors and those who frequent downtown. The brick, street lights, trees and other elements of the streetscape have established an identity for the downtown core.

With nearly twenty years of experience in improving streetscapes in downtown, CCDC has developed standards for several streetscape types. The streetscape standards consist of three components: the streetscape character map, the streetscape standards text, and the streetscape diagrams.	
The streetscape character map provides a specific plan for the improvement of streetscapes within the boundaries of the adopted urban renewal districts. The map determines the particular streetscape type to be applied to each street.	
The arrangement of the streetscape types on the map is consistent with the urban renewal plans and particularly with planned land uses and pedestrian routes. For example, the <i>neighborhood street</i> type applies to those areas where housing is emphasized, and the <i>urban-brick</i> type applies to important pedestrian routes and to the downtown core. Minor exceptions may be appropriate depending on the nature and scope of adjoining development or other conditions.	
The standards for each streetscape type are depicted in the streetscape diagrams and supporting streetscape standards text. A streetscape diagram and text are also provided for the wide urban-brick type. The diagrams and text depict the surface materials, furnishings, and tree class and installation for each street type. The standards for <i>special street</i> are presented as a prototype. The actual roadway and streetscape design will be addressed at the time the specific project is being designed. Participation in the design process by affected agencies and other stakeholders is anticipated. With few exceptions, the standards for other streetscape types are to be followed rigorously with some variation permitted based on local conditions and circumstances.	
The plan for streetscapes will be implemented by improvements tied to the development of adjoining property or by streetscape improvement projects independent of any adjoining development. As noted in the streetscape standards text, some variation in how the standards are applied may be appropriate to account for local conditions.	
Contents:	
Streetscape Character Map	3
Wide Urban Sidewalk – Concrete	4
Narrow Urban Sidewalk – Concrete	6
Urban Sidewalk – Brick	8
Neighborhood Streetscape	10
Urban Parkway	12
Capitol Boulevard Streetscape	14
Special Street	16
Celebratory Corner	18
Transit Stop – Streetscape	20

Streetscape Character



Wide Urban Sidewalk – Concrete

Description: Urban sidewalks are generally located in business and mixed-use districts where there is a concentration of higher intensity development, ground floor retail, restaurants and entertainment uses and on-street parking. They are designed for heavy pedestrian use and to be compatible with retail or similar uses in a dense urban environment. They may also be used with offices and housing at street level when such uses are located in business or mixed-use districts rather than neighborhoods.

Surfaces:

- Concrete sidewalk extends from face of building to curb.
- Total width of wide urban sidewalk is 16' or greater and includes a curb zone between curb and tree grate.
- Concrete in pedestrian zone with parallel and cross scores at 4' intervals. No parallel scores are made within 6" of building foundation. Concrete in Furnishing and curb zone is scored in 2' intervals.

Furnishings:

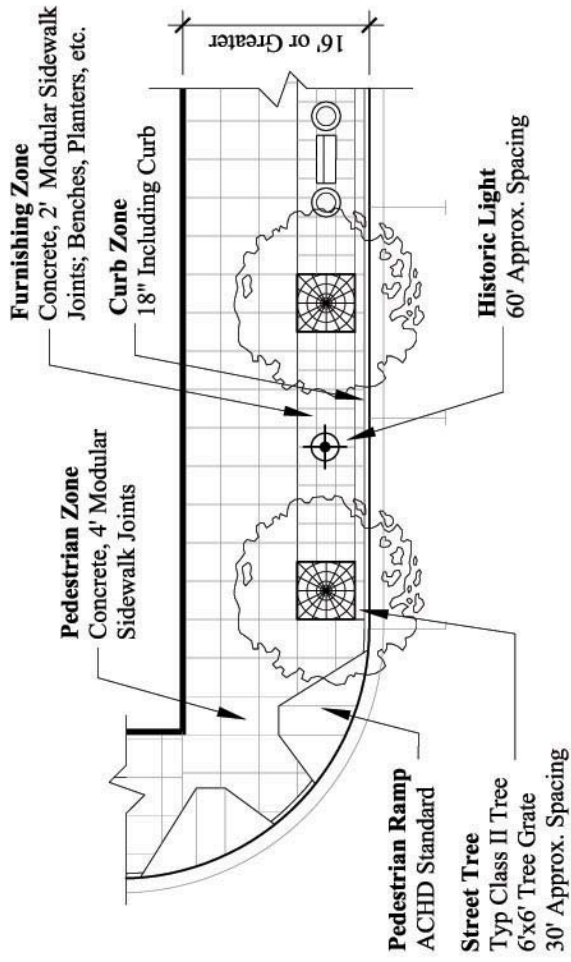
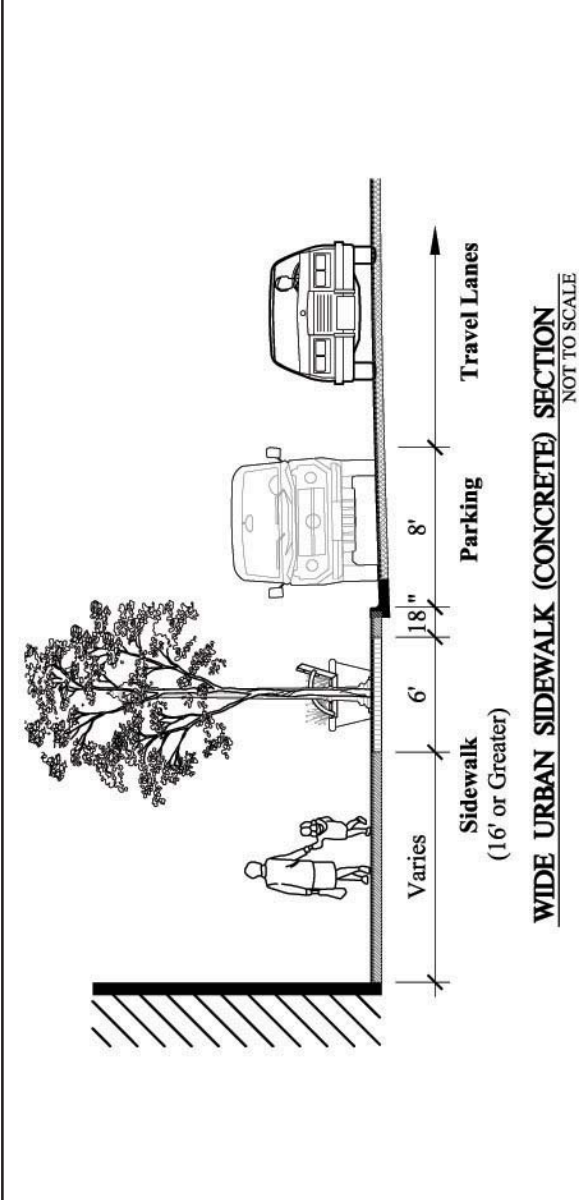
- Historic street lights with approximately 60' separation and in same alignment as trees. Lights should be centered between trees and conflicts with trees should be avoided.
- Benches, trash receptacles, bicycle racks and other furnishings shall conform to "Elements of Continuity".

Street Trees:

- Class II trees in furnishing zone and in same alignment as historic street lights. Trees spaced approximately 30' apart and installed in 6' x 6' tree grate. Where block faces are developed with storefront-style retail, trees should be spaced so they coordinate with the typical storefront width of 25' and fall on the property line between storefronts. The purpose of this standard is to improve visibility of signage on the storefronts while also retaining street trees at a consistent spacing along the block face.
- Tree species may be limited to the Class I type within 15' of overhead power lines.
- Coordinate planning and installation of trees with Boise Community Forestry.

Note:

- Some variation in how these standards are applied may be appropriate to account for local conditions.
- Furnishings and street trees shall meet Ada County Highway District sight distance requirements.
- All furnishings, trees and improvements in the street right-of-way require a license agreement and/or other form of approval by the Ada County Highway District.



WIDE URBAN SIDEWALK (CONCRETE) DETAIL PLAN
SCALE 1"=20' 8/04 JBA

Narrow Urban Sidewalk – Concrete

Description: Urban sidewalks are generally located in business and mixed-use districts where there is a concentration of higher intensity development, ground floor retail, restaurants and entertainment uses and on-street parking. They are designed for heavy pedestrian use and to be compatible with retail or similar uses in a dense urban environment. They may also be used with offices and housing at street level when such uses are located in business or mixed-use districts rather than neighborhoods.

The Narrow Urban Sidewalk is an alternate design to the wide urban sidewalk to be applied in areas where the existing sidewalk, from building to face of curb, is less than 16' and where decreasing the width of the roadway (relocating the curb) is not appropriate or feasible.

Surfaces:

- Concrete sidewalk extends from face of building to curb.
- Concrete in pedestrian zone with parallel and cross scores at 4' intervals. No parallel scores are made within 6" of building foundation. Concrete in Furnishing zone is scored in 2' intervals.
- There is no curb zone under typical narrow concrete sidewalk conditions.
- Total width of the Narrow Urban Sidewalk is less than 16'.

Furnishings:

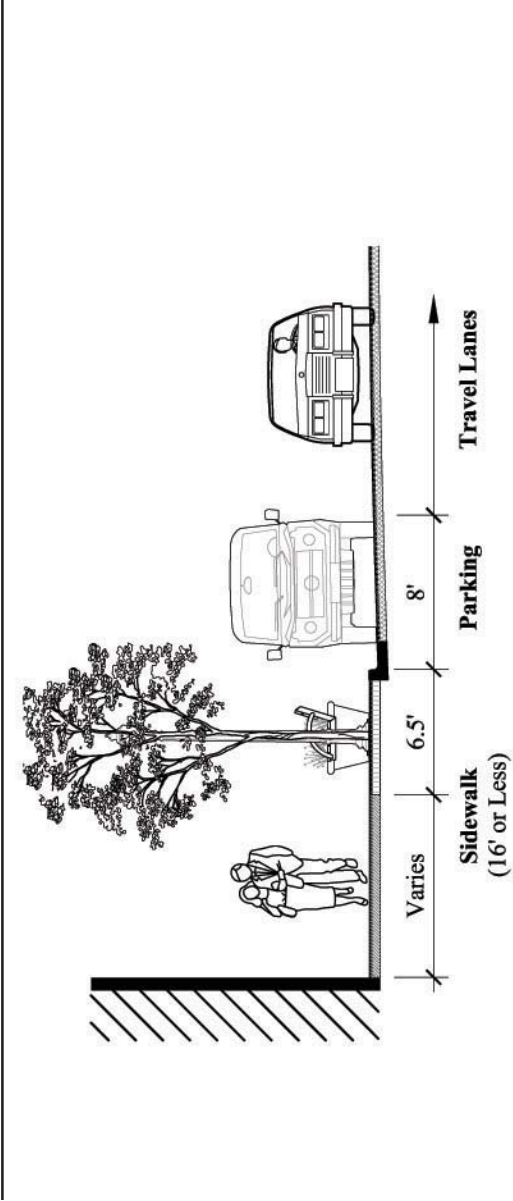
- Historic street lights with approximately 60' separation and in same alignment as trees. Lights should be centered between trees and conflicts with trees should be avoided.
- Benches, trash receptacles, bicycle racks and other furnishings shall conform to "Elements of Continuity".

Street Trees:

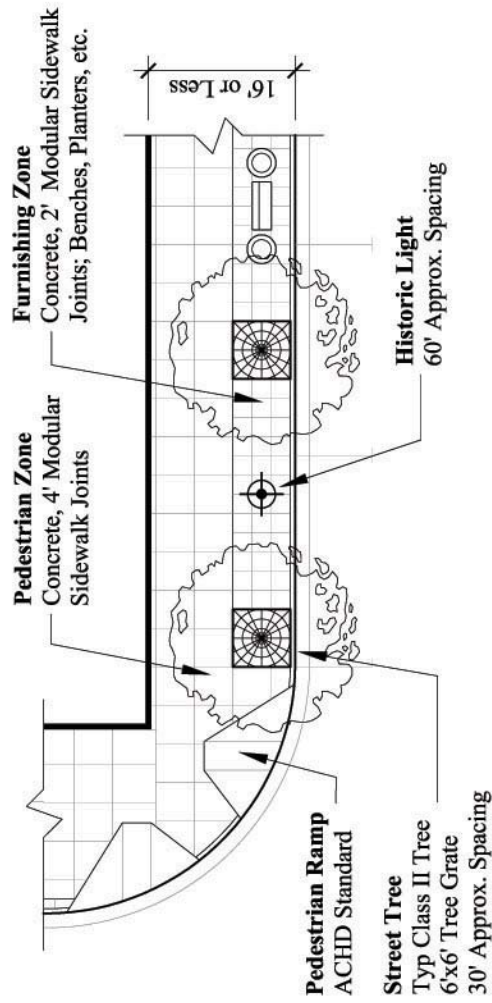
- Class II trees in furnishing zone and in same alignment as historic streetlights. Trees spaced approximately 30' apart and installed in 6' x 6' tree grate. 4' x 8' tree wells and grates shall be used where necessary to maintain a minimum 8' pedestrian zone. Where block faces are developed with storefront-style retail, trees should be spaced so they coordinate with the typical storefront width of 25' and fall on the property line between storefronts. The purpose of this standard is to improve visibility of signage on the storefronts while also retaining street trees at a consistent spacing along the block face.
- Tree species may be limited to the Class I type within 15' of overhead power lines.
- Coordinate planning and installation of trees with Boise Community Forestry.

Note:

- Some variation in how these standards are applied may be appropriate to account for local conditions.
- Furnishings and street trees shall meet Ada County Highway District sight distance requirements.
- All furnishings, trees and improvements in the street right-of-way require a license agreement and/or other form of approval by the Ada County Highway District.



NARROW URBAN SIDEWALK SECTION
NOT TO SCALE



NARROW URBAN SIDEWALK DETAIL PLAN
SCALE 1"=20'
8/04 JBA

Urban Sidewalk – Brick

Description: Urban sidewalks are generally located in business and mixed-use districts where there is a concentration of higher intensity development, ground floor retail, restaurants and entertainment uses and on-street parking. They are designed for heavy pedestrian use and to be compatible with retail or similar uses in a dense urban environment. They may also be used with offices and housing at street level when such uses are located in business or mixed-use districts rather than neighborhoods.

The Urban Sidewalk – Brick is used to designate certain streets as focused on pedestrians and to create a visible network of comfortable people-oriented streets where significant attention is given to the aesthetic quality of the pedestrian environment. Brick is required on designated street rights-of-way in high activity centers such as the downtown core to create distinctive places for people to gather, live, work, shop, dine and socialize, where extra emphasis is desired to signify the importance of these focal points of people activity.

Surfaces:

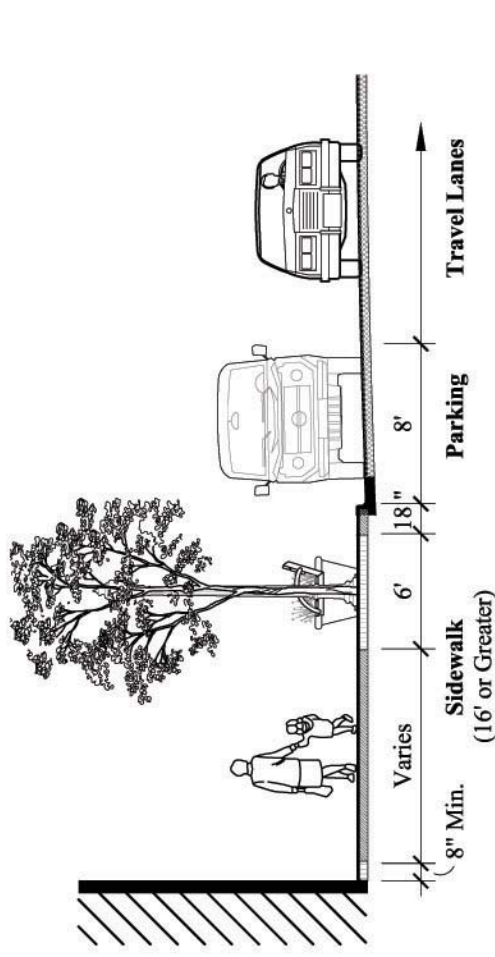
- Dimensions of pedestrian zone and furnishing zone are same as those for the urban sidewalk with concrete.
- Dry laid brick paver surface extends from curb to face of building
- Pattern of brick varies by building frontage zone, pedestrian zone, furnishing zone, and curb zone.
- Building Frontage Zone: Minimum 8" wide row of dark brick dry laid pavers in soldier course pattern. Width varies and is determined by the irregularity of the building face.
- Pedestrian Zone: Minimum 8' wide section of red brick dry laid pavers in herringbone pattern.
- Furnishing Zone: Width of zone corresponds with width of tree well and grate but generally 6'. Dark brick dry laid pavers with single soldier course each side with herringbone field.
- Curbside Zone: 1 ½ soldier courses of red dry laid brick where applicable.
- Pedestrian ramps at crosswalk are typically concrete.

Furnishings:

- Furnishings for brick sidewalk same as concrete sidewalk; however, the extent to which furnishings are applied will generally increase with the amount of pedestrian activity.
- Historic street lights with approximately 60' separation and in same alignment as trees. Lights should be centered between trees and conflicts with trees should be avoided.
- Benches, trash receptacles, bicycle racks and other furnishings shall conform to "Elements of Continuity".

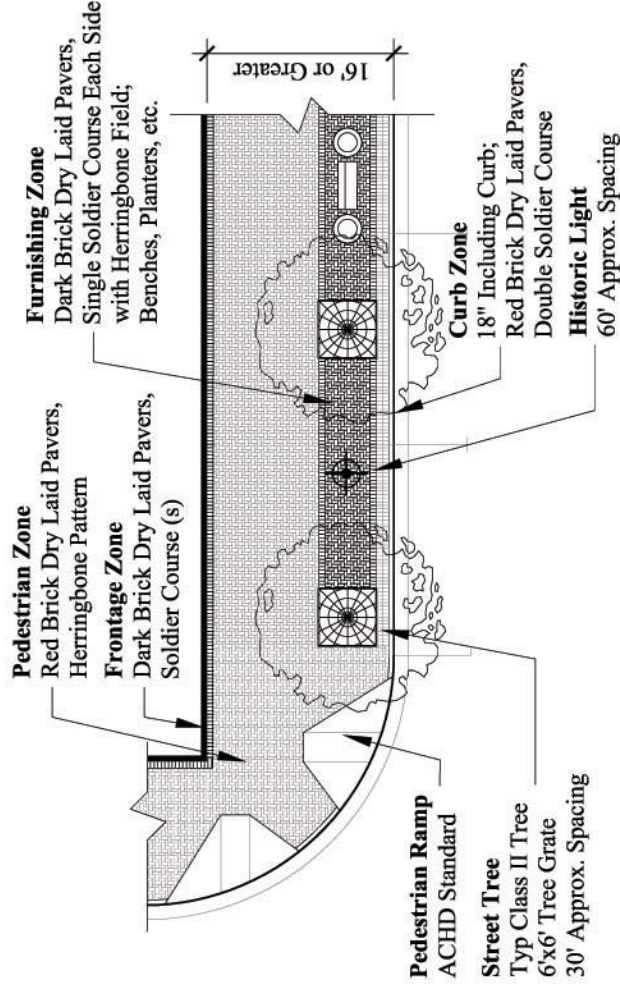
Street Trees:

- Application of trees is same as it is for the Urban Sidewalk Concrete type.
- Class II trees in furnishing zone and in same alignment as historic street lights. Trees spaced approximately 30' apart and installed in 6' x 6' tree grate. Where block faces are developed with storefront-style retail, trees should be spaced so they coordinate with the typical storefront width of 25' and fall on the property line between storefronts. The purpose of this standard is to improve visibility of signage on the storefronts while also retaining street trees at a consistent spacing along the block.



WIDE URBAN SIDEWALK (BRICK) SECTION

NOT TO SCALE



WIDE URBAN SIDEWALK (BRICK) DETAIL PLAN

SCALE 1"=20'

8/04 JBA

- Tree species may be limited to the Class I type within 15' of overhead power lines.
- Coordinate planning and installation of trees with Boise Community Forestry.

Note:

- The urban sidewalk with brick, like its concrete counterpart, may be applied in both wide and narrow sidewalk conditions. Variations in the dimensions of the pedestrian zone, furnishing zone, tree wells and grates may apply.
- Furnishings and street trees shall meet Ada County Highway District sight distance requirements.
- All furnishings, trees and improvements in the street right-of-way require a license agreement and/or other form of approval by the Ada County Highway District.

Neighborhood Streetscape

Description: The Neighborhood Streetscape is used in areas of downtown that have less intense development with a higher proportion of residential uses and more limited retail and office uses. It is designed to accommodate substantial pedestrian traffic in locations where a residential neighborhood character is desired. The distinguishing characteristic of this style of streetscaping is the use of tree lawns between the sidewalk and the street, rather than continuing the sidewalk surface to the curb. The intent of the neighborhood streetscape is to create a pleasant pedestrian environment and encourage walking as an alternative to using an automobile. The Neighborhood Streetscape is compatible with housing, office and limited retail uses.

Surfaces:

- 6' concrete sidewalk separated from curb and roadway by a lawn strip.
- The width of the lawn strip will vary according to local conditions but is generally 8' to 12'. A minimum width of 6' is generally required for street trees.
- The lawn strip consists of turf; topsoil augmentation may be needed for new turf where it has not recently existed.
- Modular scored concrete and/or an 18" concrete curbside walk may be required depending on local conditions and placemaking efforts. The purpose of the curbside walk is to protect the lawn strip and provide a more sustainable surface in areas of high pedestrian traffic or high turnover of on-street parking spaces.
- Surface treatment inside of the sidewalk will vary depending on building setbacks, use, and other conditions.

Furnishings:

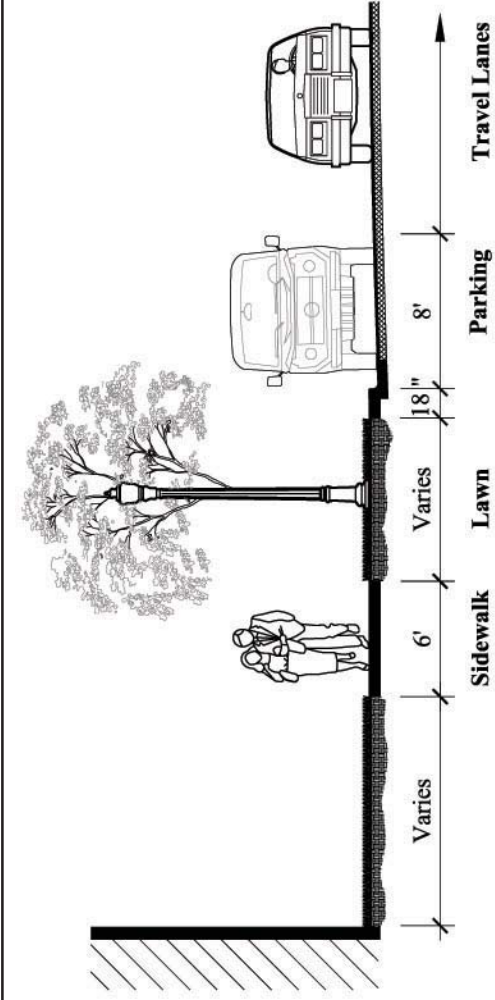
- Historic street lights approximately 4 per block and in same alignment as trees. Lights should be centered between trees and conflicts with trees should be avoided. Under most conditions, lights are to be aligned in the center of the lawn strip.

Street Trees:

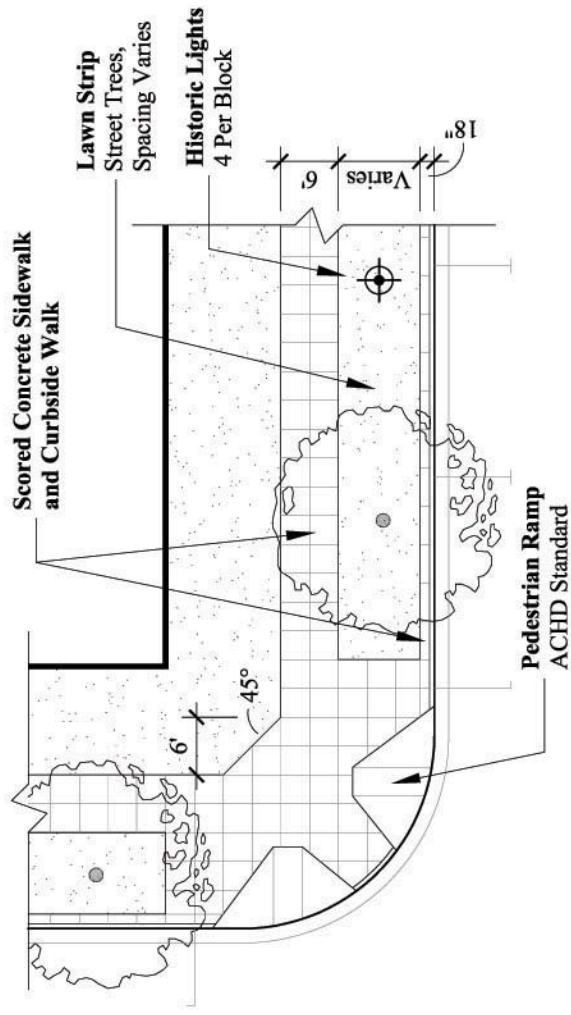
- Class II trees in furnishing zone and in same alignment as historic streetlights. Where the lawn strip at least 10 feet in width, Class III trees may be recommended or required depending on land use and other local conditions. Tree spacing depends on species and size at maturity.
- Tree species may be limited to the class I type within 15' of overhead power lines.
- Coordinate planning and installation of trees with Boise Community Forestry.

Note:

- Some variation in how these standards are applied may be appropriate to account for local conditions.
- All furnishings, trees and improvements in the street right-of-way require a license agreement and/or other form of approval by the Ada County Highway District.



NEIGHBORHOOD STREET SECTION
NOT TO SCALE



NEIGHBORHOOD STREET DETAIL PLAN
SCALE 1"=20'

8/04 JBA

Urban Parkway

Description: The Urban Parkway treatment is used on major streets that carry higher volumes of traffic at faster speeds than is typical for the downtown core. The intent of using a parkway treatment is to provide beautification of significant streets such as State Street and the Connector and to also create a comfortable pedestrian walking environment so the streets serve not only vehicles but also people. The standards are established to provide a strong visual statement, a double rather than single row of street trees, adequate room for pedestrian movement, and a buffer between pedestrians and relatively heavy auto and truck traffic.

Surfaces:

- 8' lawn strip between sidewalk and curb.
- 8' concrete sidewalk with scores or joints.
- Surface width behind sidewalk should be sufficient to accommodate a row of trees. Turf or low shrubs are appropriate but design and materials may vary according to setback standards and conditions related to site, building and use.

Furnishings:

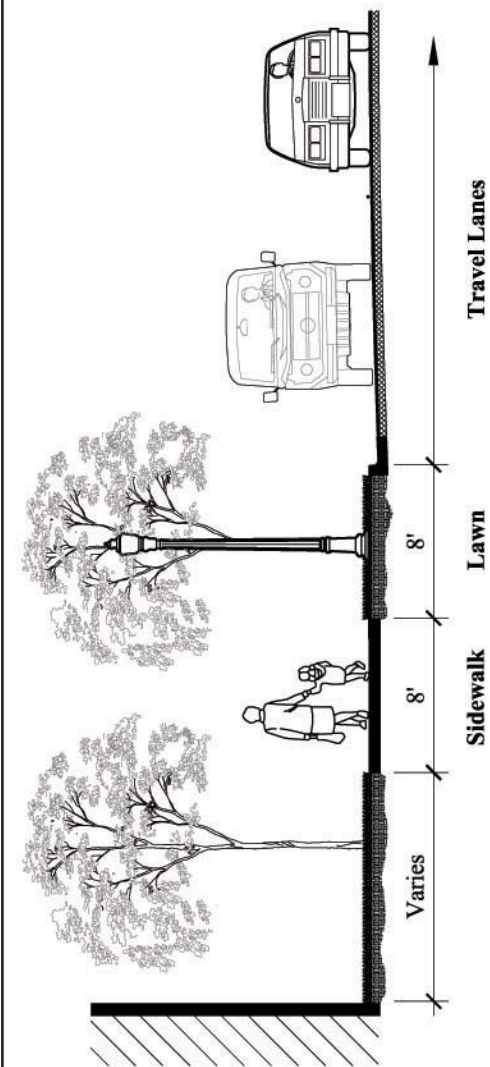
- Historic street lights with approximately 90' separation, located in lawn strip and aligned with trees 3' from sidewalk and 5' from curb.
- Other furnishings generally limited to trash receptacles and news boxes near corners. Benches and other furnishings may be appropriate depending on local conditions.

Street Trees:

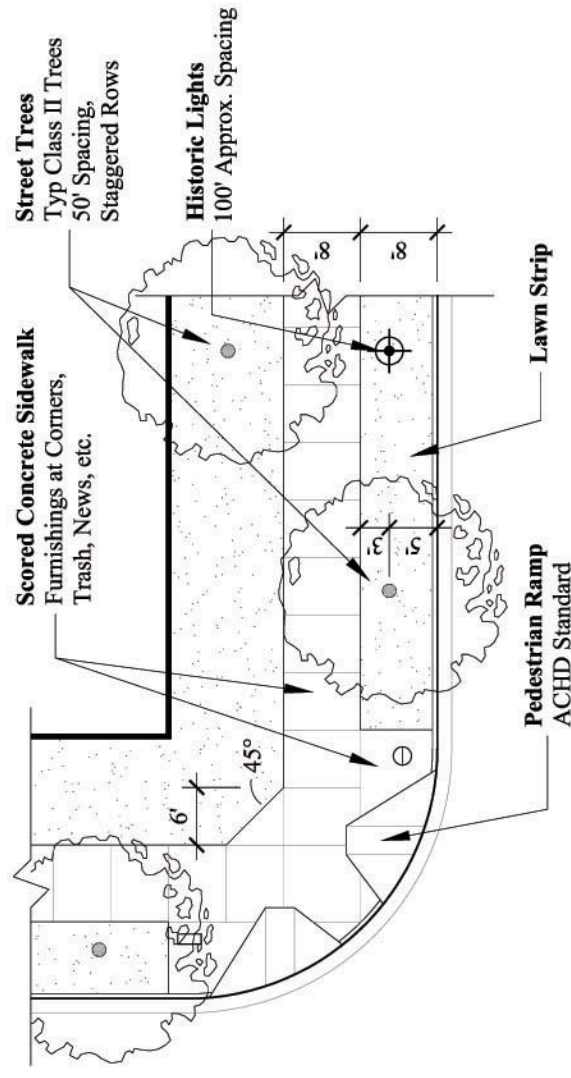
- Class II trees in lawn strip between sidewalk and curb spaced approximately 45' apart. Where the lawn strip at least 10 feet in width, consider the use of Class III trees in consultation with Boise Community Forestry. Trees in tree lawn are aligned to be closer to sidewalk than curb to prevent conflicts with vehicular traffic. Same species of tree behind sidewalk, also spaced approximately 45' apart. Tree species may be limited to the Class I type within 15' of overhead power lines.
- Coordinate planning and installation of trees with Boise Community Forestry.

Note:

- Furnishings and street trees shall meet Ada County Highway District sight distance requirements.
- Some variation in how these standards are applied may be appropriate to account for local conditions.
- All furnishings, trees and improvements in the street right-of-way require a license agreement and/or other form of approval by the Ada County Highway District.



URBAN PARKWAY SECTION
NOT TO SCALE



URBAN PARKWAY DETAIL PLAN
SCALE 1"=20'

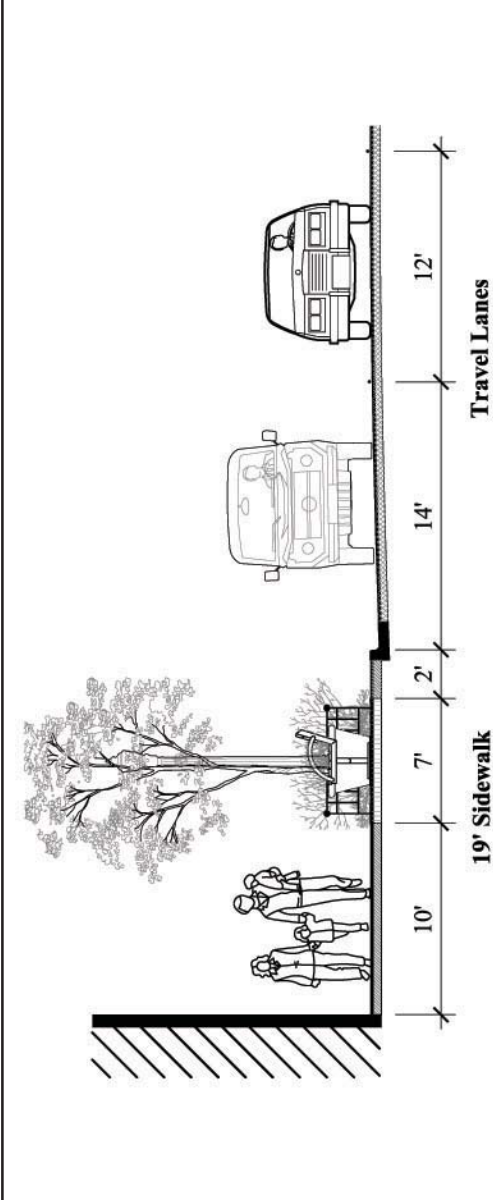
8/04 JBA

Capitol Boulevard Streetscape

Description:

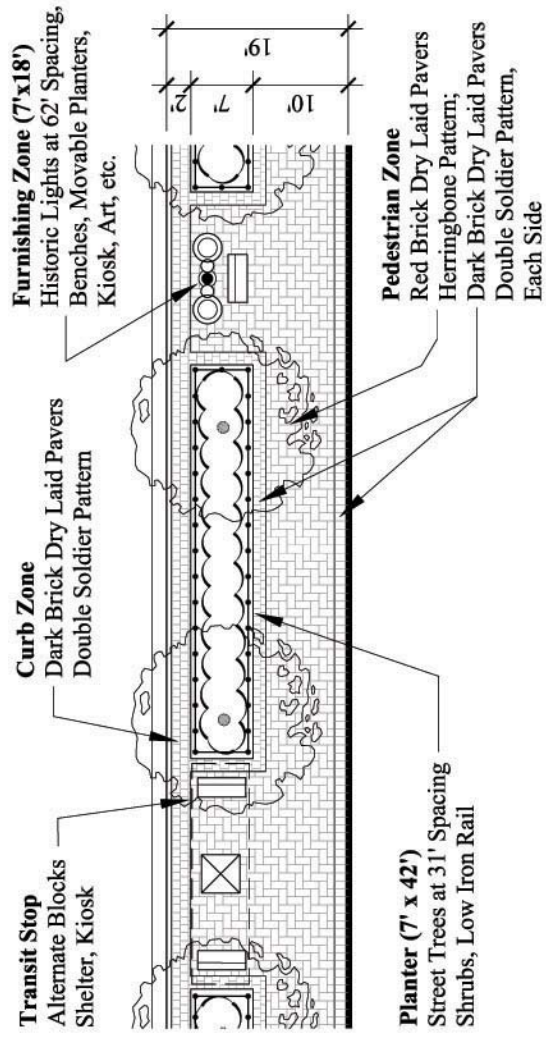
The Capitol Boulevard designation applies to that part of Capitol Boulevard within the Central Corridor section of the *Capitol Boulevard Special Design District* as described in section 11-07-09 of the Boise City Code which provides standards for the special district. The standards in the streetscape diagram are consistent with Capitol Boulevard's function as the gateway to Downtown Boise and with the purpose of the Capitol Boulevard Special Design District, which is in part to transform Capitol Boulevard into a true boulevard, to encourage pedestrian and bicycle use of the boulevard, and to strengthen the visual tie between the Boise Depot and the State Capitol.

While these street standards are being prepared, Boise City is considering modifications and refinements to the standards in section 11-07-09 of the code for Capitol Boulevard. The corresponding streetscape diagram represents the proposal being considered by Boise City at the time these standards are being prepared. The streetscape diagram will be modified as necessary to reflect the standards adopted by Boise City. In either case, improvements to Capitol Boulevard will be subject to the standards in section 11-07-09 of the Boise City Code.



CAPITOL BOULEVARD SECTION

NOT TO SCALE



CAPITOL BOULEVARD DETAIL PLAN

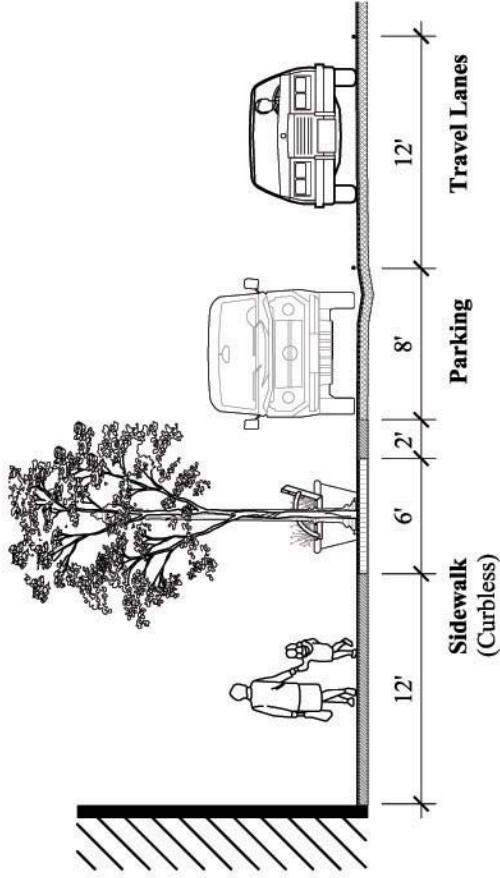
SCALE 1"=20'

8/04 JBA

Special Street

Description: The Special Street designation is used for particular street segments where an extraordinary pedestrian emphasis and placemaking effort is planned and warranted. A special street may function as both a street and also as a public space for sidewalk cafes, street fairs, festivals, cultural and special events. The best examples in downtown Boise are 8th Street between Bannock and Main and the Basque Block on Grove between Capitol Boulevard and 6th Street. It may be open or closed to vehicular traffic, or may be designed to allow temporary traffic closures. Each special street is expected to have a unique design often celebrating a particular theme.

The design standard presented here is a prototype. The actual design will vary and is expected to be determined at the time a specific project is planned and constructed. All furnishings, trees and improvements in the street right-of-way require a license agreement and/or other form of approval by the Ada County Highway District.



SPECIAL STREET SECTION

NOT TO SCALE

Pedestrian Zone

Material Varies

Furnishing Zone

Material Varies;

Benches, Planters, etc.

Urban Streetscape

Street Trees

6'x6' tree Grate

30' Approx. Spacing

Historic Lights

60' Approx. Spacing

Planting Bed

Typ each corner

Tree species may vary

from trees in grates

Special Intersection Paving

Parking and Valley Gutter

Curbless Sidewalk

SPECIAL STREET DETAIL PLAN

SCALE 1"=20'

8/04 JBA

Celebratory Corners

Description:

Celebratory Corners are designated on those sections of Main and Idaho where there is a desire to extend the character of the Central Business District and to denote the east ends of these streets as gateways to Downtown Boise. Because the treatment applies only to street corners and roadway intersections, the use of brick and other enhancements can be applied to provide visual cues without loss of the lawn strip and a degeneration of the residential character that would result. Because the treatment is limited to street corners and the roadway, it is expected that the improvements will be made as part of a capital project. Most private redevelopment projects are not likely to be of a size and nature to warrant Celebratory Corner improvements as conditions of project approval.

Surfaces:

- Dry laid brick pavers in herringbone pattern with concrete ramps behind curb.
- Roadway surfaces will be determined in cooperation with the Ada County Highway District.

Furnishings:

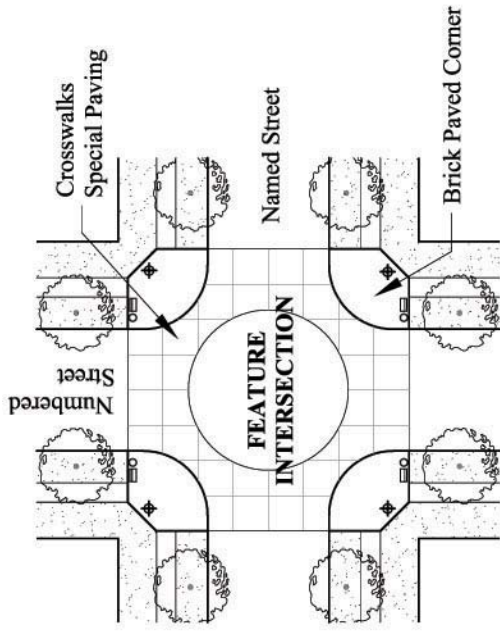
- Benches and trash receptacles as shown; may not be appropriate for all conditions.
- Historic street light behind where sidewalk would intersect under other circumstances; placement shown in streetscape diagram.

Street Trees:

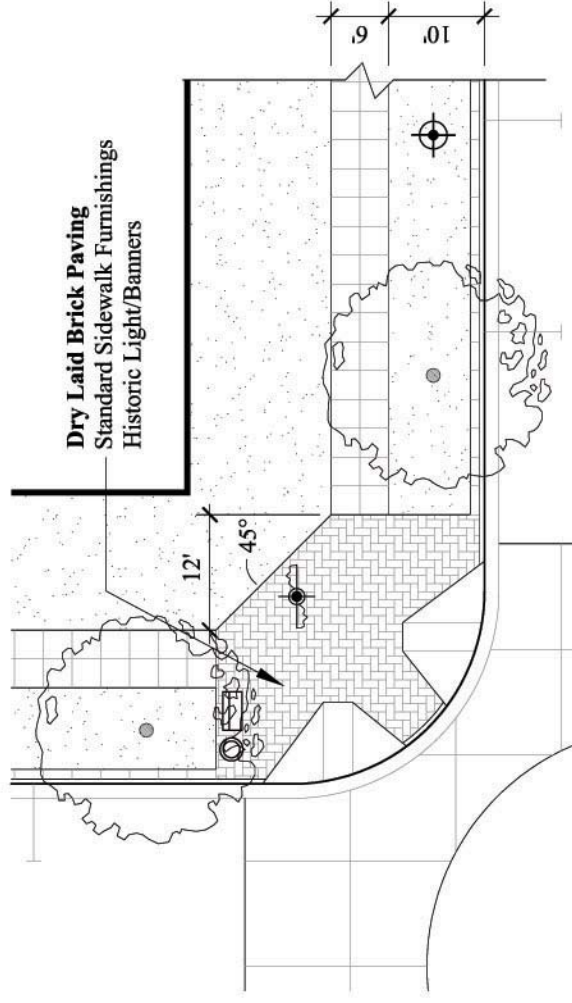
- Street trees are existing or part of other, adjacent streetscape improvements; no additional street trees are required with Celebratory Corner improvements.

Note:

- Some variation in how these standards are applied may be appropriate to account for local conditions.
- Furnishings and street trees shall meet Ada County Highway District sight distance requirements.



CELEBRATORY CORNER INTERSECTION PLAN
NOT TO SCALE



CELEBRATORY CORNER DETAIL PLAN
SCALE 1"=20'

8/04 JBA

Transit Stop – Streetscape

Description: Transit streets are those where transit service is frequent and transit ridership is high, resulting in frequent transit stops and the need for amenities for transit passengers. This standard describes the streetscaping improvements required at transit stops. The standard applies to the following street types: urban parkways, wide or narrow urban sidewalks—both concrete and brick—special streets, and Capitol Boulevard. This standard applies to that portion of the block face where the transit stop is sited. The type of surface used will depend on the type of surface generally required for the type of street on which the stop is located. For example, on urban streets improved with brick sidewalks, the surface treatment would be brick. On wide and narrow urban streets – concrete, the surface treatment would be concrete.

Surfaces:

- Surfaces are dictated by the applicable street type.

Transit Stop Location:

- Typically transit stops will be sited just past the intersection so the transit vehicle is able to clear the intersection and then come to a stop at the curb.
- Adjustments may be needed where the transit stop is located in congested areas, or where there are sidewalk cafes and other furnishings that create a cramped situation for placing a transit stop. Furnishings may need to be rearranged to accommodate a transit stop.

Furnishings:

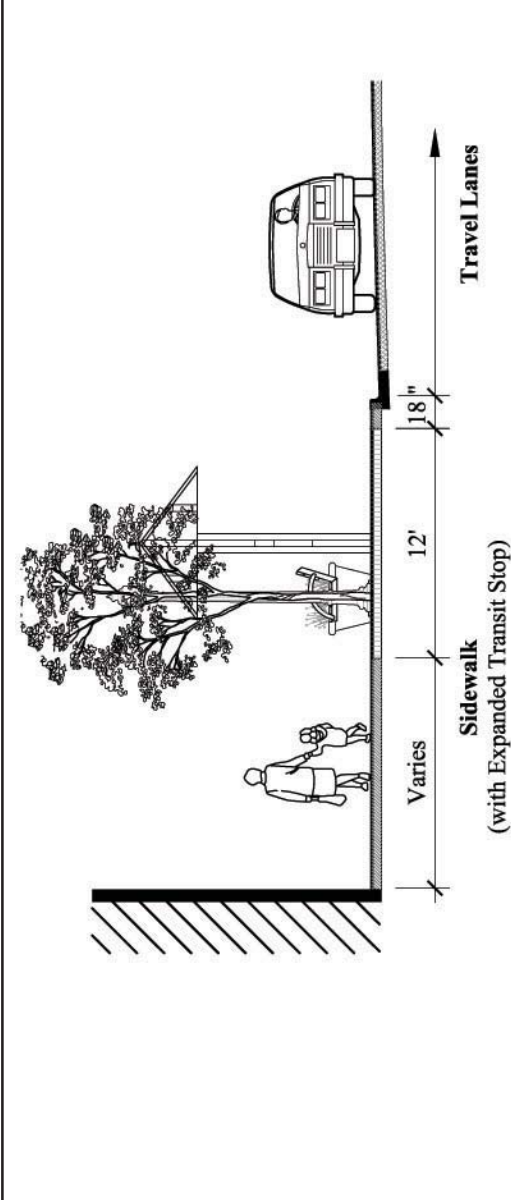
- Transit shelter is located in the furnishing zone. Refer to the standards for the applicable street type for other furnishings.
- Attention should be given to the existence of sidewalk cafes and other improvements such as benches and bike racks that may cramp the amount of room available for placing a transit shelter and for passengers and other pedestrians to use the sidewalk around the transit stop. Furnishings may need to be rearranged to accommodate a transit stop.
- The transit shelter shall be installed to be compatible with streetlights.
- When a transit stop is in front of storefront retail, the design of the transit shelter should minimize the degree to which it obscures the view of the storefronts and store signage from the street.

Street Trees:

- Refer to the standards for the applicable street type for appropriate street tree application.
- The transit shelter shall be installed to be compatible with street trees.

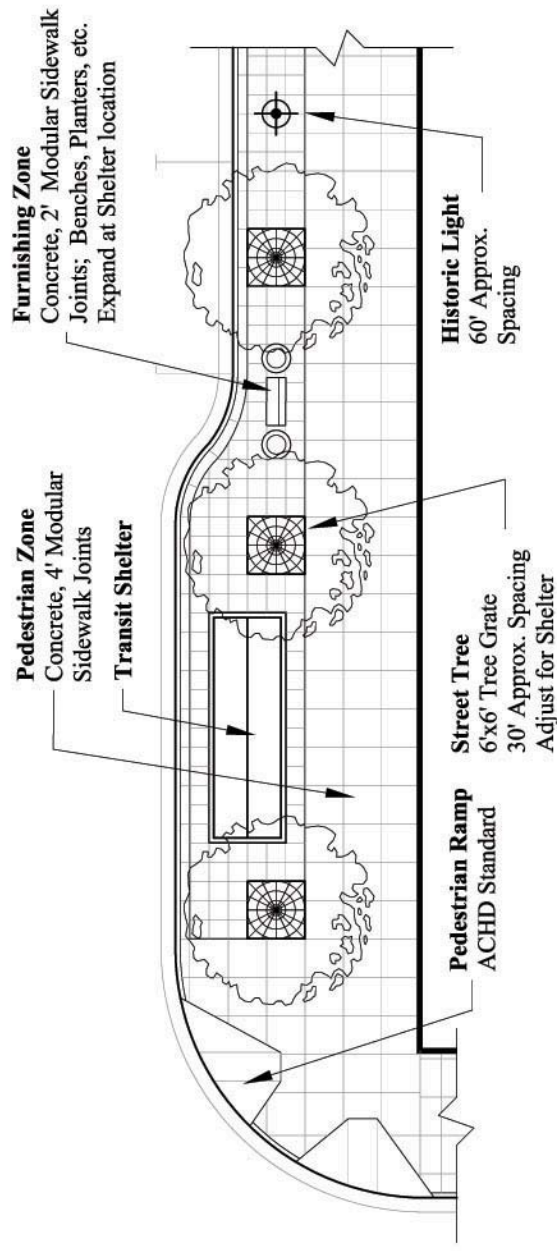
Note:

- Some variation in how these standards are applied may be appropriate to account for local conditions.
- All furnishings, trees and improvements in the street right-of-way require a license agreement and/or other form of approval by the Ada County Highway District.



TRANSIT STOP SECTION

NOT TO SCALE



TRANSIT STOP DETAIL PLAN

SCALE 1"=20'

8/04 JBA

Appendix A: Elements of Continuity



Downtown Boise Elements of Continuity

Prepared for
Capital City Development Corporation
(Boise Redevelopment Agency)
1988

Adopted as part of the
2025 Downtown Boise Redevelopment Plan
CCDC Resolution #1002

Hummel La Marche Hunsucker
Zimmer Gunsul Frasca Partnership
Don Miles Associates/Project for Public Spaces
Barton-Aschman Associates, Inc.
CH2M Hill
Jensen-Belts Associates

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Background:

The Boise Redevelopment Agency (B.R.A.) adopted the Boise Urban Design Plan in April 1986. At the time, the plan applied to a portion of the original Central Urban Renewal District created by the Boise City Council in 1965. This plan recommended the creation of a major civic space as a focal point for the community, and sidewalk improvements within and bordering the eight-block area bounded by Capitol and Bannock, 9th and Front streets. These improvements were intended to make downtown into a distinctive place marked by a high level of design quality and an attractive, people-oriented ambience. The Grove Plaza, located on the four blocks between Capitol Boulevard and Main, Front and 9th Streets, was built in 1987. The central fountain has become a downtown landmark and a delight to both adults and children especially on hot summer days. It has proven to be an exceptional space for community gatherings and special events such as Alive After Five, the Capital City Public Market and City Arts Celebration as well as a place for eating lunch or pausing for quiet reflection during weekdays.

The peripheral streets project led to installation of brick sidewalks, street trees, historic street lights, planters, benches and other street furniture being installed in the eight-block area, streets reconstructed and utilities being placed underground. Eighth Street between Bannock and Main streets was redesigned—the street itself was narrowed and the sidewalks widened. These sidewalks are now lined with restaurants with outdoor dining and the café district is alive with people activity every evening.

The original purpose of this document was to provide design details for how public spaces in the Central Urban Renewal District were to be improved to insure a cohesive identity. It addresses lighting, paving, landscaping, street furniture such as benches, planters and trash containers, graphics and other pedestrian amenities. These elements are referred to as elements of continuity. Since adoption of the Boise Urban Design Plan in 1986, the Boise City Council has approved two additional urban renewal districts: River Street-Myrtle Street and Westside Downtown. The scope of this document has been broadened so it applies to public improvements in these areas as well.

These guidelines are used by Capital City Development Corporation, as the B.R.A. is now known, to assist the agency, developers, architects and landscape architects in the design of projects by providing specific standards for the public improvements that form the elements of continuity.

The elements of continuity allow for other design solutions for public improvements with approval of the agency. Any change must be compatible with the master plan for the district in which the improvements are located, applicable planning and zoning regulations, and the regulations of the Ada County Highway District when the improvements are being installed in street rights-of-way.

Objectives

The objectives of the Elements of Continuity for public improvements are stated below:

- To provide unifying elements within and along the public spaces.
- To emphasize a streetscape, rather than park-like character.
- To create a safe, comfortable, lively, inviting and attractive place for pedestrians.
- To achieve an identity and an image as a special place for downtown Boise and to provide visual continuity and harmony.
- To reflect the qualities of a truly urban environment with intensity, sophistication, diversity and timelessness.
- To reinforce and stimulate high quality adjacent development and to complement retail uses.
- To reflect the unique characteristics of Boise and its environmental context.
- To accommodate uses such as transit, civic events, outdoor commercial uses, and passive recreation.
- To respond to the special needs of the handicapped.
- To create a flexible system allowing for a variety of applications and modifications over time, and low maintenance.

General Conditions

The Elements of Continuity are described and illustrated in general terms in this document. Detailed specifications, dimensions, and color selections are contained in other documents, available from Capital City Development Corporation, which should be consulted prior to specifying materials and design features.

All metal surfaces of the elements described in this document should be painted “Friars Hood” green (RAL 6009) as manufactured by the Ameritone Paint Corporation, Long Beach, California.

1. PAVING

Two types of paving should be provided; I – shaped unity clay pavers, and rectangular unit clay pavers manufactured by Endicott Clay Products Co., Fairbury, Nebraska, or as approved

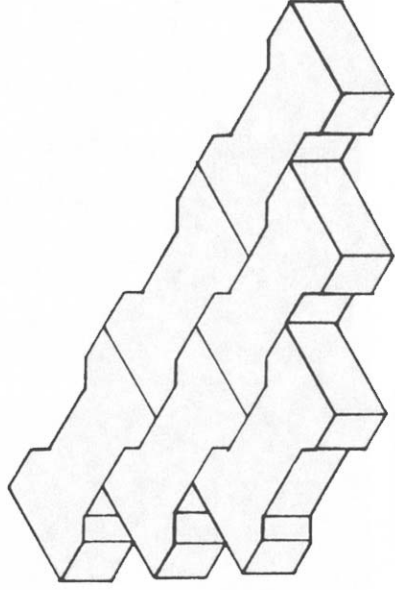
These pavers are used in a variety of ways to define various functional areas of the Major Public Open Space (MPOS) and street improvements. These areas include the following:

Paving abutting building frontages,

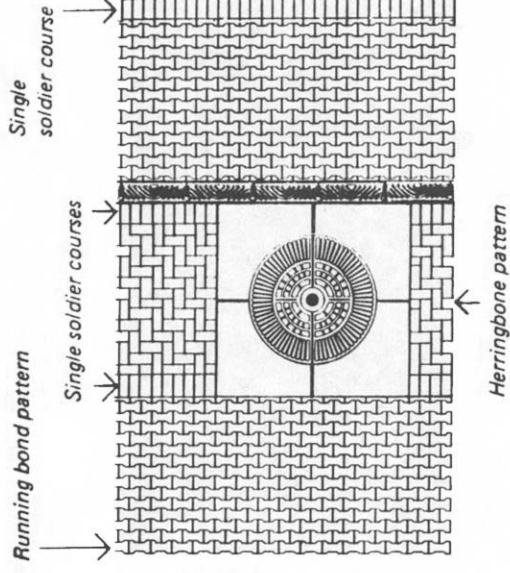
Paving within pedestrian furnishings zones,

Paving at accent borders and inscribed pavers in the MPOS.

In most cases, the unity pavers are sand-set to facilitate their removal for utility work and in the MPOS for inscription.



Running bond, interlocking paver, detail



Paving, plan

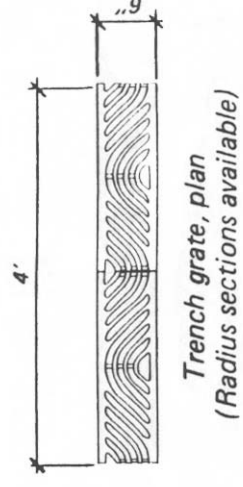
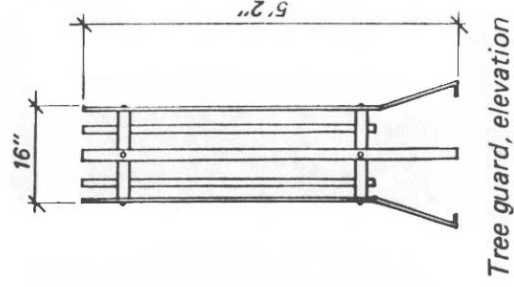
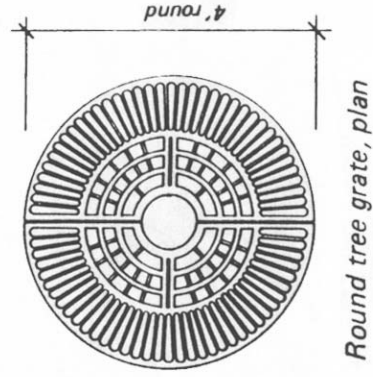
2. TREES AND GRATES

Street trees are selected to provide visual continuity by using the same size, type, and variety.

Tree grates should be “Standard Flat”, four foot round, cast iron tree grates, as manufactured by Urban Accessories, Snohomish, Washington, or as approved

The use of tree guards should be limited to areas where pedestrian activity might result in leaning on the trees. Tree guards, if used, should be the “Eco Guard”, as manufactured by Urban Accessories, Snohomish, Washington, or as approved.

Trench grates may be used under certain conditions. These grates should be cast iron in the “Wave” pattern, as manufactured by Urban Accessories, Snohomish, Washington, or as approved.



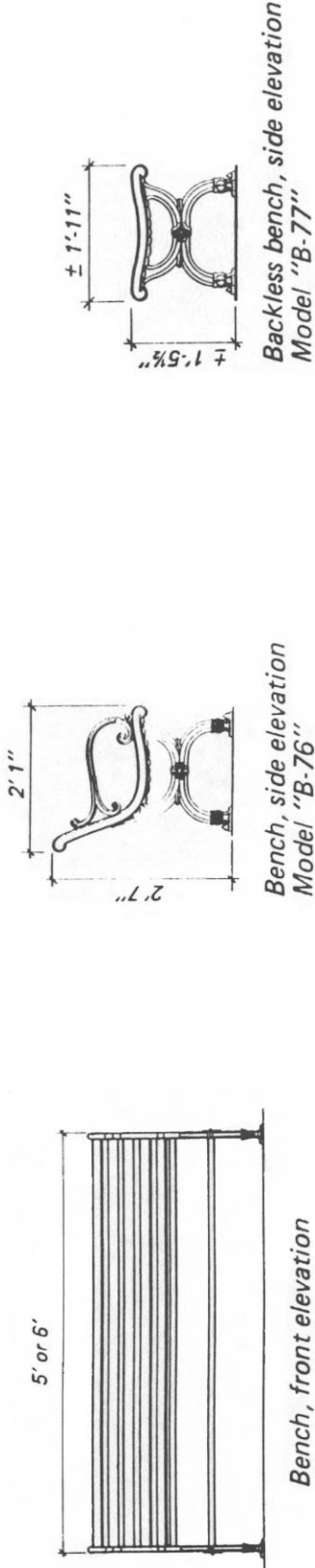
3. TRANSIT SHELTERS

(To be designed)

4. BENCHES

Benches are used in various configurations in the pedestrian furnishing zone. The benches should be cast iron with wooden slats in five or six foot lengths, models “B-76” and “B-77”, as manufactured by Bench Manufacturing Company, Boston, Massachusetts, or as approved.

The bench may be used with or without a back rest, as designated by the design drawings.



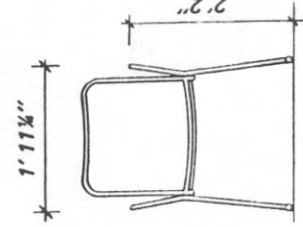
5. MOVABLE FURNITURE

Movable furniture should be used in the MPOS and for sidewalk café seating.

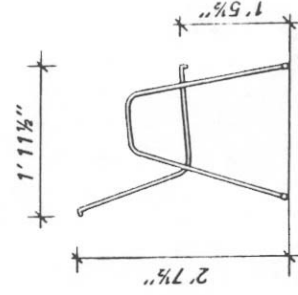
Movable furniture consists of chairs, tables, and umbrellas. These three elements may be used in various combinations, or chairs may be used singly. Chairs are stackable for storage.

Movable chairs should be “Rio Lux” and table “No. 406”, in white, as manufactured by EMU, in Italy (EMU, 2318 Fraver Drive, Reading, PA. 19605), or as approved.

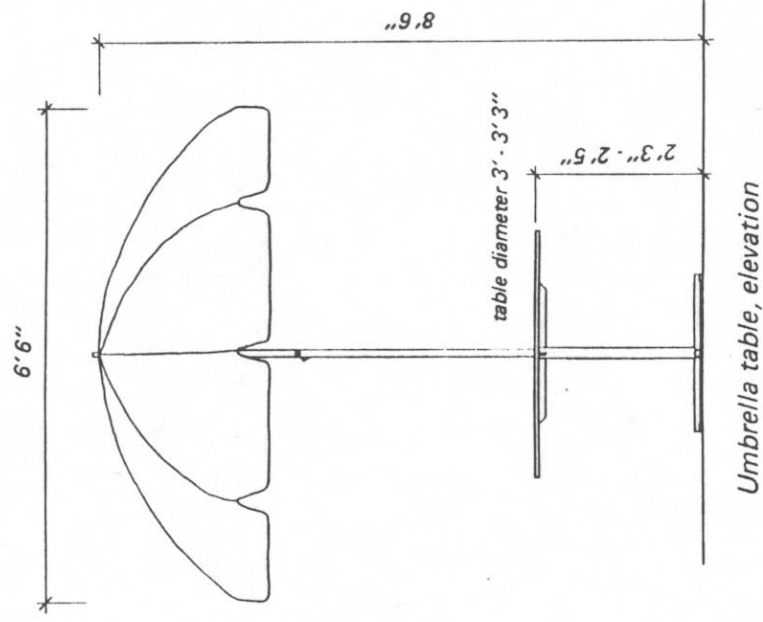
Umbrellas should be “Sail 1” with ultraviolet ray-resistant fabric, as manufactured by Kroin, Cambridge, MA., or as approved.



Movable chair, front elevation



Movable chair, side elevation



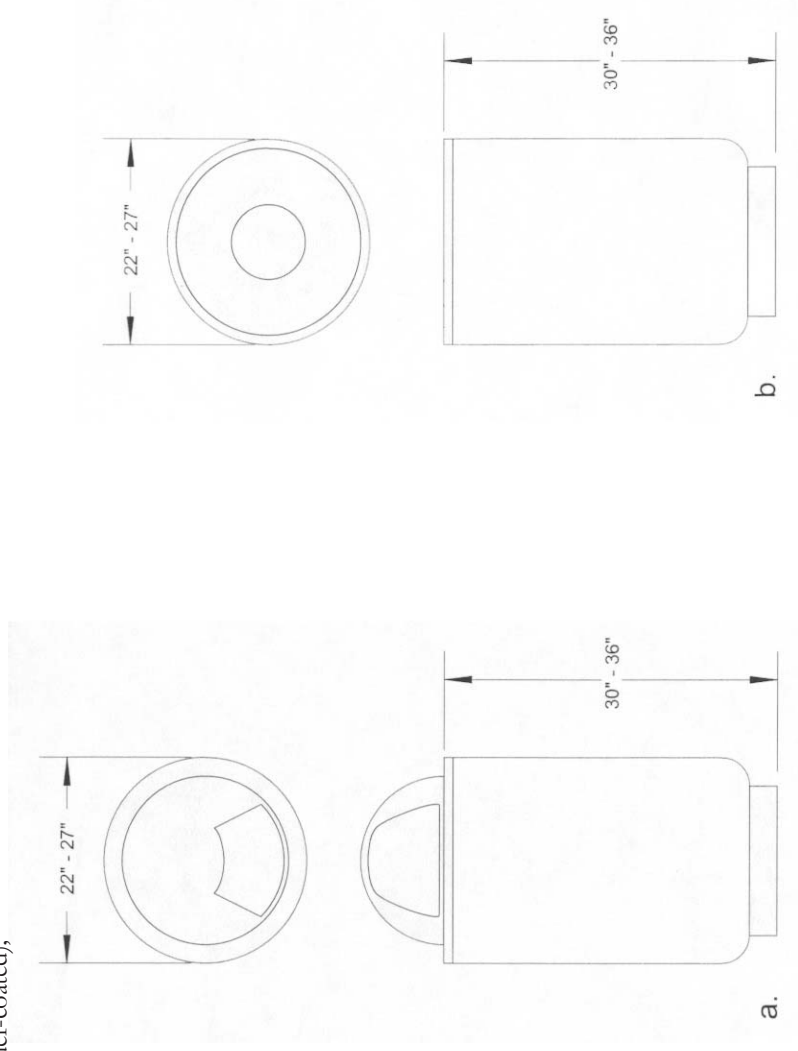
6. TRASH RECEPTACLES

Trash receptacles should be placed near seating areas.

Trash receptacle should be model no. TR-D in “dove gray”, cast stone with a light sandblast finish, as manufactured by DuraArt Stone, Fontana, CA, or an alternative approved by CCDC. Diameters vary from 22" to 27"; heights vary from 30" to 36". Alternative designs for trash receptacles may be used with approval by CCDC.

Lids should be either:

- a. #1855 domed metal lid with flap opening as manufactured by United Receptacle and supplied by West Coast Paper in Boise, ID; color shall be Empire Green (powder-coated);
- b. standard composite lid (Fiberlite) sized to fit bas as manufactured by DuraArt Stone, Fontana, CA; color should be copper;

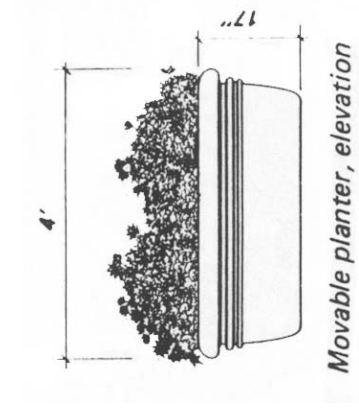


Revised: February 9, 2004
CCDC Resolution #963

7. MOVABLE PLANTERS

Movable planters should be used in the pedestrian furnishings zone and in the MPOS for seasonal plantings.

Movable planters shall be the “Design V-Grecian Planter” in “dove gray” cast stone, as manufactured by DuraArt Stone, Newark, California, or as approved.



8. DRINKING FOUNTAINS

Drinking fountains should be used in the pedestrian furnishings zone in areas of high pedestrian activity.

Drinking fountains should be model “1890”, cast metal, as manufactured by Canterbury International, Sherman Oaks, California, or as approved.



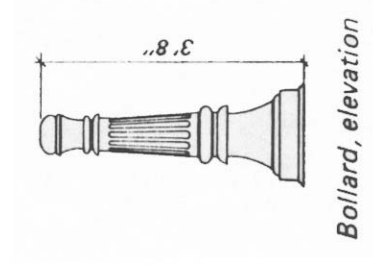
Drinking fountain, elevation

Drinking fountain, plan

9. BOLLARDS

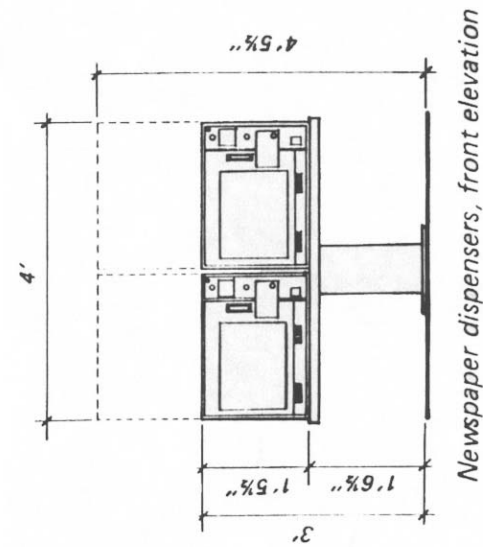
Bollards should be used to define special areas and to enhance pedestrian safety at vehicular crossings.

Bollards should be the “1980” cast metal bollard as manufactured by Canterbury International, Sherman Oaks, California, or as approved.

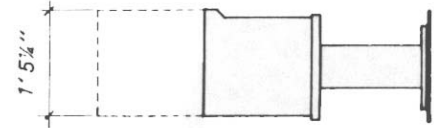


10. NEWSTANDS/NEWSPAPERS DISPENSERS

Newspaper dispensers should be located near intersections and transit facilities. A metal, tow dispenser pedestal-type unit should be used. Dispensers should be the “Sho-Rack” model “K-4916”, or “TK-4916” as manufactured by Kasper Wire Works, Shiner, TX, or as approved.



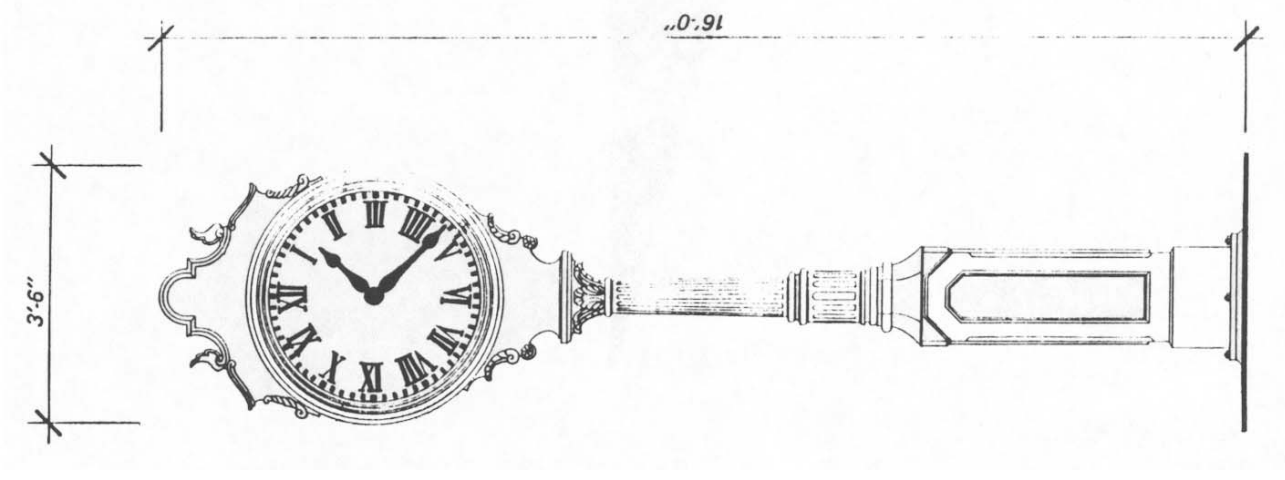
Newspaper dispensers, front elevation



Newspaper dispensers, side elevation

11. STREET CLOCKS

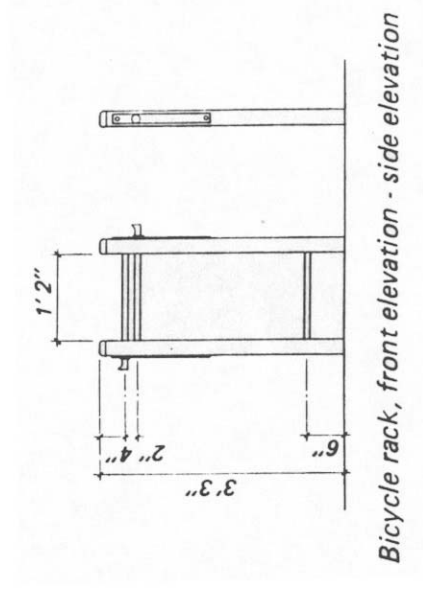
Street clocks may be located along 8th Street at locations of high pedestrian activity. Locations near jewelry stores are particularly appropriate. Street clocks should be cast metal in traditional designs such as the “Danbury Solid Top” design, as manufactured by Canterbury International, Sherman Oaks, California, or as approved.



12. BICYCLE RACKS

Bicycle racks should be located in the pedestrian furnishings zone at locations throughout development which attract cyclists.

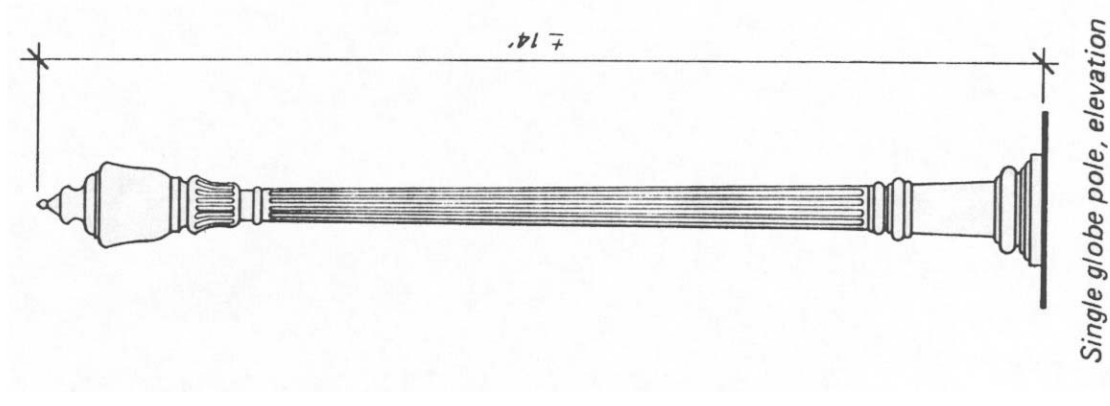
Bicycle racks should be model “C1 Bike Bollard”, as manufactured by Urban Accessories, Snohomish, Washington, or as approved.



13. LIGHTING FIXTURES

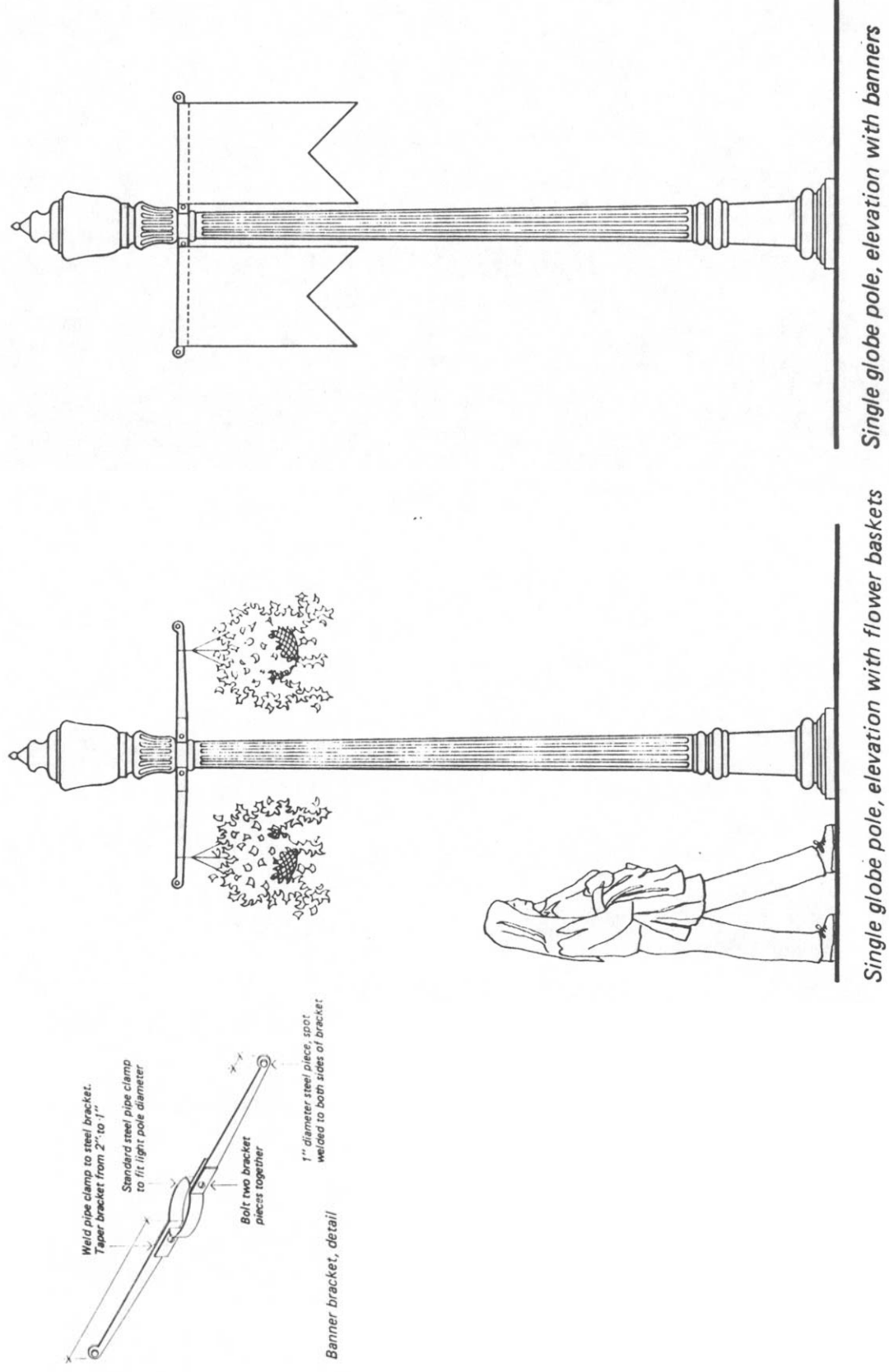
Pole top lighting fixtures should be placed centrally in the pedestrian furnishings zone at appropriate intervals.

Historic Boise cast iron light poles should be used, or a replication or facsimile.



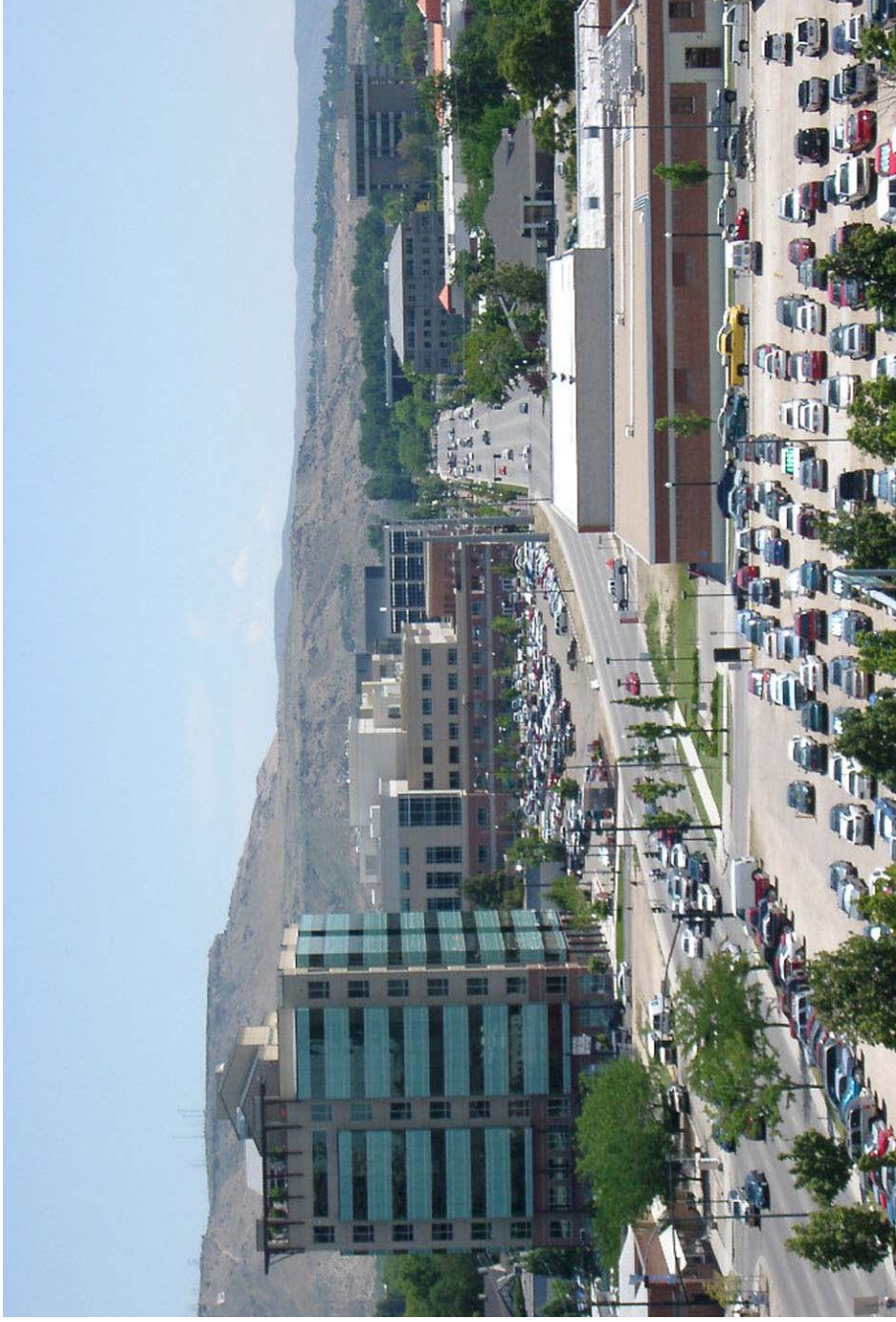
13a. BRACKETS

Brackets for banners and flower baskets should be installed on lighting fixtures.





River Street—Myrtle Street Master Plan



Prepared by Capital City Development Corporation

Boise, Idaho
Adopted by Boise City Council
November 30, 2004

Approved by the Boise City Council
(with supplemental changes to the August 23, 2004 version
approved by the CCDC Board of Commissioners – see Appendix C)

Ordinance #6362

Adopted: November 30, 2004

Effective Date: December 6, 2004

David Bieler, Mayor
Jerome Mapp, Council President
Maryanne Jordan, Council Pro-Tem
Vernon Bisterfeldt
Elaine Clegg
David Eberle
Alan Shealy

Approved by the

CCDC Board of Commissioners

Date: August 23, 2004

Resolution #1002

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Dale Krick, Vice Chairman
Cheryl Larabee, Secretary/Treasurer
Paula Forney
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Harlan W. Mann Consultant
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Quadrant Consulting

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1: Introduction



Situated on the Boise River and nestled against the foothills, Boise became a town in 1863 with the platting of ten city blocks. Boise and its surrounding cities have grown to be the largest metropolitan area in the region and downtown Boise is the center for business, government, education and culture.

The River Street–Myrtle Street Urban Renewal District was formed in December 1994 and consisted of 242 acres more or less bounded by Grove and Front streets, Broadway Avenue and River and 16th streets. The district parallels the Boise River along its entire southern edge, and is bisected by Capitol Boulevard, which runs north from the Boise Depot on the bench to the Idaho State Capitol building. Figure 1 shows the boundaries of the existing River Street–Myrtle Street district along with parcels added as part of this master planning process.

History of the River Street–Myrtle Street Area

This area has a rich history from early settlement through the railroad era to the present day. The place that would become Boise began as a gathering place for Shoshoni and Bannock tribes to barter goods. Trappers seeking furs arrived on the scene in the early 1800s. The first Fort Boise was built at the mouth of the Boise River by the Hudson Bay Company in 1834, approximately 45 miles

west of Boise's present-day location. It served as a trading post and later a way station for the first immigrants on the Oregon Trail. The discovery of gold in the Boise River Basin in 1862 brought thousands of adventurers to the area, some of whom became settlers. On July 4, 1863, Major Pinckney Lugenbeel chose the site for the second Fort Boise. The new fort was to be against the Boise foothills just west of Cottonwood Creek one-half mile from what became the Boise City town site. "A few settlers already had crops growing without irrigation along the (Boise) river. There was adequate grazing for livestock, and an abundant supply of sandstone for building purposes."¹ "On July 7, 1863, Boise held its first town meeting and, the next day, platted 10 city blocks between the river and the foothills."²

Much of the land in what became the River Street–Myrtle Street Urban Renewal District was in the floodplain of the Boise River, so it was used for farming and orchards in the early days. At the time, this area was considered to be on the outskirts of town. Changes began in the 1880s with the coming of the railroad. First, a branch of the Oregon Short Line railroad was built from Nampa to Boise in 1887, which terminated at a modest shack on the bench east of the Boise Depot's current location. Then in 1894, "trains came downtown along Front Street to a handsome new station" located between 9th and 10th streets.³ The blocks between Front and Myrtle streets became rail yards and the area between Capitol Boulevard and 9th Street south of Front Street filled with warehouses that are still evident today.

At the turn of the century, a middle class neighborhood appeared in the west end of the district and became known as the River Street neighborhood. A significant feature was Riverside Park, on the northeast corner of 11th and River streets. The park included a baseball field, with covered seating for one thousand; a covered outdoor theater, with 700 seats; and a two-story dance pavilion, with dining on the second floor. The park was a major entertainment center in the early 1900s, offering opera, musicales, vaudeville, dancing and popular baseball games.

In 1907, Thomas Jefferson Davis donated 87 acres along the Boise River for parkland, in honor of his wife, Julia. Julia Davis Park now adjoins the east end of the River Street–Myrtle Street district and has become the location of many cultural and educational institutions of statewide importance. These include the Boise Art Museum, Idaho State Historical Museum, Idaho Black History Museum, Discovery Center of Idaho and Zoo Boise. A second neighborhood of modest homes developed between the park and Front Street, now referred to as the Parkside neighborhood.

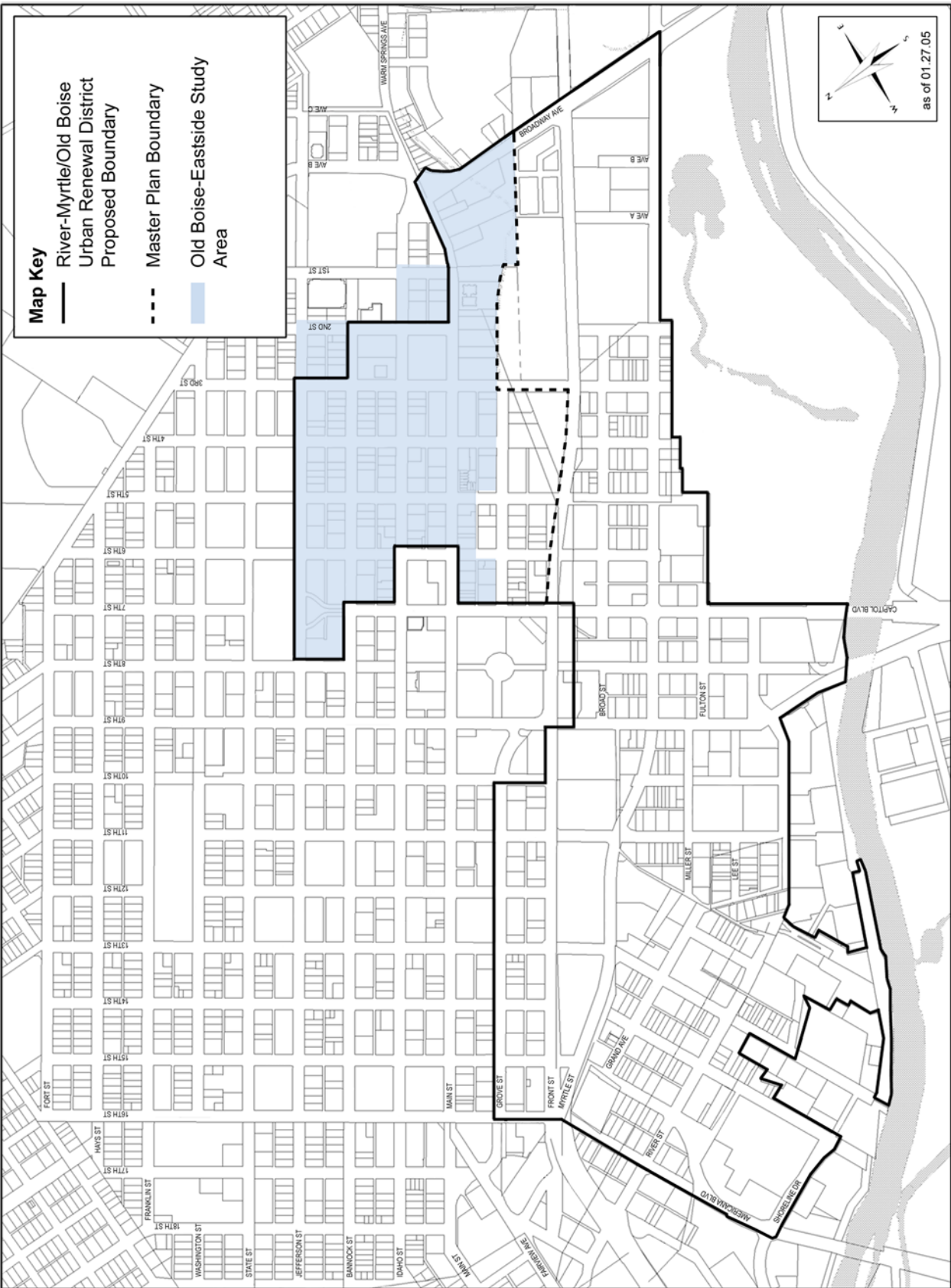
Disintegration of the River Street and Parkside neighborhoods was brought on by economic downturns; an increase in rail traffic that put these neighborhoods on the wrong side of the tracks; and a perceived need for more industrial space that led to a 1928 zoning change allowing industrial and commercial uses in these neighborhoods. The migration of the more prosperous families to Boise's East End and elsewhere added to the decline of these older neighborhoods.

¹ Source: Arthur H. Hart, *Historic Boise*, Historic Idaho, Inc. (1993), p. 10.

² Source: Barbara Herrick, *Boise: A Global Community in the West*, Community Communications, Inc. (1996), P. 17.

³ Source: Arthur H. Hart, *Historic Boise*, Historic Idaho, Inc. (1993), p. 30.

Figure 1: Additions to the River Street–Myrtle Street District



World War II brought the construction of Gowen Field, a B-24 bomber-training site south of Boise, and with it an enormous influx of servicemen and their families to town. The well-worn housing stock in the River Street and Parkside neighborhoods offered cheap rentals for these newcomers, including for the first time black residents. The River Street neighborhood has remained integrated to the present.

Lucky Peak Dam was completed in 1955, reducing the area subject to flooding in the River Street–Myrtle Street district and making more intensive development possible. By the end of the 1970s the North Bank Project brought \$1.4 million in federal Community Development Block Grant Funds to the River Street neighborhood. The funds were used to extend the Boise River Greenbelt, paint the 8th Street Bridge, build a footbridge to Ann Morrison Park and construct the Pioneer Walkway.

Creation of the River Street–Myrtle Street Urban Renewal District

The Boise City Council created the River Street–Myrtle Street Urban Renewal District in 1994 to respond to the following issues:

- **Construction of the Broadway–Chinden Connector.** The connector consists of two one-way roadways—each having five lanes—that form a ground-level couplet running from Broadway Avenue to 13th Street where they join together and continue as a grade-separated interstate highway to I-84. One of the reasons for building the connector was to redirect the traffic using Idaho and Main streets as a route through downtown to a higher capacity roadway that was outside the central business district (CBD).

The connector improved automobile access to, from and through downtown, but it also created a concern about how adjacent properties would be developed. The connector bordered the area where the rail yards had been abandoned, leaving large blocks of undeveloped land. The connector gave these properties increased visibility to automobile traffic. There was a concern that development of these properties would be dominated by suburban-style, strip commercial uses that would separate the central business district from the Boise River, Julia Davis Park and the two neighborhoods. Preparing

an urban design plan for the area, and creating an urban renewal district, provided a way to describe the community's goals for the area and to guide new development and redevelopment in a more urban direction. It also generated resources that could be invested in the area to stimulate the desired type of development.

- **A desire to protect investments made in the downtown core.** Revitalization efforts in the CBD had resulted in substantial private and public investment in property development and infrastructure. Forming an urban renewal district in the adjacent River Street–Myrtle Street area, was seen as a way to assure that redevelopment of this area would complement the work already done in the CBD, reconnect the two areas and extend the vitality of the downtown core into the River Street–Myrtle Street area.

- **A desire to create a model promoting a more sustainable development pattern for the future.** Consistent with national trends, most of the growth in Boise since the 1960s has favored automobile-dependent suburban neighborhoods at the community's edge. The negative consequences of this pattern have received more and more attention as the roadways and other public facilities and service systems become overloaded and traffic and air pollution increase. At the time the River Street–Myrtle Street Urban

Renewal District was formed, there was a desire to use this area as a place to demonstrate an alternative. The urban renewal plan envisioned creation of a series of subdistricts where mixed-use, infill neighborhoods with pedestrian-friendly streets could be created. These neighborhoods could accommodate a considerable number of housing units, along with commercial uses, within walking distance of downtown employment centers and the cultural, educational and recreational opportunities along the Boise River.

Updating the River Street–Myrtle Street Urban Design Plan

Idaho Code requires an urban renewal plan be created when an urban renewal district is established. The typical time frame for an urban renewal plan is 24 years. An essential element of this urban renewal plan is a master plan which states the desired future character for the district and sets forth a development concept, objectives and guidelines to assist decision makers with the physical redevelopment and development of the district. It also includes action steps as to how urban renewal resources should be applied to achieve plan objectives. The River Street–Myrtle Street Urban Design Plan was prepared in 1994 to articulate the vision and desired outcomes for the River Street–Myrtle Street Urban Renewal District. The Boise City Council approved the urban design plan when it created the district, and later, the council adopted the urban design plan as part of the 1997 Boise City Comprehensive Plan.

The intervening years since 1994 have brought new insights into the issues this plan needs to address. The more significant issues are highlighted below.

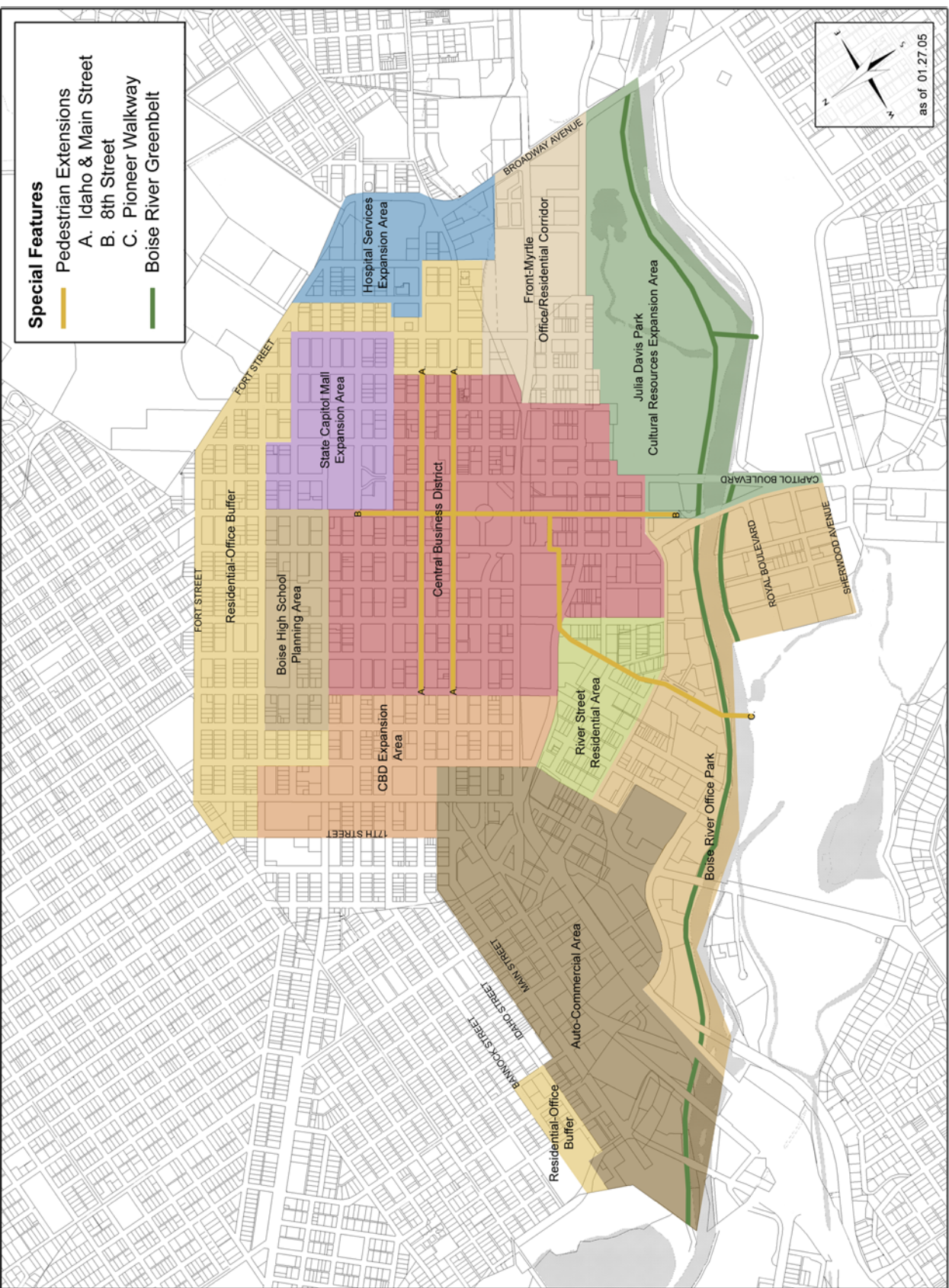
- **A more expansive view of downtown.** As Boise City has grown, so has its downtown. The geographic area that is regarded as downtown Boise has been expanding over time. In 1993, Boise City adopted the Downtown Boise Plan, which included a map showing the area considered to be the logical future extent of the downtown (see Figure 2). This area extends from Fort Street on the north to the Royal Boulevard commercial district south of the Boise River, and from Broadway and Avenue B on the east to 17th Street on the west, and then extending along Main and Fairview to the Boise River west of 30th Street. Downtown Boise Mobility Study Area, established in 2002, envisioned downtown as also including Ann Morrison Park and the Boise State University campus but only goes west to 27th Street (see Figure 3).

Downtown is becoming much more than just the central business district. It is transforming into a series of subdistricts that include the CBD but also specialized places like the Cultural District, Ada County Courthouse Corridor and in-town residential neighborhoods. It has become important that connections be created between these subdistricts while encouraging them to become distinctive places that have their own identity. Ultimately, there is a desire to have downtown function as an integrated and organic whole that remains healthy and prosperous over time. This plan update recognizes the River–Myrtle

area as an integral part of downtown Boise and identifies ways to connect this area to the other activity centers in downtown. It also gives more attention to recognizing and strengthening the subdistricts in the River–Myrtle area, and to creating vibrant places where people can live, work and play within this part of downtown.

- **A need to respond to a changing economy.** The American economy continues its shift away from resource production and manufacturing to information technology and this change is nowhere more dramatic than in Idaho. In spite of traditions and lingering perceptions, the technology sector now contributes more to the state's gross economic product than agriculture, timber and mining combined. The new economy is focused on innovation in high tech and other industries, and its resource base is people: mobile, well-educated knowledge workers. Creating a place that is attractive to this new economy is a top priority in economic development efforts nationwide, and is the driving force behind the Boise Smart City Initiative. The Smart City project, originated by the Boise Mayor's Office and CCDC in 2000, is a citizen-led effort to identify what the people of the new economy are looking for in a place to live and work, and to create such a place in Boise. The Smart City project focused initially on the River Street–Myrtle Street Urban Renewal District as a place that could become a model for the application of Smart City ideas. Recommendations generated by this effort are reflected in the development guidelines and action steps that appear later in this plan.
- **Importance of creating sustainable places.** It

Figure 2: Downtown Boise Plan (1993) – Land Use



was recently reported that Boise is the most sprawling city in the Pacific Northwest.¹ Sprawl puts pressure on resources, especially water, building materials, roads and fossil fuels. It also tends to produce more air pollution and traffic congestion, while consuming farmland close to metropolitan centers. Creating urban centers where places to live, work and play are concentrated and where people can make connections by walking, bicycling and using transit is seen as an effective antidote to sprawl. Downtown Boise and the neighborhoods immediately surrounding it are viewed as having a more sustainable pattern than is typical of the rest of the Treasure Valley. This update places greater emphasis on the issue of sustainability in the development objectives and action steps for the River Street–Myrtle Street planning area.²

- **Growing interest in downtown living.** Many metropolitan cities in the United States have experienced a surge of housing development in their urban centers. Portland, Seattle, Denver, Los Angeles and other cities are experiencing a renaissance of downtown living, which appears tied to the steady reduction in household size, a desire

for more stimulating living environments, reduced commuting distances and the availability of jobs in the CBD. The 2003 Downtown Housing Market Analysis compared downtown Boise to four other cities where downtown housing has accelerated and noted that Boise shares many of the same characteristics. The analysis concluded that there is most likely a pent-up demand for downtown housing in Boise. The Downtown Boise Housing Survey conducted in 2004 estimates that there are 5,000 one- and two-person households in Boise and Ada County that would consider moving to downtown Boise in the next 2-3 years, if housing choices were made available.³

Increasing the supply of housing in downtown Boise is seen as essential to maintaining and increasing vitality, reducing air pollution and traffic congestion and creating a more livable community. Specific steps for developing a much larger supply of housing in the River Street–Myrtle Street area, especially in the Cultural District and River Street neighborhood, receive much more attention in this update than in the previous River Street–Myrtle Street Urban Design Plan.

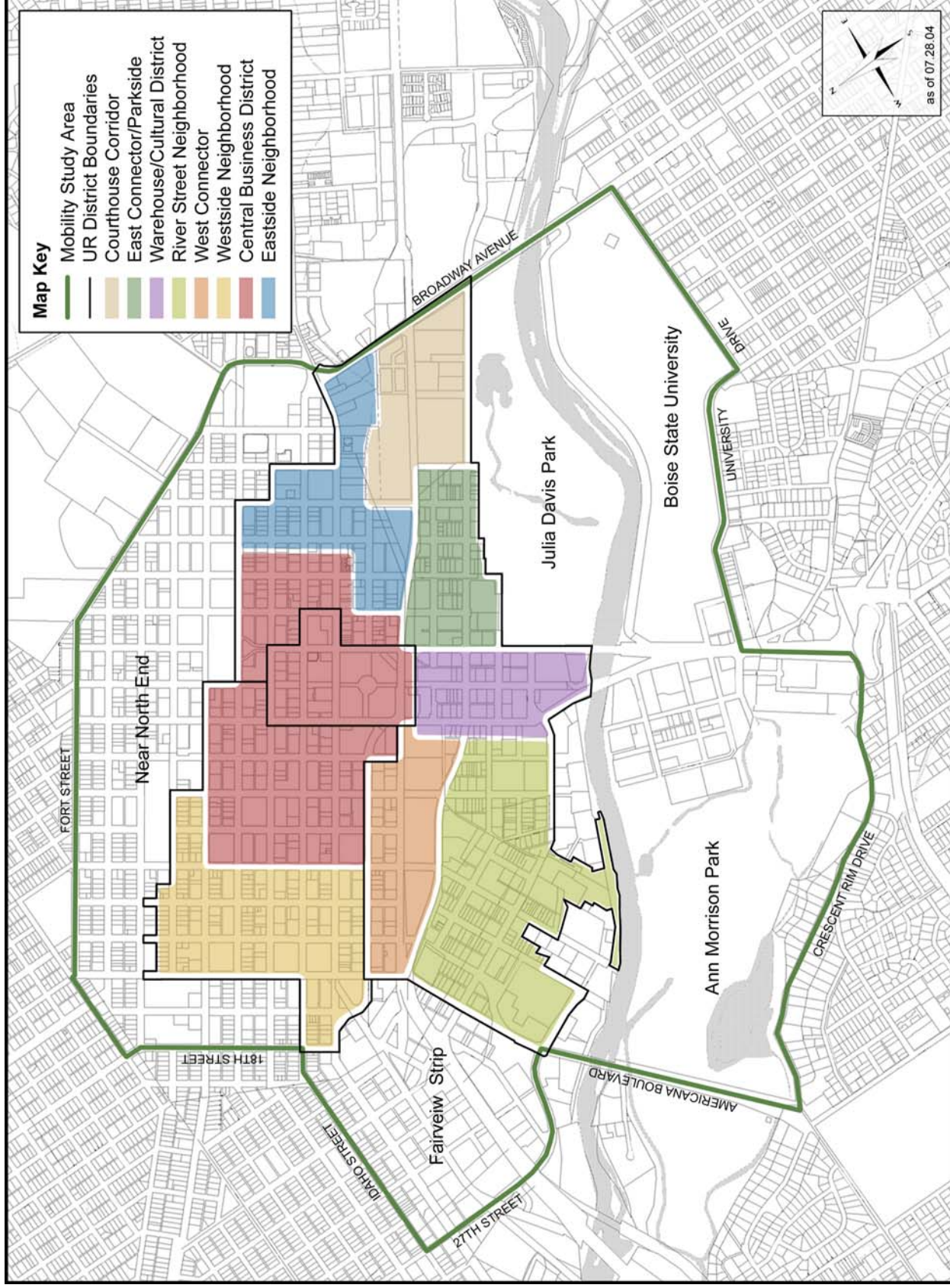
■ **Recognizing changes in the River Street–**

¹ Source: Northwest Environment Watch (http://www.northwestwatch.org/scorecard/map1_boise.asp) as reported by Joe Kolman, “Boise Sprawls the most in the Pacific Northwest,” Idaho Statesman, March 3, 2004, p. 1.

² Since the River Street–Myrtle Street Urban Renewal District was created in 1994, it has been amended to include the Old Boise–Eastside area. Two planning areas were created within the boundary of the larger district: River–Myrtle planning area and the Old Boise–Eastside planning area. The River–Myrtle planning area covers five of the seven subdistricts in the River Myrtle–Old Boise Urban Renewal District (see Figure 11).

³ This conclusion was based on data collected by Clearwater Research by telephone from one- and two-person households in Boise City (400 households) and Ada County (200 households), which formed a scientific sample from which projections could be made. This was the sample used to estimate that 5,000 one- and two-person households would consider moving to downtown Boise in the next 2-3 years. In addition, results were also obtained from approximately 800 participants who took the survey either on the Internet or by completing a paper survey. The Internet-based and paper surveys were distributed primarily to people working in downtown Boise. Approximately two-thirds of these participants said they would consider moving to downtown Boise in the next 2-3 years.

Figure 3: Downtown Boise Mobility Study Area & Urban Renewal (UR) Districts



Myrtle Street district. Many changes have occurred in the River Street–Myrtle Street district that were not anticipated in the original plan. These include construction of the Ada County Courthouse, Civic Plaza Apartments, Idaho Water Center, the Kriesenbeck office building and Winco food store; the groundbreaking for the BoDo retail development at 8th and Broad streets; the opening of the Fulton Street Theater and Big Easy Concert House; and the creation of the Pioneer Corridor plan and CCDC’s downtown housing initiative. Expansion of the Boise Centre on the Grove (Convention Center), which was planned for a site between Front, 11th, Myrtle and 13th streets, suffered reverses at the polls and it is now uncertain if it will expand and where. More is known about how the River Street–Myrtle Street district will develop in the future, and the plan needs to reflect the changes that have developed over the years. Issues that have emerged and need more attention since the original River Street–Myrtle Street Urban Design Plan was adopted include:

- Development in the Ada County Courthouse Corridor
- Concentration of destination retail, entertainment and cultural uses in and near the historic 8th Street Marketplace
- Questions regarding the convention center expansion and how it affects development between Front and Myrtle west of 9th Street
- More emphasis on housing development in the River Street and Parkside neighborhoods with the launch of the Boise Downtown Housing Initiative
- Clearer picture of how the Pioneer Walkway

could be improved to create a grand boulevard-style pedestrian and bicycle link

- **Making the plan more thorough and easier to use.** The original River Street–Myrtle Street Urban Design Plan focused on three framework issues: mixed uses, open space and pedestrian elements, and street character. It also identified five subdistricts, described their existing character and recommended actions for the future. The streetscaping standards either specified brick or allowed for brick to be used as an upgrade for almost all of the streets in the River Street–Myrtle Street district. In Boise, the master plans for the urban renewal districts are used on a regular basis to evaluate development proposals for their consistency with plan objectives. Over the last ten years, it has become evident that more specific information is needed to do these evaluations than is available in the River Street–Myrtle Street Urban Design Plan.

This update provides more specific information, especially on land uses and densities and how the subdistricts are to be differentiated from one another. It also has a clearer set of development guidelines and includes revised streetscape standards that select certain streets to be improved using brick sidewalks and others to be improved using modularly scored concrete to create more continuity of treatment and to contain costs.

This River Street–Myrtle Street Master Plan serves as the update to the River Street–Myrtle Street Urban Design Plan. It covers all of the subdistricts identified in the

earlier plan with the exception of the Old Boise Expansion Area. This area is covered by the Old Boise–Eastside Master Plan, a companion document to this plan (see pp. 11–14 for more information). Some of the subdistricts have been renamed as noted in the outline below, and a new subdistrict has been added for the Courthouse Corridor. For the remainder of this plan, in order to avoid confusion, the area covered by this plan is referred to as the River–Myrtle planning area. The River Street–Myrtle Street Master Plan addresses:

- Eligibility for Urban Renewal Activities
- Planning & Policy Context
- Market Context
- Preferred Development Concept
- Overall Plans, Guidelines & Action Steps
 - Preferred Development Concept
 - Preservation, Revitalization & Renewal
 - Building & Site Design
 - Street Character, Civic Spaces & Pedestrian Ways
 - Transportation & Parking
- Subdistrict Plans, Guidelines & Action Steps¹
 - West Connector (Former Rail Yards)²
 - River Street Neighborhood
 - Warehouse/Cultural District
 - East Connector/Parkside³
 - Courthouse Corridor
 - Old Boise–Eastside
- Implementation

Use of subdistrict plans

Because of the large size of the River–Myrtle planning area, it has been divided into subdistricts, each of which has a plan describing current conditions, objectives, development guidelines and action steps. These subdistrict plans appear in Chapter 7.

Old Boise–Eastside as part of the River Street–Myrtle Street District

Preparing this update of the River Street–Myrtle Street Urban Design Plan was precipitated in part by work being done to evaluate an adjacent area—the Old Boise–Eastside area—for formation of a separate urban renewal district. Both of these areas are shown in Figure 1. CCDC began the evaluation of the Old Boise–Eastside area at the request of property owners in Old Boise commercial area. Idaho State Code requires that an area must meet at least one of the criteria in the definition of a deteriorated or deteriorating area before it is eligible to become an urban renewal district. The CCDC Board of Commissioners received an eligibility report on the Old Boise–Eastside area in September 2001, which concluded that the area met the requirements of the Idaho State Code. In September 2002, the Boise City Council received this report, made a finding that the area was eligible for urban renewal activities and directed CCDC to prepare a master plan for consideration by the city council.

¹ The original 1994 River Street–Myrtle Street Urban Design Plan included a subdistrict called Old Boise Expansion Area in addition to others listed in this master plan. This area is now considered to be part of Old Boise–Eastside Planning Area, which is covered by its own master plan. This subdistrict no longer discussed in the River Street–Myrtle Street Master Plan although there is a brief reference to it in the chapter on subdistrict plans. Please see the Old Boise–Eastside Master Plan for more information on this subdistrict.

² Designated as the CBD Expansion Area in the 1994 River Street–Myrtle Street Urban Design Plan.

³ Designated as Myrtle Street North of Julia Davis Park in the 1994 River Street–Myrtle Street Urban Design Plan.

Tax Increment Financing

Tax increment financing (or revenue allocation as it is called in the Idaho State Code) is the method established for funding revitalization projects and activities in urban renewal districts. At the time an urban renewal district is established, a city council also decides whether to create a revenue allocation area that coincides with the district. If this area is created, the county assessor establishes the current value for each property in the district. This value is referred to as the *base value*. Over time, as both public and private dollars are invested and development occurs in the district, property values tend to rise. The increase in value over the base is called the *incremental value* or increment. In Idaho, the taxes generated by this incremental value are shared by the school district and the urban renewal agency. In Boise, the city council has formed revenue allocation areas for each of its urban renewal districts. The revenues derived from the increment for urban renewal activities are allocated to CCDC as Boise's urban renewal agency.

CCDC proceeded with preparation of the master plan.

Idaho State Code also requires that an economic feasibility analysis be done before a district is formed to determine how anticipated revenues compare to estimated costs for revitalization projects and programs. In the case of Old Boise–Eastside, a preliminary analysis was done in spring 2003. It showed that anticipated tax increment revenues from Old Boise–Eastside might not be enough to cover the expected cost of improvements (see sidebar).

At the time, some of the factors seen as reasons for this situation were:

- Constructing at least one public parking garage and upgrading the Old Boise business district to have the same streetscaping and pedestrian amenities as the downtown core was expensive.
- Old Boise–Eastside does not have large unused parcels that could generate significant new development.
- The preferred development concept for Old Boise–Eastside endeavored to complement the existing character of the area and thus shows only a modest amount of new development.
- The amount of tax increment expected from the preferred development concept for Old Boise–Eastside was correspondingly modest.

Amending the River Street–Myrtle Street District to include Old Boise/Eastside

To address this situation, the CCDC Board of Commissioners decided to evaluate what the financial situation would be if the Old Boise–Eastside area were added to the existing River Street–Myrtle Street Urban Renewal District.

The possibility of combining the two areas meant that the River Street–Myrtle Street Urban Renewal Plan including the master plan and financial plan needed to be brought up-to-date. Hence the preparation of this River Street–Myrtle Street Master Plan.

Adding Old Boise–Eastside to the River Street–Myrtle Street Urban Renewal District offered the following advantages:

- The number and size of parcels suitable for redevelopment varies between different geographic areas of downtown Boise. As a result, the redevelopment potential and the ability to generate tax increment varies between these areas. The need for improvements in different geographic areas also varies. There are instances where the ability to generate revenue and the need for improvements are not a perfect match. Having a larger land area in an urban renewal district tends to moderate these differences and allows a balance between costs and revenues to be achieved.

-
- Certain types of improvements, such as parking garages, have areas of benefit or service areas. When an urban renewal district is small, service areas may extend beyond the boundary of the district. The district cannot access resources generated by development that is in the service area but outside the district. When urban renewal districts are larger, it is more likely that service areas for improvements will fall within the district boundaries.
 - Typically bonds are used to finance improvements in urban renewal districts. The ability to issue bonds depends on whether investors believe that sufficient revenues will be generated to repay the bonds. In the case of bonds backed by tax increment revenue, the ability to repay bonds depends on future development generating increment. How much and what type of development activity will occur in a small geographic area is more unpredictable than for larger areas. Having a larger area in an urban renewal district increases the likelihood that development projections will be met and investors will be willing to buy bonds.
 - Certain types of development, like a lifestyle retail center, have the potential for generating significant tax increment and for creating spin-off development that also generates revenues. Such developments also have very specific locational requirements and there are only very limited locations that work for them. Having a larger urban renewal district allows revenues from these unique developments to be reinvested over a wider area than is possible in a smaller district.
 - A geographic area may have characteristics that are valued on a community-wide basis. It may make a unique contribution to the economic prosperity of downtown which would damage downtown's competitive position and synergy if lost. The Old Boise Historic District is an example. The contribution made by such areas would be enhanced by public improvements, but compromised if wholesale redevelopment were undertaken. Placing such areas in a larger district makes it possible to improve these areas without overdeveloping them.
- Adding Old Boise–Eastside to the River Street–Myrtle Street district also raised the issue of geographic distribution of public investment and redevelopment activity. During the public review of this plan, property owners in the existing River Street–Myrtle Street district expressed concerns that adding Old Boise–Eastside would drain resources from the River Street–Myrtle Street district and leave some areas of River Street–Myrtle Street district without resources.

Long Term Financial Plan for River–Myrtle and Old Boise–Eastside

At the end of the master planning process for both the River–Myrtle planning area and the Old Boise–Eastside planning area, Keyser Marston Associates prepared a financial analysis for a combined River Myrtle–Old Boise Urban Renewal District and for Old Boise–Eastside as a stand-alone district. In both cases, the financial analysis projects that sufficient resources from tax increment revenue and other sources will be sufficient to pay for the proposed improvements. In the intervening time between the preliminary and final analysis for Old Boise–Eastside, the assumptions regarding the value of future development and the cost and extent of improvements were refined for this area and led to a positive result. (The financial analyses for the combined River Myrtle–Old Boise District and Old Boise–Eastside are available from CCDC.)

The CCDC Board of Commissioners recommended and the Boise City Council elected to proceed with a combined River Myrtle–Old Boise Urban Renewal District because of the advantages of larger districts discussed earlier.

At the same time, CCDC wanted to address the concerns expressed by property owners about the geographic distribution of resources. This plan includes CCDC's commitment to prepare a capital improvement plan for the River Myrtle–Old Boise district every five years for the remaining life of the district, and that the five-year plan be re-examined each year as part of CCDC's annual

budget process. The first five-year plan would cover 2006-2010. A stakeholder committee including property owners, business owners and residents in the district would assist in the preparation of the five-year plan and make recommendations to the CCDC Board of Commissioners on the type and timing of public investments in the various subdistricts of the River Myrtle–Old Boise district. These subdistricts are shown in Figure 11.

Relationship between River Street–Myrtle Street & Old Boise–Eastside Master Plans

The urban renewal plan for the combined River Myrtle–Old Boise Urban Renewal District has been structured so it includes two master plans: one for the original River Street–Myrtle Street district and one for the Old Boise–Eastside study area, with some modification of the boundary between them.¹ Figure 11 on page 63 shows how the two areas relate to one another. The Central Business District and Eastside neighborhood shown on the map are in the Old Boise–Eastside planning area and the remaining subdistricts are in the River–Myrtle planning area. This approach maintains continuity in the planning work for the River–Myrtle area. Preparing two master plans also recognizes that the property owners, business owners and residents in the Old Boise–Eastside area who participated in the development of the initial development concept and goals and policies for this area have a desire for a master plan focused on Old Boise–Eastside.

¹ An adjustment has been made to the boundary between the two master planning areas so that properties which were in the original River Street–Myrtle Street Urban Renewal District, but relate better to the Old Boise–Eastside area, are now covered by the Old Boise–Eastside Master Plan. These properties are located north of Front Street between Capitol Boulevard and 3rd Street.

2: Eligibility for Urban Renewal Activities



The Big Easy Concert house and several restaurants use this alley as their front door—just one of the unique places in the River-Myrtle planning area.

Preparation of an eligibility report represents the first step in determining whether an urban renewal district can be established. It documents whether an area meets the definition in the Idaho State Code for a deteriorated or deteriorating area, and thus is eligible for urban renewal activities. The local governing body makes this determination based on whether the area meets at least one of the criteria in the definition.

The original evaluation of the River Street–Myrtle Street area was done in three parts:

- Eligibility report of the Union Pacific Railroad Property (bounded by 15th, Front, 9th, and Myrtle

streets) and adjacent areas dated September 23, 1991.

- Eligibility report for the Front–Myrtle area (bounded by Capitol, Grove/Front, Broadway, and Myrtle) and adjacent areas dated April 8, 1994.
- Supplemental Report on Eligibility of the Front–Myrtle and River Street areas dated September 19, 1994.

These reports prepared by Harlan W. Mann Consultant cover all of the properties within the boundary of the River Street–Myrtle Street Urban Renewal District as approved by the Boise City Council on December 6, 1994.

As part of amending the River Street–Myrtle Street Urban Renewal District to include the Old Boise–Eastside study area, CCDC and Boise City identified a limited number of other parcels to be added to the district. These included the following groups of parcels depicted in Figure 1:

- **Boise City Library, Log Cabin Literary Center and Anne Frank Memorial.** Boise City owns these parcels and including them would allow the application of CCDC resources to projects that benefit the community and implement the urban renewal plan. This addition also includes the Capitol Boulevard right-of-way from River Street (formerly Battery Street) to the Boise River, which would allow improvements made to this street to be continue from Front Street to the Capitol Boulevard bridge, rather than stopping part way.
- **Greenbelt parcel at the Ann Morrison Park Bridge.** This parcel is owned by the city and managed as a part of the Boise River Greenbelt system. It lies at the terminus of the Pioneer Corridor, so including it in the district would allow improvements along the Pioneer Corridor to extend to and connect with the greenbelt pathway.

- **700 N. Americana/1500 Shoreline and adjacent parcels.** Nearly 10 acres in size, the property at 700 N. Americana/1500 Shoreline represents a significant redevelopment opportunity. The adjacent parcels include several properties that are underdeveloped.

Other minor adjustments have been made in the boundary to incorporate both sides of certain streets so CCDC would have the ability to complete streetscaping improvements on both sides. An example is Broadway Avenue from the alley north of the Idaho Water Center to Myrtle Street. (The eligibility report for these additional parcels is available from CCDC.)

The CCDC Board of Commissioners accepted the eligibility report for the added parcels noted above on July 12, 2004. The Boise City Council received the report on July 13, 2004, and made a finding at its meeting on July 20, 2004 that the added parcels were eligible for inclusion in an urban renewal district based on the requirements of the Idaho State Code.

3: Planning & Policy Context



Increasing the supply of housing in the River–Myrtle planning area is one of the objectives of this plan. The River Plaza Apartments were built in 2000 through a public-private partnership and affordable housing tax credits.

This River Street–Myrtle Street Master Plan carries forward the previous planning work done for this area in the Downtown Boise Plan (1993), the River Street–Myrtle Street Urban Design Plan (1994) and the Boise City Comprehensive Plan (1997), and in some cases recommends changes to these earlier plans. The general direction, however, remains the same. This section discusses these plans and how the concepts they present relate to the River–Myrtle planning area.

- **Downtown Boise Plan (1993)**

Boise City adopted this plan “to preserve and enhance the strengths of downtown businesses by

providing guidance for future growth and development in the downtown area.” The Downtown Boise Plan promotes downtown as a center of employment and services but also envisions downtown as a desirable place for residential neighborhoods. Figure 2 shows the subdistricts defined by this plan.

The Downtown Boise Plan undoubtedly influenced the River Street–Myrtle Street Urban Design Plan, adopted only one year later. Of particular relevance to the River–Myrtle planning area, is the Downtown Boise Plan land-use plan

(see Figure 2). It shows the extent of the central business district, which includes portions of the River–Myrtle planning area. The plan describes the goals and policies that pertain to the CBD, the treatment of the Connector and 8th Street and the general promotion of residential use surrounding the CBD. The Downtown Boise Plan did not anticipate the extent of development that has occurred in the Ada County Courthouse Corridor. More specifically, this plan discusses the following issues which pertain to the River–Myrtle planning area:

- **Extent of CBD:** The Downtown Boise Plan anticipates that the CBD will extend into the area south of Front Street, between 9th and 11th, with hotel, intense retail and office uses. It also implies dense, mixed-use development at the east end of the River Street neighborhood. The Downtown Boise Plan shows the CBD occupying a somewhat larger area in the River Street neighborhood than is expected in the River Street–Myrtle Street Master Plan and in the 2025 Downtown Boise Redevelopment Plan (see Figure 3). Regardless of the differences between the plans on the extent of the CBD, these plans are in agreement when describing the type of development expected in the CBD, i.e. high intensity, compact development with a diverse mix of uses, high level of pedestrian use and 24-hour activity.

- **Connector:** The Downtown Boise Plan discourages strip commercial development and encourages high-intensity uses along that part

of the Connector within the planned CBD between 4th and 13th streets. For areas along the Connector outside of the CBD, the plan encourages commercial and mixed-use development when it is proposed as part of a planned development with residential uses.

- **8th Street:** The Downtown Boise Plan envisions the street level of 8th Street to be the pedestrian focus of the central business district. It recommends that new development be designed to promote a festival marketplace atmosphere and pedestrian activity on this street. It implies that this people-oriented atmosphere, evident on 8th Street from Bannock to Front Street, be encouraged to continue south through the River–Myrtle planning area to River Street (formerly Battery Street).

- **River Street Residential Area and Front–Myrtle Office/Residential Corridor:** The Downtown Boise Plan identifies two areas in the River–Myrtle planning area as potential residential neighborhoods. The first is the River Street Residential Area, shown as a relatively confined area west of 11th Street, between Myrtle and River streets. It is described as a prime area for high-density residential development given its location near city parks, Boise State University and the central business district. Both the Downtown Boise Plan and the River Street–Myrtle Street Master Plan recommend a greater range of housing types be developed to upgrade the perception of this neighborhood and to overcome its

current deteriorated state and historic stigma of being on the wrong side of the tracks. The River Street–Myrtle Street Master Plan designates this area as the River Street Neighborhood, and envisions it extending into the areas identified by the Downtown Boise Plan as the Boise River Office Park and the Auto-Commercial Area (see discussion below). Increasing the size of the area identified as the River Street Neighborhood is seen as an important way to achieve a critical mass of housing and to create a feeling of community.

The second area identified for a potential residential neighborhood is the Front–Myrtle Office/Residential Corridor east of 4th and north of Julia Davis Park. The Downtown Boise Plan notes that the existing development pattern consists of many small, individually held lots that preclude easy consolidation for large development proposals. The plan recommends a mix of office and residential uses designed as coordinated developments and the avoidance of strip commercial uses. The River Street–Myrtle Street Master Plan designates this area as East Connector/Parkside and envisions a similar development pattern with medium- to high-density office and residential development. Residential development would be clustered on Broad Street and Myrtle Street.

- **Boise River Office Park:** The Downtown Plan identifies a relatively large area for development of an office park between River

Street and the Boise River, from 9th Street to 14th Street. It implies that uses similar to the Forest River office development would be extended. The River Street–Myrtle Street Master Plan includes the western half of this area (from approximately 12th Street) as part of the River Street neighborhood, and envisions a more urban development pattern with a mix of uses, including a substantial number of housing units in this same area. This plan includes a recommendation that the Downtown Boise Plan be amended to adjust the extent of the office park area.

- **Auto-Commercial Area:** The area between 15th and Americana is shown for auto-oriented commercial uses in the Downtown Boise Plan. These uses are planned to extend toward the west, along Main and Fairview, to 30th Street. The River Street–Myrtle Street Master Plan identifies this area as part of the River Street neighborhood to indicate that housing would be a more prominent element in the mix of uses and that the development pattern would be more urban than suburban in nature. Service businesses are expected to develop along 16th and Americana, along the western edge of the River–Myrtle planning area.

- **Boise City Comprehensive Plan (1997)**
Adopted in 1997 as the general plan for all of Boise and its area of impact, the Boise Comprehensive Plan designates all of the River–Myrtle planning area as mixed use. The land-use matrix indicates the mixed-use designation is open to a wide range of zoning districts and land uses.

New Urbanism Principles

New urbanism, when practiced in a downtown environment, envisions a lively mix of uses such that housing, employment, cultural, educational, recreational uses, health care and other services are in close proximity to each other, allowing people to satisfy daily needs by walking, bicycling and transit. Development is at urban densities, thus making such alternative modes of transportation more feasible. Dependence on individual vehicles for transportation is minimized. A mix of uses may occur in a single building or there may be single-use buildings with different uses within the same block or subdistrict of downtown. Retail uses are clustered into both small and large shopping districts. Housing is clustered into neighborhoods. Large concentrations of office buildings, which tend to go dark in the evenings and on weekends, are discouraged. Buildings are built close to or at the sidewalk line, using designs that create interest at the street level for passersby. Sidewalks are improved with street trees, furniture, planters and distinctive lighting and may include special paving. There is a system of public spaces with seating, plaza space, landscaping and art work where people can gather for relaxation, people-watching, socializing, dining and cultural events. Specific attention is given to creating a lively, welcoming public realm for people which structures and brings coherence to the urban fabric. The street system retains the traditional grid system common to downtowns at the turn of the century. Surface parking is minimized since it tends to sap urban vitality. Every effort is made to screen surface parking lots from street view. Where parking lots are on the street, they have an edge that replicates the building line along the sidewalk and are landscaped to soften their impact. Most parking is supplied by parking structures. Whenever possible, these structures are designed so the first floor is lined with shops, restaurants, housing units and other activating uses whenever this first floor faces a street, thus making the parking garage invisible to pedestrians.

The Boise City Comprehensive Plan acknowledges downtown Boise as the civic, economic and cultural center of the Boise City Planning Area, which includes land within Boise city limits and its area of impact. Downtown Boise is expected to continue to develop with a concentrated, higher density central business district (CBD) surrounded by integrated subdistricts, each maturing under new urbanism principles as applied by specific design overlays (see sidebar). The River Street–Myrtle Street Master Plan includes development guidelines that endeavor to apply new urbanism principles in the downtown environment.

The Boise City Comprehensive Plan contains several policies that mirror or support the objectives and guidelines in the River Street–Myrtle Street Master Plan. The following policies are especially noteworthy for what they suggest about downtown housing, public spaces and the character of new development, the Cultural District and pedestrian connectivity:

- The residential component of the downtown shall be given particular emphasis to encourage living opportunities within walking distance of the downtown employment centers as well as neighborhood services. (policy 8.17.4)
- Additional civic open space facilities shall be incorporated into the downtown environment. (policy 8.17.7)
- Strip commercial, auto-intensive uses shall not be allowed in the downtown core except west of the CBD in an area bounded by 23rd Street, 27th Street, Main Street and Americana Boulevard. (policy 8.17.8)

- In order to complement the emerging cultural facilities and activities around South 8th Street and the west end of Julia Davis Park, a strategy plan shall be prepared to provide a distinct identity for the area as a cultural district. (policy 8.17.9)
- Downtown shall continue to develop with a framework of streets, paths and open spaces that build upon existing downtown networks and strengthen connections to the Boise River and between subdistricts. (policy 8.17.10)

2025 Downtown Boise Redevelopment Plan

Boise City now has three urban renewal districts that encompass the heart of downtown as one coherent piece of geography: Central District, River Myrtle–Old Boise and Westside Downtown.

During the preparation of the River Street–Myrtle Street Master Plan, it became obvious that the vision and many of the design and development guidelines for downtown redevelopment are common to all of the urban renewal districts. In addition, it is helpful to look at certain elements such as land uses, streetscaping, civic spaces, bicycle and pedestrian routes, and transportation and parking on a downtown-wide basis to see the overall picture. From a functional standpoint, the boundaries for the urban renewal districts are actually artificial lines that are needed to determine where tax increment revenue is generated and spent. One of the goals of downtown redevelopment is to arrive at a point where these districts function as interconnected places, land uses and activity centers in one district complement and support the other

districts and downtown functions as an organic whole.

The 2025 Downtown Boise Redevelopment Plan was created to illustrate the seamless nature of downtown and to describe both the overall vision and goals for its revitalization and design and development guidelines shared by all of the districts. It depicts common elements on maps that show all of the districts and the periphery around them, so that these elements are put into context. Plans, objectives, design and development guidelines and action steps that apply to a particular district are included in the master plan for that district. The 2025 Downtown Boise Redevelopment Plan serves as a preface to these master plans.

The 2025 Downtown Boise Redevelopment Plan has a time frame of 20 years because all of the existing districts are scheduled to sunset on or before 2025. Boise City and CCDC have established a vision (see sidebar) and overall goals for downtown revitalization as part of this plan. Downtown revitalization goals include:

- **Heart of region:** Assure that downtown Boise remains the foremost urban center in the region for business, government, medical care, culture, education and urban living.
- **People place:** Develop downtown as a place for people, offering a delightful and enriching environment for living, working and playing. Assure that the mix of land uses and the design of

the urban fabric support urban vitality. Emphasize the needs of people over vehicles.

- **Economic development:** Promote a prosperous economy in downtown and the region by creating a place that is attractive to cutting-edge companies and the workforce they employ.
- **Urban form:** Create a compact, urban form with a lively mix of uses. Emphasize quality, sustainability and a feeling of permanence in the how downtown is built. Develop subdistricts, in-town neighborhoods and activity centers so each has its own character and sense of identity.
- **Public realm:** Develop a system of parks, plazas and public spaces that become focal points in the downtown, act as catalysts for private development, provide refreshment and relief from urban surroundings and add to the livability of downtown. Improve streets and sidewalks so they create a welcoming pedestrian environment.

- **Transportation system:** Create an effective multimodal transportation system in downtown Boise that promotes walking, bicycling, carpooling and transit, and reduces dependence on automobiles. Ease traffic flow in downtown by improvements to street design and signalization and by traffic demand management. Develop a multimodal transportation system for the region that addresses potential traffic impacts caused by future growth in downtown, and makes travel to, from and within downtown by alternative modes practical and attractive.

Vision of Downtown Boise - 2025

The 2025 Downtown Boise Redevelopment Plan envisions downtown remaining the foremost urban center in the region for business, government, culture, education and urban living. It is and will continue to be a collection of districts, each with its own unique character and function. Districts, activity centers and in-town neighborhoods will be connected by a well-designed and functional multimodal transportation system. This transportation system will provide connections within downtown and to the surrounding region.

Downtown will become a mosaic of delightful places that evoke a sense of community and invite people to explore and enjoy. It will have a rich collection of both historic and contemporary buildings—valuing its ties to the past as well as the possibilities of the future with architecture that conveys a sense of quality and permanence. These buildings will provide the setting for a lively mix of uses, including housing, offices, retail, hotels and convention facilities, and cultural, entertainment and learning opportunities.

Downtown will become a model for sustainable land use patterns, building practices and transportation and how to make wise use of resources. Downtown will offer an invigorating environment with bustling streets, music, festivals, public art, parks and plazas, neighborhood taverns and great restaurants.

There will be a rich intellectual environment that attracts talented people and sparks creativity and innovation, supported by state-of-the-art telecommunications and technology. People from all walks of life will feel welcome and become engaged in the life of downtown. The history and culture of its peoples will be celebrated. There will be room for quirkiness and spontaneity. Downtown Boise will be a vibrant place where people thrive and businesses prosper—a place that contributes to the social, spiritual, environmental and economic well-being of residents, workers, visitors and the region.

■ Parking: Provide an adequate supply of parking while minimizing the use of land for surface parking lots. Achieve maximum utilization of parking by developing land uses that have offsetting parking demands, making private parking available to the public when not needed otherwise and making public parking facilities easily accessible from the various subdistricts and activity centers in downtown.	■ Create a destination-oriented shopping district between 9th, Front, Capitol and Myrtle with the possibility of expansion west to 10th Street. ■ Cluster hotel, retail, convention and tourism-oriented uses along Front and Myrtle streets east of 13th Street. ■ Add development to the Courthouse Corridor so a more complete mix of uses is created and the institutional effect of the existing buildings is softened. ■ Create a major mixed-use development on the southwest corner of Front and Broadway. ■ Increase densities in the River–Myrtle planning area to achieve the development forecast, increase urban vitality and make alternative modes of transportation more practical. ■ Promote the use of walking, bicycling and transit. ■ Maintain the vitality and character of River–Myrtle planning area for the long term.
Like previous plans for downtown Boise, the River Street–Myrtle Street Master Plan follows the guiding principles of mixed land uses, compact, urban-style development and pedestrian-oriented urban design. It is essentially consistent with the more recent of these plans and moves these plans forward by focusing in particular on how to: ■ Improve the Broadway–Chinden Connector so it becomes an attractive boulevard compatible with a people-oriented downtown and suitable for development of a wide range of uses along its edges, and it does not impede north-south connections between the River–Myrtle planning area and the remainder of downtown. ■ Create flourishing urban neighborhoods in the River Street area and in the Warehouse/Cultural District. ■ Achieve a balance between office and housing uses such that housing does not disappear but increases. ■ Construct an enhanced, boulevard-style Pioneer Walk through the River Street neighborhood, and use the walkway as both an anchor and a catalyst for development and as a link between the neighborhood and downtown.	This plan proposes specific objectives, design and development guidelines and action steps for this unique area of downtown Boise.

4: Market Context



Buildings in the Warehouse/Cultural District have been renovated and reused but still retain their historic flavor.

Preparation of the River Street–Myrtle Street plan update included a review of the prospects for office, retail and housing development in downtown Boise to 2025 to determine if the amount and form of development envisioned by this plan is realistic. The review relied upon a market forecast prepared by Strategic Economics for the Downtown Boise Mobility Study and reported in a document entitled Market Analysis and Growth Projections. The Downtown Boise Mobility Study is a joint effort by Boise City, CCDC, Valley Ride, Boise State University, Ada County Highway District, Idaho Transportation Department and the Community Planning Association of Southwest Idaho to assess and plan for long-term transportation needs in downtown Boise. As a part of this effort, Strategic Economics was

asked to estimate the type, amount and approximate location of future development in downtown Boise to the year 2025. This work was done in three steps:

- Assessing the regional market for housing, office and retail and estimating the share expected to be captured by downtown for each type of development.
- Identifying opportunity sites in downtown that were likely to develop or redevelop and to what level of density.
- Allocating the growth identified in the first step to the opportunity sites and then testing the pattern of development against local knowledge.

Using this process, Strategic Economics estimated the amount of development expected to occur in each of 28 traffic analysis zones. The zones in the River–Myrtle planning area were aggregated into the totals presented below. Strategic Economics forecasts the following demand in the River–Myrtle planning area for new development between 2005 and 2025, which is then compared with the total development expected in the downtown mobility study area:

Percent of Downtown Market to 2025	
■ 2,470 housing units	58% of total
■ 1,577,286 square feet of office	41% of total
■ 367,405 square feet of retail	75% of total
□ 250,000 destination serving	
□ 117,405 local serving	

The forecast reflects the fact that River–Myrtle planning area has a substantial amount of land that is either vacant, in parking lots or underutilized. It anticipates the River–Myrtle planning area will become a focal point for large-scale retail development in downtown Boise. The projected amount of retail is in addition to the BoDo

development at 8th and Broad, which is currently under construction and includes 81,700 square feet of retail, 55,520 square feet of office, a 168-room hotel and an 11-screen movie theater. The BoDo development is seen as an anchor that will attract additional lifestyle- and destination-type retailing in close proximity. The most likely location for this additional development is between Front and Myrtle to the west of 9th Street. The forecast allocates 100% of all new regional-serving retail in the downtown mobility area to the River–Myrtle planning area.

The River–Myrtle planning area is also seen as a prime location for additional office and housing development. Office development is expected to occur along the Connector and along River Street between 9th and 11th streets. Housing development is expected in the River Street neighborhood, with higher density housing clustered along the Pioneer Walk and 11th Street, and possibly in a major mixed-use development on the west side of Broadway between Front and Myrtle. The market forecast does not envision much housing being added to the Parkside neighborhood by 2025 because other locations are more likely to take precedence.

5: Preferred Development Concept



Creating public spaces that enhance private development is one way to establish a sense of place for people. The pedestrian promenade through the Courthouse Corridor connects the Idaho Water Center, Civic Plaza Apartments, and the Ada County Courthouse (not pictured) and provides an attractive setting for these office and housing developments.

The preferred development concept for the River–Myrtle planning area depicts the desired pattern of development for this area over the remaining 20-year life of the plan. It builds on the earlier work done in the River Street–Myrtle Street Urban Design Plan, which described the overall goals and objectives and development concept as noted in Appendix A.

This update describes a more refined and detailed development concept for the River–Myrtle planning area. It provides more specific information on land uses and densities and how the subdistricts are to be differentiated from one another. It has a clearer set of development

guidelines. It also includes revised streetscape standards that call for certain streets to be improved using brick sidewalks and others to be improved using modularly scored concrete to create more continuity of treatment and contain costs. The development concept emerged from the work done to identify overall objectives for the River–Myrtle planning area and to create specific plans for each of the five identified subdistricts in the area (see Chapters 6 and 7).

The preferred development concept is prefaced by a general description of the existing character in the River–Myrtle planning area.

Existing Character of River–Myrtle Planning Area

The River–Myrtle planning area is the largest planning area in an urban renewal district in downtown Boise. As such, it has a varied character reflecting its history, land ownership patterns and market dynamics. This section identifies the key elements that shape the nature of this part of downtown Boise. The subdistrict plans which appear in Chapter 7 provide more detailed information on current conditions, as well as objectives, design and development guidelines and action steps for specific areas of the River–Myrtle planning area.

Key Elements

The River–Myrtle planning area is shaped by certain organizing elements that form its structure and character. These include:

- Broadway–Chinden Connector running east and west through the planning area
- Capitol Boulevard, which is the central north–south axis through the planning area
- 8th Street, which runs north–south parallel to Capitol Boulevard but is a much quieter, people oriented street
- Warehouse/Cultural District situated in the center of the planning area
- Boise River, Boise River Greenbelt and riverfront parks along the southern edge
- River Street and Parkside neighborhoods on the west and east of the Warehouse/Cultural District
- Pioneer Walkway through the River Street neighborhood
- Courthouse Corridor

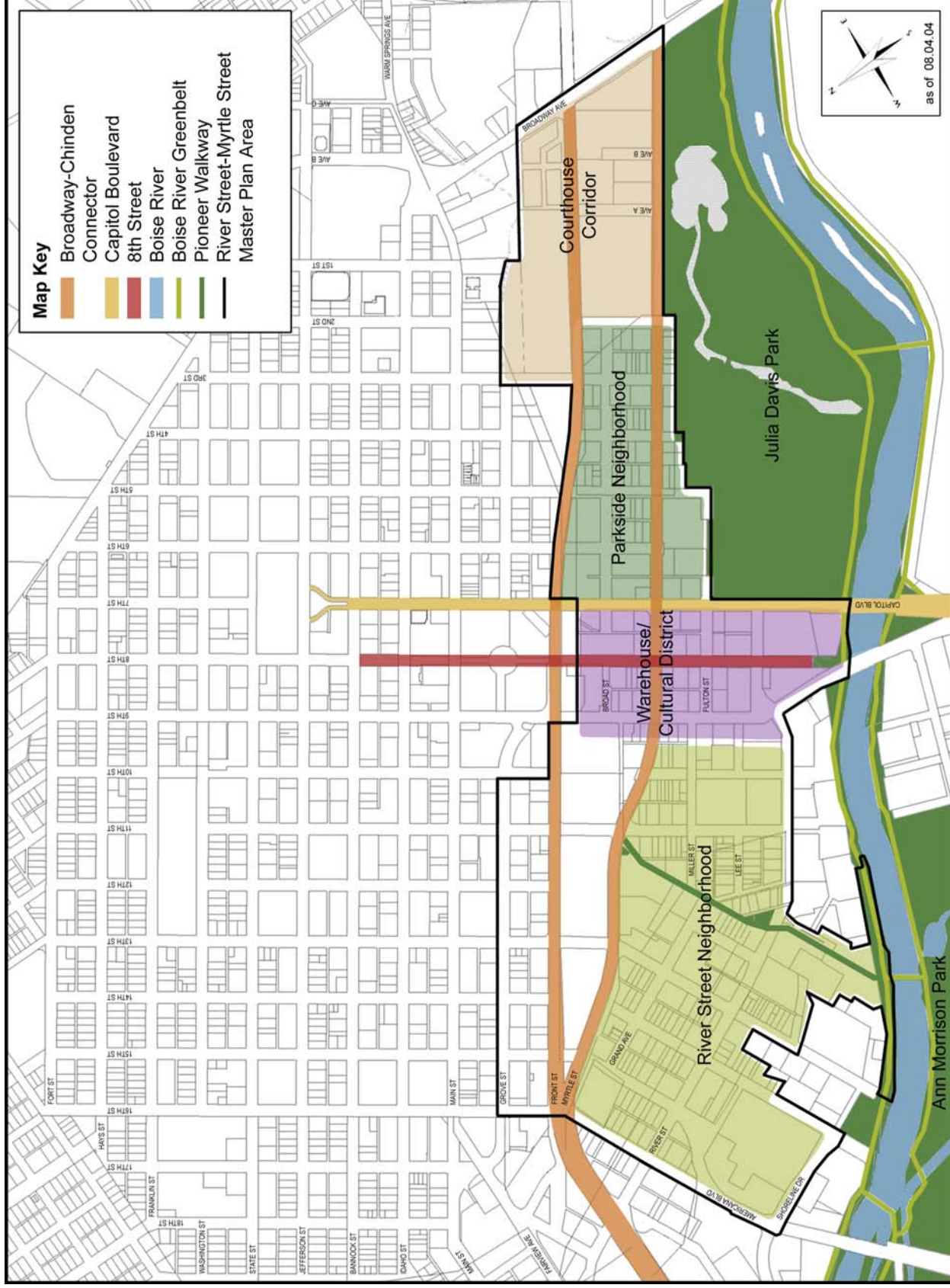
These elements are described below to provide a flavor of River–Myrtle planning area as a distinctive place within downtown Boise with its own challenges, assets and opportunities.

■ **Broadway–Chinden Connector**

The River–Myrtle planning area stretches from Broadway Avenue to Americana Boulevard and from Grove Street to the Boise River. One of its most prominent features is the Broadway–Chinden Connector that runs east and west through the planning area. It reflects the historical location of the rail yards in downtown Boise. Front Street and Myrtle Street, which form the Connector, are each five lanes with a wide-open feeling that tends to encourage higher speed driving than occurs on the traditional grid of downtown streets. Certain sections of Front Street have signals at each cross street and development along the street edge, while sections of Myrtle have no signals for several blocks and very low-scale or nonexistent development. The street sections without signals are very automobile oriented and intimidating to pedestrians. West of 9th Street there is a series of superblocks between Front and Myrtle. East of 9th Street to 2nd Street, Broad Street runs between the two sides of the connector, but it ceases to exist from 2nd Street to Broadway. Winco Foods occupies four city blocks from 2nd Street to Avenue A. The University of Idaho Foundation owns property close to four city blocks in size from Avenue B to Broadway.

In total, there are the equivalent of 64 block faces bordered by the Connector. The size and design of Front and Myrtle streets exert a powerful impact on adjacent properties and a strong influence on the

Figure 4: Key Elements



type of development being built in this environment. Development along this roadway tends to be more monumental than is typical of downtown and favors office and institutional uses. It is difficult to locate residential or retail development on these streets. The Connector also tends to create a barrier between the River–Myrtle planning area and the remainder of downtown. The impact of the Connector should not be underestimated, and specific development guidelines rigorously applied along these streets are needed to achieve an outcome that is compatible with the desired character for the River–Myrtle planning area and for downtown Boise as a whole.

■ **Capitol Boulevard**

Capitol Boulevard is the most significant street in downtown given it begins at the Boise Depot and ends at the Idaho State Capitol Building, and is the major gateway to downtown. It has the potential to become a grand ceremonial-style street. However, the intensity and quality of development along its edges vary. Parking lots and low-scale buildings dominate the section between Fulton and Front streets, and the pedestrian environment is unappealing. The section between Front and Bannock streets has an urban character with a series of high-rise buildings on its western edge. This powerful edge remains unanswered on the east side of the street, which is for the most part lined with smaller-scale historic buildings. An urban design plan for beautifying this street was prepared in 1989 and is being revised at this time. More could be done to celebrate this street.

■ **8th Street**

In contrast to the Connector and Capitol Boulevard, 8th Street is considered to be one of the premier pedestrian-oriented streets in downtown. It runs north and south near the center of the River–Myrtle planning area connecting The Grove Plaza, the Boise City Library, Anne Frank Human Rights Memorial and Boise River Greenbelt. This stretch of 8th Street does not yet have the same lively atmosphere or pedestrian amenities that are in place between Bannock and Main streets, although the plans for the BoDo retail development suggest a change in this direction is coming. The street does have a pleasant feel for both walking and bicycling with its historic flavor, trees lining its sidewalks and low volumes of traffic.

■ **Warehouse/Cultural District**

The Warehouse District, left from the railroad era, is bordered by 9th and Front streets, Capitol Boulevard and the Boise River. Many historic brick buildings remain standing and are occupied by light manufacturing, service businesses, professional offices, retail shops and restaurants. The buildings lining one block of 8th Street between Myrtle and Broad streets and two blocks of Broad Street between Capitol and 9th were renovated into the 8th Street Marketplace in the 1970s, intended as a destination-shopping district that had only limited success. These blocks have been incorporated into the new BoDo retail development that is focused on the four blocks bordered by Myrtle, 9th and Front streets and Capitol Boulevard.

This area has also been identified as Boise's Cultural District. The Esther Simplot Performing Arts Academy is located here along with the Boise City Library, Log Cabin Literary Center, Anne Frank Human Rights Memorial, Foothills School of Arts and Sciences, Idaho Urban Research and Design Center and Boise State University studio arts program. The Fulton Street Theater opened in 2000 in a renovated warehouse. The alley running parallel to 8th Street between Broad and Myrtle streets hosts a lively entertainment scene with the opening of the Big Easy Concert House and several bars and restaurants. (The full range of cultural venues in the Cultural District and nearby are described in Chapter 7 in the subdistrict plan for the Warehouse/Cultural District.)

The Warehouse/Cultural District has been identified as a prime location for creating an in-town residential neighborhood given its proximity to cultural amenities, Julia Davis Park, the Boise River and its greenbelt, BSU and shopping and dining opportunities in the central business district and the Old Boise Historic District.

■ **Boise River, Boise River Greenbelt and Riverfront Parks**

The River–Myrtle planning area has a treasure trove of recreational amenities at its doorstep. The Boise River and the greenbelt along its banks run along the south edge of the River–Myrtle planning area. The greenbelt contains a pathway used by walkers, runners and bicyclists, which extends for 26 miles.

Two beautiful, large-scale parks—Julia Davis Park on the east end and Ann Morrison Park on the west end—offer cultural facilities, playgrounds, tennis courts, playing fields and open green space immediately adjacent to the planning area.

■ **River Street and Parkside¹ Neighborhoods**

The remnants of the historic River Street neighborhood are located on the west end of the River–Myrtle planning area. There is a sizeable stock of single family houses built in the late 1800s and early 1900s, which are in fair to poor condition. Most of them being used for rental housing or have been converted to offices. These are mixed in with newer garden-style fourplexes and higher density apartment buildings. The River Street neighborhood also includes Community House, Boise City's homeless shelter and soup kitchen, and the Head Start Center. Low- to medium-rise office buildings have clustered along the north edge of this neighborhood on Myrtle Street.

The Parkside (former Myrtle Street) neighborhood is squeezed between the two sides of the Connector and 6th and 2nd streets. It is smaller than the River Street neighborhood, only two blocks deep, and has become more fragmented due to houses being cleared and the construction of warehouses and low-rise and medium-size office buildings. There are no apartment buildings in this area as there are in the River Street neighborhood. Most of the houses are in fair to poor condition.

¹ The 1994 River Street–Myrtle Street Urban Design Plan identified Myrtle Street north of Julia Davis Park as one of the subdistricts in River–Myrtle. This area was also referred to as the Myrtle Street neighborhood. This area has been reconfigured somewhat and renamed the East Connector/Parkside Neighborhood for purposes of this plan update. The existing residential neighborhood occupies a portion of this subdistrict.

Both of these neighborhoods have vacant land and surface parking lots, and the River Street neighborhood has a significant supply of underutilized land. These properties represent opportunity sites for the revitalization of these neighborhoods. Because the River Street neighborhood is so much larger and has less impact from the Connector, it offers more opportunity to re-establish a flourishing residential neighborhood. The survival of the Parkside neighborhood is less certain.

■ **Pioneer Walkway**

At present, the Pioneer Walkway is an asphalt path that runs diagonally from the Boise River at approximately 12th Street to the intersection of 11th and Myrtle streets. It runs through the River Street neighborhood. A design competition held in 2001 resulted in a concept for transforming this pathway into a grand, boulevard-style link for pedestrians and bicyclists from this neighborhood to the central business district that would also serve as a gathering place for people, an anchor for the neighborhood and a catalyst for development.

■ **Courthouse Corridor**

In the last four years, an amazing transformation has occurred on land owned by Ada County north of Front Street between 4th Street and Broadway Avenue, and on an adjacent parcel. During this short span of time, the following projects have been completed or are close to completion:

- Ada County Courthouse – 322,825 square feet of office and courtroom space

- Civic Plaza Apartments – 299 units of affordable housing
- Civic Plaza Parking Garages (A&B) – 805 parking spaces
- Idaho Water Center – 207,300 square feet of office and laboratory space
- Pedestrian promenade from the Ada County Courthouse to the Idaho Water Center
- Kriezenbeck Building – 64,500 square feet of office

These projects have created a place now known as the Courthouse Corridor that is characterized by an urban level of intensity and monumental buildings. It expands the area considered to be downtown to include what used to be bare ground, and it marks the intersection of Front and Broadway as an entrance into downtown. It has dramatically changed the picture of what the east end of the River–Myrtle planning area will become.

Taken together, these descriptions show that the River–Myrtle planning area does not have one uniform character but is composed of a series of places or subdistricts each of which has its own character. Some of these places have a character that will most likely persist into the future either because construction is recent like the Courthouse Corridor or there are historic buildings that will be retained, as is the case in the Warehouse/Cultural District. The future of the River Street and Parkside neighborhoods and the West Connector is less certain and will depend on the interplay of land values, market opportunities, public investment and the implementation of the objectives and guidelines contained in this plan.

Desired Character for River–Myrtle Planning Area – 2025

The preferred development concept forms the basis for the plan’s policy emphasis and is used to predict the tax increment revenue that will be available to pay for the capital improvements needed to implement the plan. It is intended to take the long view—to depict what the River–Myrtle planning area will be like in the year 2025. This description serves as a summary of Chapters 6 and 7, which explain in more detail the basis for this concept presented here.

The ideal result of implementing the River Street–Myrtle Street Master Plan would be:

- The River Street neighborhood becomes a flourishing urban neighborhood with a lively mix of uses, a full range of housing choices and a neighborhood-shopping district.
- The Pioneer Walkway is a delightful and inviting boulevard-style pedestrian link between the Boise River to downtown through the River Street neighborhood used by the community and visitors for walking, bicycling, shopping, dining and recreation.
- The Pioneer Walkway is lined with urban intensity residential, office and retail development, restaurants with outdoor dining, artwork and other cultural amenities.
- The Broadway–Chinden Connector west of Capitol Boulevard becomes a focal point for hotels, destination-type retail and entertainment, convention and tourism-oriented uses.
- A destination-shopping district is developed between 9th, Front, Capitol and Myrtle with the possibility of expansion west to 10th Street.

- Historic buildings in the Warehouse/Cultural District are restored and fully occupied where appropriate. Weaker buildings are replaced by infill development that, where applicable, complements the existing historic fabric.
- Eighth Street is recognized as the primary pedestrian connection between the central business district and the Boise River, and is improved in the River–Myrtle planning area in a manner similar to how this street is improved between Bannock and Main.
- A compact, urban-style residential neighborhood develops in the Warehouse/Cultural District between 9th Street, Myrtle Street, Capitol Boulevard and the Boise River.
- The Parkside neighborhood has an eclectic mix of land uses with office uses clustered along Front Street and residential uses clustered along Broad Street and on Myrtle Street where views of Julia Davis Park would create an attractive environment for housing. Design changes will be made to Myrtle Street to soften its impact on adjacent properties and make housing more feasible.
- Additional development occurs in the Courthouse Corridor along Front Street that is compatible with the existing apartments, expands the range of uses, and softens the current institutional effect of the existing buildings.
- The property on the southeast corner of Front and Broadway is developed into a major mixed-use development.
- Improvements are made to the Broadway–Chinden Connector so that it becomes an attractive boulevard compatible with a people-oriented downtown and suitable for development of a wide range of uses along its edges, and does not impede north-south connections between the River–Myrtle planning area and the remainder of downtown.

- Thirteenth, 11th, 8th, 5th and 3rd streets and avenues A and B become pedestrian and bicycling routes across the Connector.
- Fourth Street and either 10th or 12th streets or both are re-established as through streets between Grove and Myrtle streets.
- Blocks currently dominated by surface parking are redeveloped with a mix of building uses designed to maximize the capacity of each block, giving consideration to the scale and character of adjacent buildings.
- Densities in the River–Myrtle planning area increase to achieve the development forecast, create urban vitality and make alternative modes of transportation more practical.
- Public and private parking garages are built to replace surface parking, with public garages situated so as to provide reasonable coverage for the River–Myrtle planning area.
- Safe and convenient routes for walking and bicycling are created, and these modes become the predominant means by which people meet their daily transportation needs in downtown Boise.
- The River–Myrtle planning area is better served by transit.

intervention and incentives, especially to achieve the level of housing envisioned. People interested in living downtown view the Cultural District as being a desirable location for housing. It already has a certain ambience and is close to existing downtown amenities. The River Street and Parkside neighborhoods in their current condition have less appeal.¹ Public investments that create a sense of community will be needed to develop confidence in future prospects for the area and attract housing developers, buyers and renters.

The vision anticipates a major transformation of the River–Myrtle planning area to a much more urban place with a vibrant cultural and shopping district at its center, a concentration of destination and tourism-related businesses along the Connector west of Capitol Boulevard, flourishing in-town neighborhoods in the River Street neighborhood and the Cultural District and higher density office and housing development along the Connector east of Capitol Boulevard. Development will be leveraged by investments in public facilities, such as construction of streetscapes, public parking facilities and an enhanced Pioneer Walkway, and public-private partnerships to develop housing.

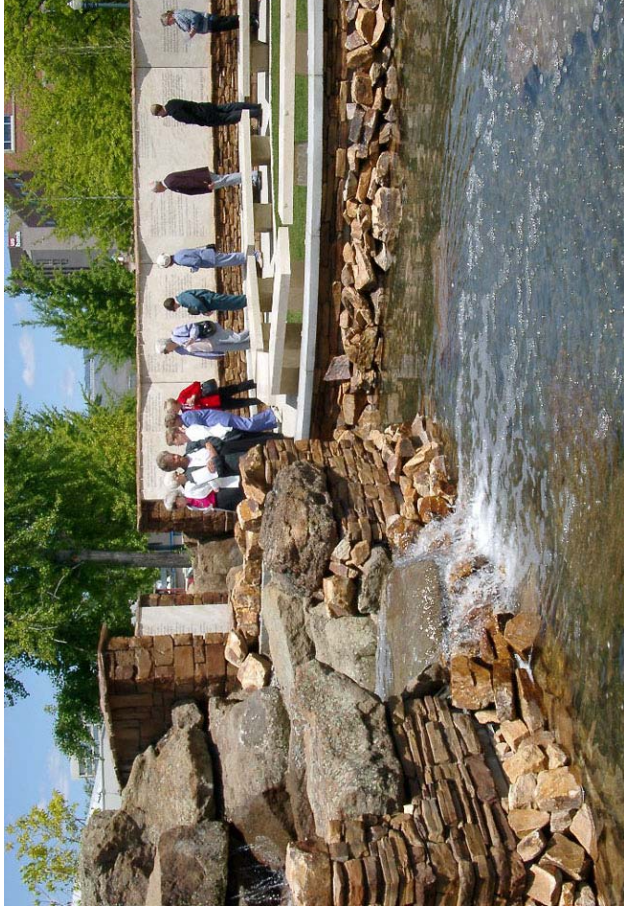
Currently, only a fraction of the River–Myrtle planning area’s economic potential has been realized. The expectation is that every parcel would contribute fully to the social and economic vitality of this planning area and to downtown Boise as a whole.

Achieving the Concept

Achieving this development concept will most likely require re-evaluating zoning regulations and market

¹ In spring 2004, CCDC conducted a consumer survey of people living in Boise and in Ada County to measure the level of interest in downtown living. Two of the questions focused on what areas of downtown were considered most desirable as a place to live. When respondents were asked whether they were interested in living in each of six different areas of downtown, they chose the Cultural District more frequently than River Street, Parkside and Westside districts but less frequently than the Heart of Downtown or Old Boise. When asked to pick their top choice among the six areas, approximately 12% of the respondents chose the Cultural District, 12% chose the River Street District, and 9.7% chose the Parkside District as their first choice.

6: Overall Plans, Guidelines & Action Steps



Creating attractive and refreshing places where people experience a feeling of community adds to the livability and enjoyment of the downtown environment. The Anne Frank Human Rights Memorial—located in Boise's Cultural District—is one such place.

The most important function of the River Street–Myrtle Street Master Plan is to identify the significant issues affecting this area, describe what the current situation and desired future outcomes are relative to these issues and then communicate how the plan proposes to arrive at these outcomes. Plans have been prepared at two levels for the River–Myrtle planning area because covers such a large geographic area. First, there are a series of overall plans that focus on the following issues:

- Preferred Development Concept
- Preservation, Revitalization & Renewal
- Building & Site Design
- Street Character, Civic Spaces & Pedestrian Ways
- Transportation & Parking

The overall plans provide an overview of the master plan recommendations. They help make understanding the overall intent of the River Street–Myrtle Street Master Plan easier and focus on recommendations common to all of the geographic areas in the River–Myrtle planning area. They also provide the basis for the capital investments proposed to implement the plan. The plan for each issue includes a discussion of existing conditions and desired outcomes. It presents design and development guidelines and action steps as way to communicate what needs to be done to implement the plan recommendations for that issue.

1.0 Preferred Development Concept

Second, the River Street–Myrtle Street Master Plan includes a series of subdistrict plans. The River–Myrtle planning area covers approximately 230 acres and naturally divides into a series of geographic areas that vary in character from one another. A map of these areas or subdistricts is on page 63. The River Street–Myrtle Street Master Plan includes a specific plan for each of these subdistricts, which describes the existing conditions, desired future character, design and development guidelines and action steps for the subdistrict. The guidelines and action steps chart a course for achieving the desired character for each subdistrict. The subdistrict plans are in Chapter 7.

If a guideline or action step affects more than one subdistrict, it is typically found in the overall plans. If a guideline or action step is specific to one subdistrict, it typically appears in the subdistrict plan.

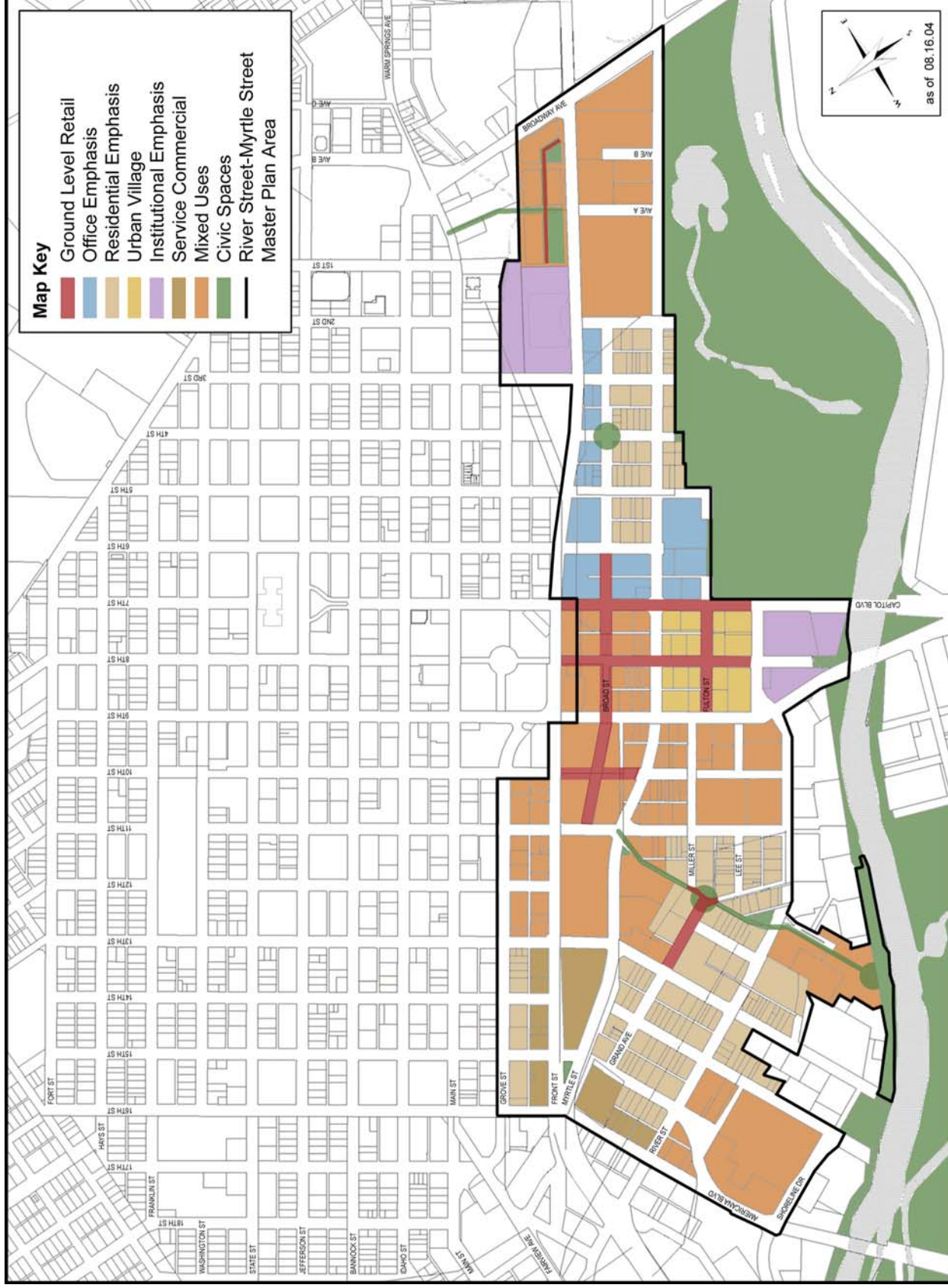
The 2025 Downtown Boise Redevelopment Plan serves as a preface to this River–Myrtle Master Plan. It contains design and development guidelines for urban design, building and site layout and civic spaces that apply to all of the downtown urban renewal districts. Understanding the full intent of what is intended for the River–Myrtle planning area requires referring to the 2025 Downtown Boise Redevelopment Plan and both the overall and subdistrict plans in this River–Myrtle Master Plan.

The preferred development concept takes a long-term view of future land-use patterns in the River–Myrtle planning area. It describes the pattern of land uses desired by the year 2025 using emphasis areas as well as other land-use categories. Emphasis areas have a predominant use but they are not limited to that use only. In fact, a mix of uses in each of the emphasis areas is encouraged to promote urban vitality and allow people to live, work, shop and play either within their own neighborhood or in other areas of downtown. Having a mix of uses in close proximity to one another also encourages travel by walking, bicycling and transit.

The types of uses anticipated by the preferred development concept are described below and shown in Figure 5.

- **Mixed Uses:** Higher intensity areas that typically have a mix of office, residential, retail, restaurants and entertainment uses, and may also have lodging and convention or conference facilities. These areas typically have a high concentration of pedestrian activity.
- **Residential Emphasis:** Areas with a mix of housing in terms of type and cost, where housing projects are clustered into neighborhoods, that may also have retail, restaurants, office, service businesses and other support uses that meet people's daily needs.

Figure 5: Preferred Development Concept



■ Urban Village: Areas with a concentration of higher density residential uses that also have smaller-scale office uses and neighborhood-oriented retail and restaurants. ■ Office Emphasis: Areas where office uses are concentrated that also may have retail, restaurants, lodging and convention or conference facilities, and higher density residential uses. ■ Institutional Emphasis: Areas where government offices are concentrated such as the Ada County Courthouse Corridor that also may have offices, retail, restaurants, lodging and residential uses.	The preferred development concept recognizes the major features in the planning area and how they influence the types of land uses that will be successful. It shows a concentration of mixed use, office, institutional and service commercial uses along the Connector, Capitol Boulevard, Broadway and Americana. These streets are classified as principal arterials and carry high volumes of traffic relative to other downtown streets. Given their functional classification and design, traffic volumes on these streets are expected to increase as downtown intensifies over the next 20 years. (Estimates of traffic demand in 2025 on streets in the River-Myrtle planning area are discussed in Section 5: Transportation and Parking of this chapter. These estimates were derived from the traffic modeling done as part of the Downtown Boise Mobility Study in 2004.)
■ Ground Level Retail: Street frontages where ground level retail and other active, people-oriented uses are encouraged. The River Street-Myrtle Street Master Plan recommends clustering retail uses into shopping districts rather than allowing retail uses to become dispersed. Retail uses are encouraged to occupy the first floor of buildings where upper floors are occupied by other uses.	It is difficult to locate residential uses facing directly onto such streets. Residential uses tend to be more successful when clustered into neighborhoods served by lower volume, local streets. In the River-Myrtle planning area, two areas have been identified as residential emphasis areas and one as an urban village.
■ Service Commercial: Areas where businesses such as records storage, auto parts and repair, car washes and auto detailing, plumbing, and electrical and HVAC installation and repair are located. These businesses offer services to people living and working in downtown, and often need outdoor storage, truck parking and shop space.	■ River Street neighborhood located between the Connector and the Boise River and between Americana Boulevard and 11th Street. ■ Parkside neighborhood located between Front, 6th and 2nd streets and the northern boundary of Julia Davis Park. ■ Fulton Street Village between Capitol Boulevard and Myrtle, River (formerly Battery) and 9th streets in the Warehouse/Cultural District.
■ Civic Spaces: Park and plaza spaces designed and intended for public use.	

The plan has an objective to increase the amount and range of housing options available in the planning area, consistent with CCDC's downtown housing initiative. Achieving this objective is one of the greatest challenges in implementing this plan. By identifying housing emphasis areas, the plan makes clear that housing development is desired and that housing projects should be focused in certain areas rather than scattered.

Mixed-use areas are shown along the Connector from 11th Street to Capitol Boulevard and from 2nd Street to Broadway Avenue. Mixed uses are also shown between 11th, Myrtle, 9th and River streets between the River Street neighborhood and the Warehouse/Cultural District. This area is considered a transitional area. It is uncertain how this area will develop; several possibilities are plausible. Office development may occur due to this area's proximity to Forest River, 9th Street and the Connector or more housing may occur if the residential portion of the River Street Neighborhood expands. The plan also identifies three sites as having potential for major mixed-use projects: at Americana and Shoreline, on Broadway between Front and Myrtle and along the Pioneer Corridor south of River Street. The first two locations have large sites and excellent visibility from major streets. The location on the Pioneer Corridor has exceptional natural features on which to build.

Office uses are concentrated along Front Street, Capitol Boulevard and Myrtle Street but are also expected to be present in the mixed-use areas.

Institutional uses are concentrated in two areas: at the southern end of 8th Street where the Boise City Library, Log Cabin Literary Center and Anne Frank Human

Rights Memorial are located, and in the Courthouse Corridor.

Ground level retail is concentrated on a limited number of streets. This pattern is consistent with one of the objectives of this plan, which is to cluster retail into shopping districts rather than dissipating retail energy by allowing it to locate on any street frontage. This approach is contrary to the current requirement in the C-5 zoning district that 70% of the ground floor on each street frontage be designed to accommodate retail and other people-oriented, active uses. The River Street-Myrtle Street Master Plan recommends that this requirement be revised so it would only apply on streets designated for street level retail in the Preferred Development Concept.

Service commercial uses are clustered at the periphery of the River-Myrtle planning area along the Connector west of 13th and along Americana. Civic spaces indicate where existing and proposed public parks and plazas are located.

Design & Development Guidelines

Design and development guidelines regarding preferred land uses are best stated as part of the subdistrict plans. These plans are in Chapter 7.

Action Steps

Actions for implementing the preferred development concept presented here focus on the degree to which existing zoning designations support the objectives of the River Street-Myrtle Street Master Plan. Proposals to

revise a zoning designation that would apply to any subdistrict where this zoning designation occurs are noted here. The reasons for these recommendations are discussed Chapter 8. The subdistrict plans contained in Chapter 7 contain action steps regarding revisions to zoning designations that would apply to specific parcels in a particular subdistrict.

- Work with Boise City to revise the provision in the C-5 zoning district that requires 70% of each street frontage to be designed to accommodate retail or other people-oriented, active uses so it applies to streets designated for ground level retail uses on the Preferred Development Concept (Figure 5). Amend the list of acceptable uses to include residential at the ground level.
- Evaluate whether the existing R-O zoning regulations need to be revised to better support plan goals promoting urban intensities, housing and mixed use in the River–Myrtle planning area, and work with Boise City to revise these regulations if needed.
- Evaluate if a new zoning category is needed which would give more weight to residential uses to replace the R-O zoning designation in the River Street and Parkside neighborhoods, and work with Boise City to develop a new zoning category if needed.
- Evaluate whether minimum residential densities are needed in zoning districts applicable to the River–Myrtle planning area to achieve housing goals and propose zoning revisions if warranted.

2.0 Preservation, Revitalization and Renewal

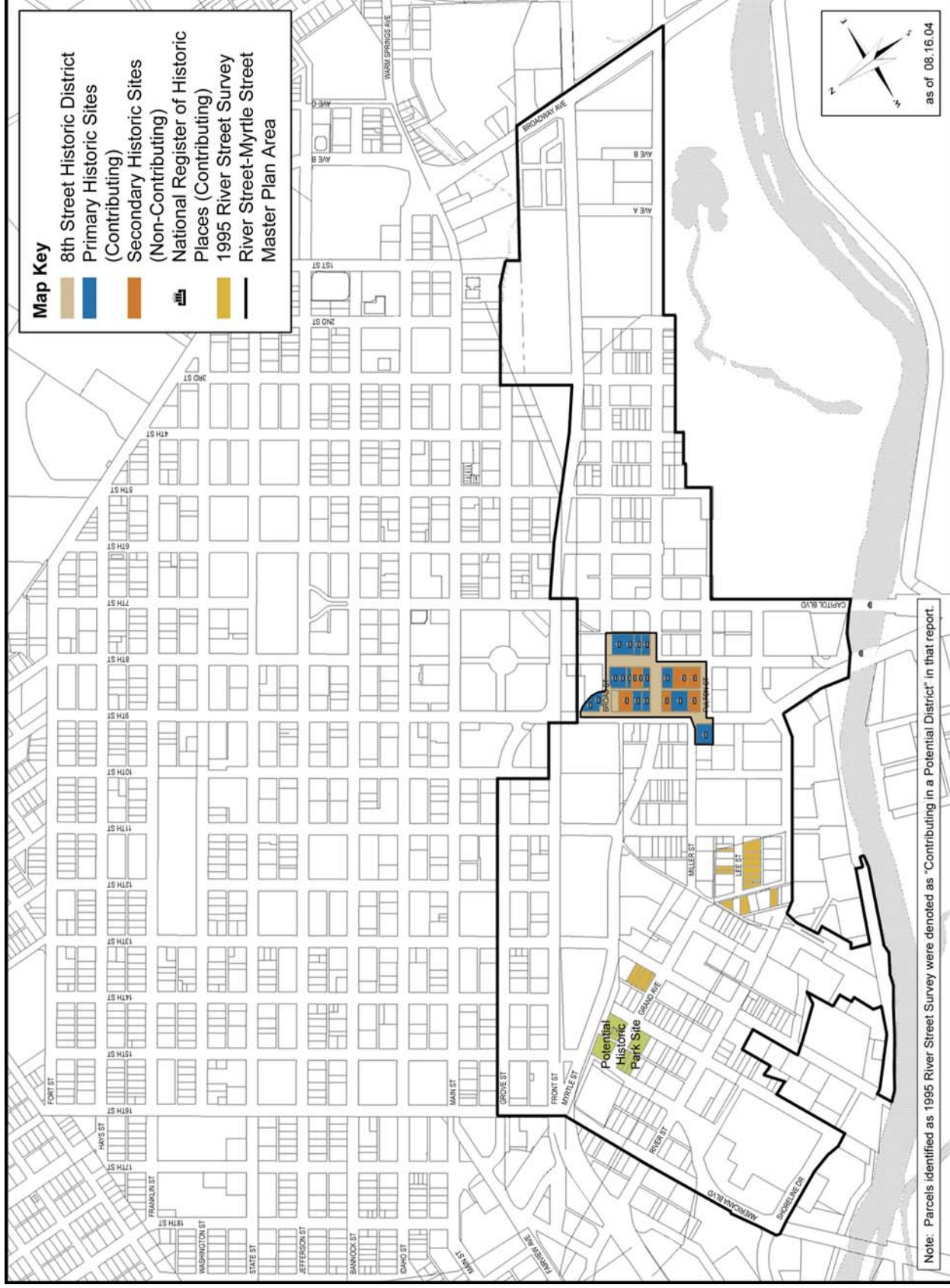
Existing Conditions

The River–Myrtle planning area is distinguished by a notable collection of historic commercial buildings and warehouses between Front, Capitol, Myrtle and 9th. These beautiful brick buildings that form intact block faces have been included in a historic district established by Boise City (see Figure 6). Two additional areas in the River Street neighborhood have been studied for possible inclusion in a historic district: houses located on Lee and Ash streets, and a commercial building and houses on 13th Street between Myrtle Street and Grand Avenue. In addition there are houses from the late 1800s and early 1900s in the Parkside neighborhood, although their historic value has not been evaluated.

The buildings along 8th Street in the Warehouse/Cultural District provide an irreplaceable link to Boise’s history, and add a unique and authentic character to this part of the River–Myrtle planning area. A succession of property owners has viewed them as worth saving and renovating. The BoDo project currently under construction builds upon this existing historic fabric and adds new buildings that have a compatible style.

The houses on Lee Street are more problematic. Most if not all of these are rentals. Property values are rising but rents are lagging behind, creating an economic squeeze for the property owners. As the land gets more valuable, it becomes harder to resist clearing these properties and building new development that would generate higher return. The houses on Lee Street are symptomatic of a

Figure 6: Designation of Historic Resources



larger issue in the River–Myrtle planning area, which has a large number of older houses in fair to poor condition in a similar situation. These houses provide not only a historic record of the River Street and Parkside neighborhoods but also a reservoir of affordable housing. If land values continue upward, it will be more and more difficult for property owners to hold onto them. Several courses of action are available including retaining such buildings in place, relocating them or removing them. The plan recommends evaluating if a collection of the best examples of historic residences could be moved to a historic park within the River Street neighborhood while allowing for removal of the remaining houses and redevelopment of these sites.

Desired Outcomes

The exceptional collection of brick warehouses and commercial buildings from the railroad era in the 8th Street Historic District would be preserved, and renovated and reused as part of redevelopment. On occasion, a particular building in this district may have deteriorated to the point it is unsalvageable, and in such cases, the building could be removed. Replacement structures in the historic district would be compatible with the existing historic fabric but may be of a different architectural style. Preservation of historic structures elsewhere in the River–Myrtle planning area is more problematic due to the fact that many of them are single-family houses in fair to poor condition situated on lots that are rising in value. The plan recommends evaluating if some of these houses could be relocated, renovated and preserved in a historic park in the River Street neighborhood.

General guidelines and action steps related to historic resources in the River–Myrtle planning area are stated

here. Specific guidelines and action steps related to 8th Street Historic District are included in the subdistrict plan for the Warehouse/Cultural District. Guidelines regarding the historic houses in the River Street and Parkside neighborhoods are included in the subdistrict plans for those areas. (See Chapter 7 for subdistrict plans.)

Design & Development Guidelines

- 2.1 Whenever feasible, preserve the historic character in areas where historic buildings are concentrated, while encouraging development of underutilized property. If increasing property values and property taxes work against retaining historic buildings, evaluate if the buildings could be moved in order to save them.
- 2.2 Facilitate the renovation and reuse of historic buildings whenever feasible.
- 2.3 Where a historic structure is in poor repair or obsolete, evaluate alternative approaches including reuse of the structure, reuse of the façade or replacement. Weigh the contribution made by the structure to the historic fabric and to the community's record of its history with the benefits derived from redevelopment proposals. Give weight to proposals that result in increasing the supply of housing in the River–Myrtle planning area.
- 2.4 Encourage a range of architectural styles in new buildings in a way that does not detract from areas of historic character in the River–Myrtle planning area.

2.5 Design new infill buildings in areas dominated by historic buildings so they respect the current scale and massing of these buildings although new buildings may have a variety of architectural designs.

Action Steps

- Work with the Boise City Historic Preservation Commission (HPC) to create a method of evaluating redevelopment proposals affecting historic structures. Weigh the assets being gained, preserved and lost, and the degree to which redevelopment will achieve the desired character and preferred development concept stated in this plan. Work with the HPC early in development review process.
- Consult with the HPC when redevelopment proposals involving historic structures are initiated to obtain early input on ways to meet preservation and redevelopment goals.

3.0 Building and Site Design

The 2025 Downtown Boise Redevelopment Plan includes guidelines that apply to building and site design throughout the urban renewal districts in downtown Boise. The purpose of these guidelines is to establish an overall approach to downtown development that is consistent with the principles of compact, mixed-use design established in 1987 with the revision of the Central District plan. These principles have been reaffirmed in the Downtown Boise Plan (1993), Boise City Comprehensive Plan (1997), the River Street–Myrtle

Street Urban Design Plan (1994) and Westside Downtown Framework Master Plan (2001).

This section describes the desired pattern for development intensities in the River–Myrtle planning area. Attention to development intensity is important if the long-term development forecast for the River–Myrtle planning area is to be achieved. This forecast envisions the addition of 2,470 housing units, 1,577,286 square feet of office and 367,405 square feet of retail. This forecast can only be achieved if building densities are increased, which raises issues regarding the current zoning in the River–Myrtle planning area.

Existing Conditions

At present, the River–Myrtle planning area is characterized by relatively low densities throughout the area with the exception of the Courthouse Corridor, the brick buildings in the historic district along 8th Street, a cluster of buildings at the intersection of 13th and River streets (Shoreline Plaza, River Plaza and Community House) and the Pioneer Plaza buildings on Myrtle Street.

The central section of the planning area is zoned C-5, which is the most intense mixed-use zoning district allowed in Boise City. This zoning district has no height limit. It has a 4:1 limit on floor area ratio¹ for office and commercial uses but no limit for residential uses. Most of the remainder is zoned Residential-Office (R-O), which has a typical height limit of 35 feet and 47.5 feet if rooflines are articulated. These height limits indicate that the R-O district anticipates low- to medium-intensity development. Boise City allows these height limits to be exceeded with approval of a conditional use permit by the Planning & Zoning Commission. The intense

development in the Courthouse Corridor, which includes the Ada County Courthouse, Civic Plaza Apartments and Idaho Water Center, is the result of height exceptions being granted.

The R-O zoning district also has a limit of 87.5 units per acre on residential uses. This density limit may need to be raised in the River–Myrtle planning area in order to improve the economic feasibility of building housing as land prices continue to rise here and throughout downtown. Raising densities will also most likely be needed to achieve the amount of housing desired. The plan recommends that the R-O district be re-evaluated and modified if needed, or a new zoning district created, to achieve the objectives in this plan (see Chapter 8).

Desired Outcomes

Figure 7 shows recommended building heights as a way to express the desired development densities in the River–Myrtle planning area. This map illustrates how higher densities would be concentrated in the following areas:

- Courthouse Corridor
- Front Street from 2nd to Capitol Boulevard
- Capitol Boulevard between Myrtle and Front streets
- Area bounded by Front, 9th, River and 11th streets
- Area bounded by Grove, 9th, Front and 13th streets
- Three areas where higher intensity, mixed-use development is anticipated:
 - Pioneer Corridor between the Boise River and River Street

- Area bounded by River (formerly Battery) Street, Capitol Boulevard, Boise River and 9th Street
- Broadway between Front and Myrtle streets

The map also illustrates that lower densities would occur in the areas anticipated to be residential neighborhoods, properties in the Warehouse/Cultural District in and adjacent to the historic district, and properties west of 13th Street.

Design and Development Guidelines

3.1 Use the Built Form map (Figure 7) to guide decisions regarding development intensities when entering into development partnerships and commenting on development proposals.

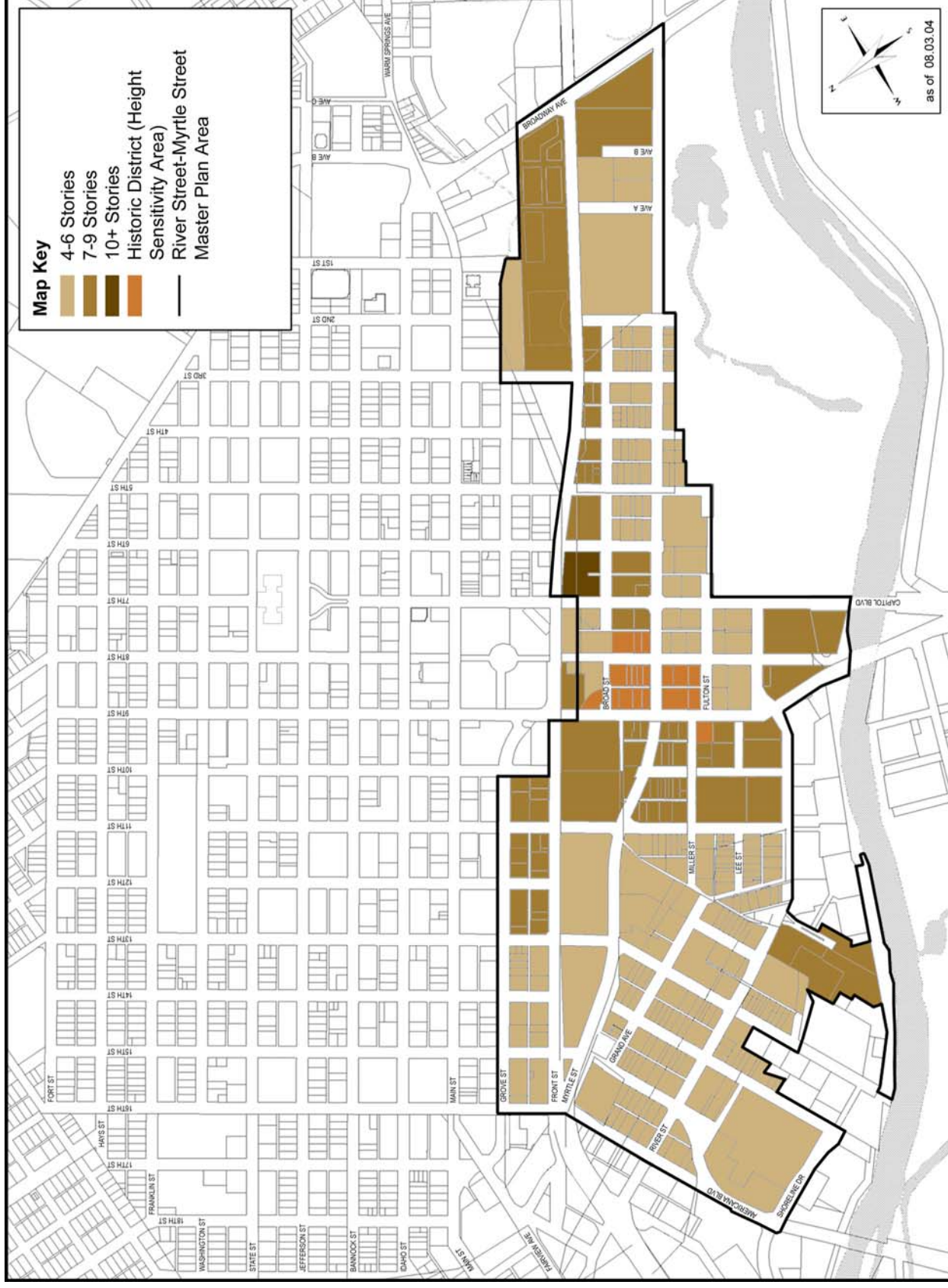
3.2 Apply the Design and Development Guidelines regarding building and site design contained in the 2025 Downtown Boise Redevelopment Plan to development proposals in the River–Myrtle planning area.

Action Steps

- Evaluate whether the existing R-O zoning district needs to be revised to better support plan goals for an urban level of intensity in the River–Myrtle planning area. Propose revisions to R-O or develop a new zoning category if needed to allow higher development densities particularly for housing.

¹ Floor area ratio (FAR) is the ratio of gross floor area of a building to the total land area of the site on which the building is located. In zoning regulations, limits on floor area ratios are used as a way to control the amount of building area constructed on a given building site. For example, a limit of 4:1 on floor area ratio means that four square feet of built for every one square foot of site area.

Figure 7: Built Form



- Re-evaluate the use of the R-O zoning district in areas where actual development consistently exceeds the typical density limits for this district, and determine if these areas should be rezoned to a different zoning category. Where warranted initiate a rezoning proposal for consideration by the city council.
- Evaluate if minimum densities need to be added to zoning categories affecting the River–Myrtle planning area to achieve plan goals for development densities, and propose zoning revisions as needed.

4.0 *Street Character, Civic Spaces and Pedestrian Ways*

Places are shaped not only by buildings but also by the public realm that forms the setting for buildings. The public realm includes streets, sidewalks, parks, plazas and other public places. The public realm can become a focal point that lends identity to a place. Public places can contribute to the beauty, enjoyment, livability and vitality of their surroundings when they are designed and located well. Improvements to the public realm can have a beneficial effect on the desirability of a particular area and serve as catalysts for development—such as the addition of a park to a neighborhood.

Street Character

How street rights-of-way are designed and improved affects what types of land uses are likely to locate along them, whether pedestrians and bicyclists feel comfortable using them and the ambience of the surrounding area. Right-of-way improvements include how the street itself

is designed and improved and how the area between the curb and the private property line is treated, otherwise known as streetscaping. Streetscaping includes how the sidewalk is paved and the use of street trees, lighting and furnishings. Adjusting how streets are designed and improved can influence the land-use mix and character of the surrounding area, and improve its livability for people. The River Street–Myrtle Street Master Plan includes recommended streetscape treatments for each street in the River–Myrtle planning area. These recommendations are based on how the street functions, the existing character of the area surrounding the street or the desired future character for this area. In most cases, a street in the River–Myrtle planning area has more than one designation because the purpose of the street and character of its surroundings change along its route. Figure 8 shows the streetscaping treatments recommended for each street in the River–Myrtle planning area. These are described under Desired Outcomes – Street Character.

Existing Conditions – Street Character

The River–Myrtle planning area has the full range of street types. At one end of the spectrum, it has tree-lined neighborhood streets that are comfortable for pedestrians and bicyclists to use and where traffic moves slowly. These streets work well with residential development of all types, low- to medium-density office and neighborhood retail. On the other end of the spectrum, the planning area is traversed from east to west by the Connector, which consists of two five-lane streets—Front and Myrtle—paired as a couplet. These major thoroughfares provide excellent vehicular access and visibility but are intimidating for pedestrians and bicyclists. They also exert a strong influence on the types of land uses that are developing in the Courthouse Corridor. The severity of Front Street has limited

adjacent new development to high-intensity office and institutional uses and a warehouse grocery store. The only new residential development—Civic Plaza Apartments—is actually set back from Front Street by 120 feet. Retail space built as part of the Courthouse Corridor project has been very difficult to lease. One of the reasons is that high-volume streets such as Front Street do not work well as local or community shopping streets. Major mixed-use and destination-type retail centers that exert a regional draw, however, could work on the Connector. They benefit from the ease of access and are strong enough to draw people off the Connector and create their own internal sense of place.

Americana Boulevard and 16th, 15th and River streets are examples of streets that fall between the two ends of the spectrum. They are wider, higher-volume streets that have sidewalks, and except for River Street, have bicycle lanes. Medium- to high-density office, residential and community commercial uses work well with these types of streets.

The streetscapes in the River–Myrtle planning area vary in quality, ranging from excellent to poor. In many cases the quality is uneven within a block or between blocks on the same street. There is little consistency. Portions of 14th, 13th, 12th, Grand, Ash, Miller, Lee and 4th streets have wide tree lawns with mature trees. There are mature trees in tree grates along 8th Street from Front Street to River (formerly Battery) Street. New street trees and sidewalks have been installed with recent developments such as Pioneer Plaza, River Plaza, Shoreline Plaza, Orthopedic Surgery Center, CW Moore Building, Ada County Courthouse and Administration Building, Idaho Water Center and Kreizenbeck Building. On the other hand, there are streets lacking street trees in all of the subdistricts. There are areas in the Warehouse/Cultural District

that lack sidewalks. There are also sections of sidewalk in this subdistrict that have stamped-concrete paving that is deteriorating to the point where it is difficult to walk on.

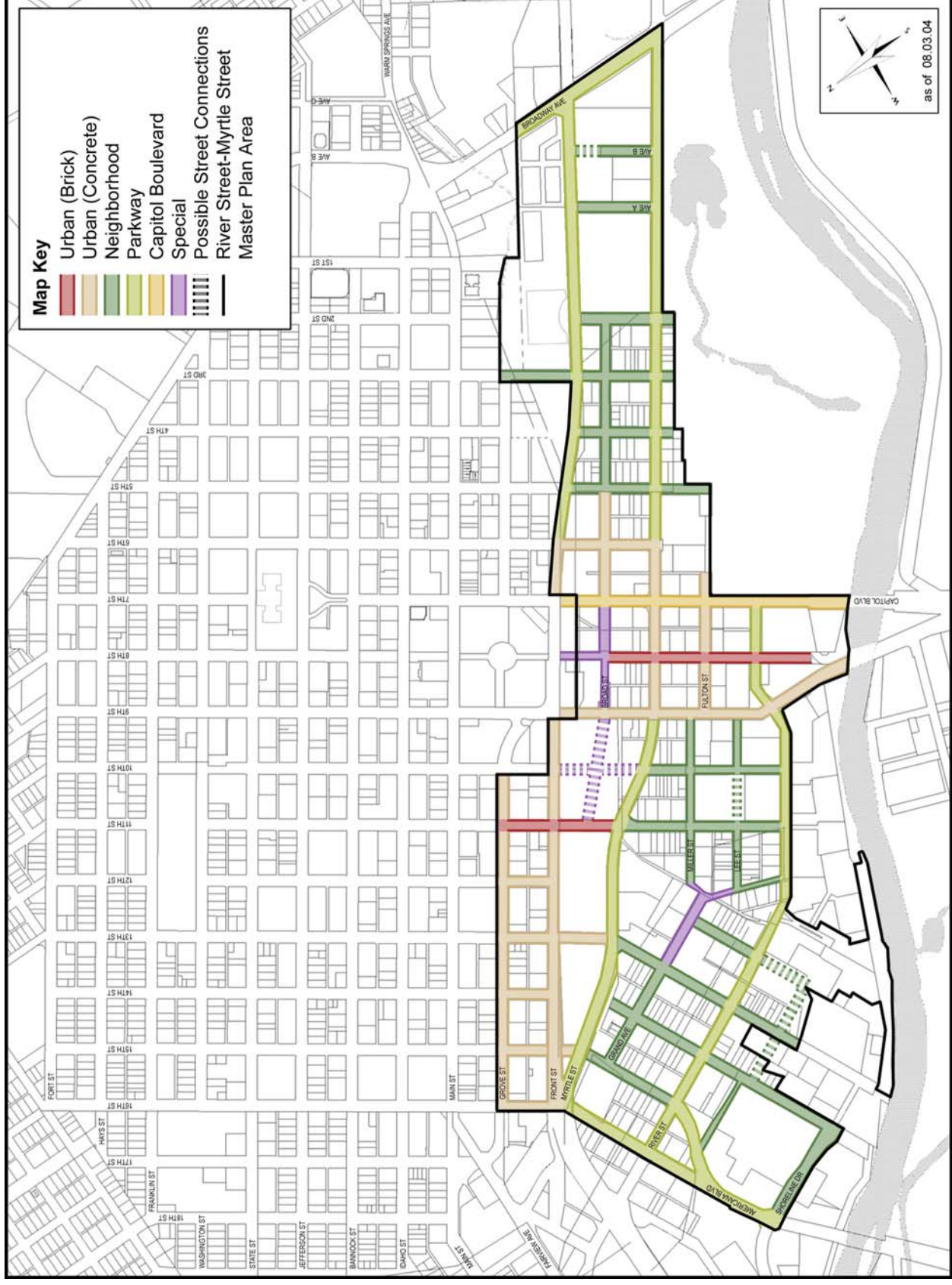
Desired Outcomes – Street Character

Achieving the preferred development concept for the River–Myrtle planning area depends in part on creating an attractive setting for the types of land uses identified as desirable in this concept, and on encouraging new development and redevelopment in general. The preferred development concept emphasizes creating subdistricts with a mix of uses such that people are able to live, work, shop, socialize and play in downtown Boise. So it becomes important to create streets that welcome a wide range of uses. This plan gives attention to how streets are improved so they will make a positive contribution to creating this setting and to the livability of the areas surrounding them.

The 2025 Downtown Boise Redevelopment Plan sets standards for the different streetscape treatments used in the downtown urban renewal districts. These standards address the width and types of paving used, spacing of street trees and historic street lights and the placement of planters, benches and trash containers. They also specify if paving is used from the building face to the curb (considered an urban treatment) or if a tree lawn is used between the sidewalk and the curb (considered a neighborhood treatment). Streetscapes treatments are designed to complement the existing character of an area or are used to transform the character of an area as it develops or redevelops. Descriptions of the types of streetscape treatments used in the 2025 Downtown Boise Redevelopment Plan are provided below. Figure 8 shows where these treatments are expected to be used in River–Myrtle planning area.

- **Urban:** The urban-style streetscape is generally used in business and mixed-use districts where there is a concentration of higher-intensity development, ground floor retail, restaurants and entertainment uses and on-street parking. Urban sidewalks are designed for heavy pedestrian use and are often lined by retail or similar uses in a dense urban environment. They may also serve offices and housing at street level when such uses are located in business or mixed-use districts rather than neighborhoods. The urban streetscape standard for downtown Boise has either concrete or brick paving from the inner edge of the sidewalk (usually at the building line) and the curb. Brick is used in areas where there is particular emphasis on aesthetics in order to create a distinctive place within the larger downtown.
- **Neighborhood:** The neighborhood-style streetscape is used in areas of downtown that have less intense development with a higher proportion of residential uses and more limited retail and office uses. It is designed to accommodate pedestrian traffic in locations where a residential neighborhood character is desired. The distinguishing characteristic of this style of streetscaping is the use of tree lawns between the sidewalk and the street, rather than continuing the sidewalk surface to the curb. The intent of the neighborhood streetscape is to create a pleasant pedestrian environment and encourage walking as an alternative to using an automobile. The neighborhood streetscape is compatible with housing, office and neighborhood-level retail uses.
- **Parkway:** The urban parkway treatment is used on major streets that carry higher volumes of traffic at faster speeds than is typical for the downtown core. The intent of using a parkway treatment is to provide beautification of significant streets such as State Street and the Connector and create both a comfortable walking environment for pedestrians and an enjoyable driving experience. Harrison Boulevard through the North End neighborhood is an example of a parkway street that accomplishes all of these goals. The streetscaping standards for parkway streets include wider sidewalks and use a double rather than single row of street trees to make a strong visual statement and provide a buffer between pedestrians and relatively heavy auto and truck traffic.
- **Capitol Boulevard:** Capitol Boulevard has a special status in the Boise City Zoning Ordinance, which establishes the Capitol Boulevard Special Design District. This design district includes a description of the streetscaping to be done along its edge.
- **Special:** The special street designation is used for particular street segments where an extraordinary pedestrian emphasis and placemaking effort is planned and warranted. A special street may function as both a street and also as a public space for sidewalk cafes, street fairs, festivals, cultural and special events. The best examples in downtown Boise are 8th Street between Bannock and Main and the Basque Block on Grove between Capitol Boulevard and 6th Street. It may be open or closed to vehicular traffic, or may be designed to allow temporary traffic closures. Each special street is expected to have a unique design often celebrating a particular theme.

Figure 8: Streetscape Character



Civic Spaces & Pedestrian Ways

Existing Conditions – Civic Spaces & Pedestrian Ways

Figure 9 depicts the existing civic spaces in and adjacent to the River–Myrtle planning area. The most significant public spaces include Julia Davis and Ann Morrison parks, the Boise River and the greenbelt along its banks. They represent invaluable assets to the River–Myrtle planning area because of their proximity and the incomparable facilities and natural features they offer. The Boise River is a year-round river bordered by a 70-foot wide greenbelt and pathway for walking and bicycling that extends 26 miles. Julia Davis and Ann Morrison parks contain extensive green space, totaling 133 acres within walking distance of the River–Myrtle planning area. Ann Morrison Park includes athletic fields, tennis courts, disc golf course, a playground and picnic sites. Julia Davis Park includes major cultural facilities for the city and state—the Boise Art Museum, Idaho State Historical Museum, Idaho Black History Museum, Zoo Boise, Discovery Center of Idaho and the Gene Harris Bandshell as well as tennis courts, playground, picnic sites, rose garden and lagoon with paddle boats.

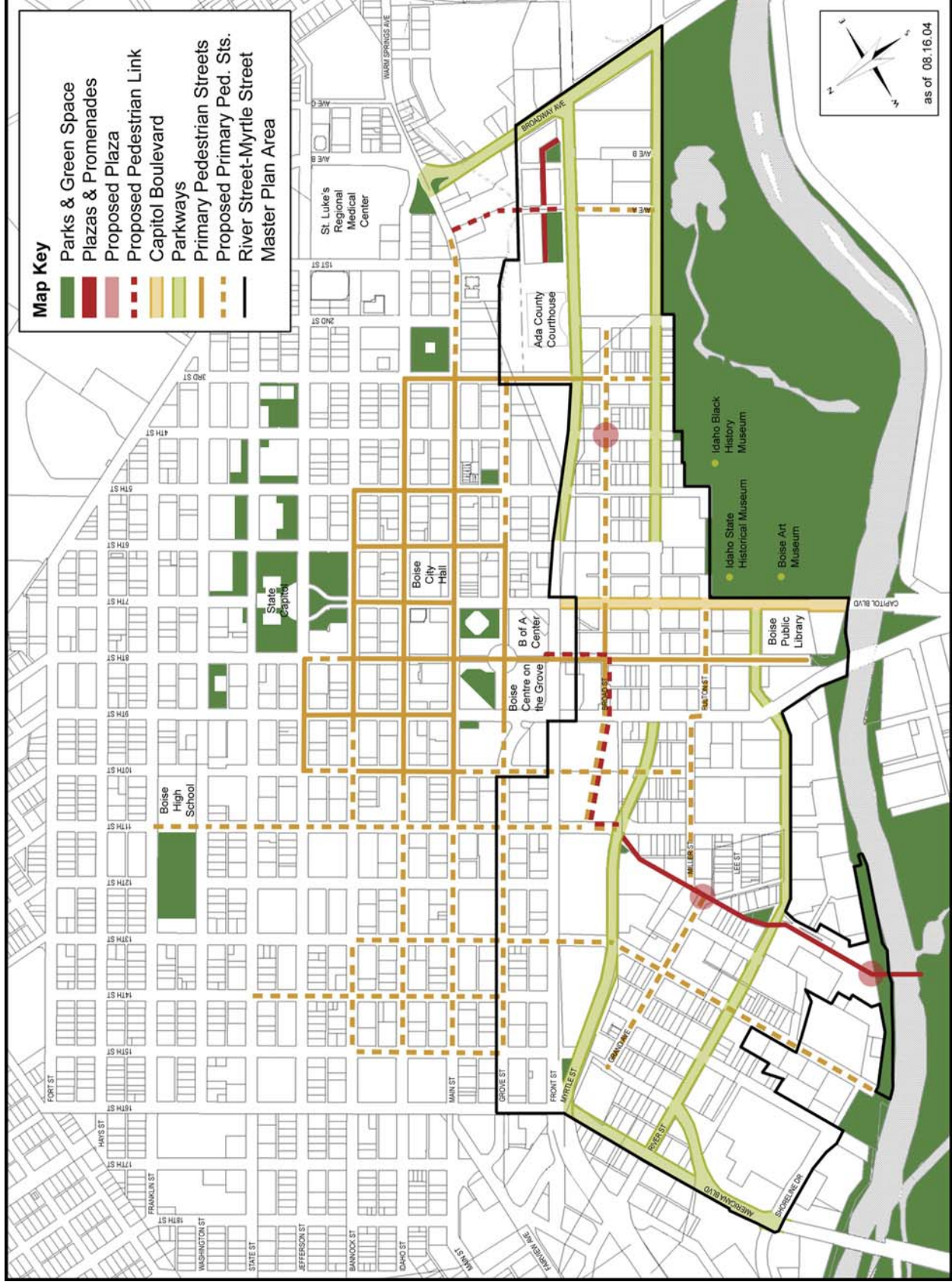
Within the River–Myrtle planning area, the public realm is much more modest and needs attention. There is a tot lot at the intersection of Grand Avenue and Ash Street in the River Street neighborhood. It includes playground equipment for young children but is on a very small site. The Pioneer Walkway—an asphalt walking path—runs from the Boise River through the River Street neighborhood to the intersection of 11th and Myrtle. It provides an off-street walking and bicycling route but it does not have a strong presence and

its northern end appears to disappear into an apartment development; consequently, it is not well-known or well-used. The Log Cabin Literary Center and Anne Frank Human Rights Memorial are located in the Cultural District at the south end of 8th Street. The grounds provide green space, seating, a beautiful water feature and food for thought in the form of quotations on human rights. There is a plaza at 8th and Front streets, but it lacks amenities that would attract people to use it for anything more than a walking route to other destinations.

The Courthouse Corridor contains several public spaces that add to the ambience of this area. These include the plaza in front of the Ada County Courthouse at 2nd and Front streets and the pedestrian promenade that connects the county building to the Idaho Water Center at Front and Broadway. The courthouse plaza is paved with brick and includes a cascading water feature. The pedestrian promenade has been improved with brick pavers, planters, seating and lighting that create an attractive place for people to gather. Civic Plaza Apartments face onto the promenade and so the residents have the benefit of this public space at their doorstep. Across the promenade from the apartments, one of the building sites in the Courthouse Corridor is covered by lawn for now, and so it provides green space, albeit temporary in nature. It is unknown if residents in the apartments, workers in the area or people in general will be able to use this green space other than as visual open space. There are no public spaces in the West Connector and East Connector/Parkside subdistrict beside street rights-of-way.

Eighth Street functions as the primary north-south pedestrian and bicycling route through the River–Myrtle

Figure 9: Civic Spaces



planning area, and as such contributes to the people-oriented public space in this area. Certain other streets could become a network of pedestrian-oriented routes with additional improvements. The existing and potential primary pedestrian streets are shown in Figure 9. The Pioneer Walkway that runs from the Boise River Greenbelt at approximately 12th Street to the intersection of Myrtle and 11th streets is a modest asphalt pathway at present, but has significant potential to become a grand, boulevard-style pedestrian and bicycle link through the River Street neighborhood.

Desired Outcomes - Civic Spaces & Pedestrian Ways

The River–Myrtle planning area would benefit from more attention to the public realm. The desired character for this area would include celebrating Americana Boulevard, Broadway Avenue, and 15th, Front and Myrtle streets as gateway streets by transforming them into urban parkways. They would remain primary routes for vehicular traffic but richer, more systematic streetscaping would improve the pedestrian environment on these streets. Capitol Boulevard would receive special treatment befitting its ceremonial status as the connection between the Boise Depot and the Idaho State Capitol Building. Local streets in the River Street and Parkside neighborhoods would be lined with trees and tree lawns on a more consistent basis, creating a lush, green environment and a respite from the more formal urban places elsewhere in the planning area. This streetscaping treatment would signal a shift in atmosphere and provide an attractive setting for new residential development, and make existing residences more desirable.

The Pioneer Corridor would be improved to its potential consistent with the concepts developed in the winning design from the Pioneer Corridor design competition. It would provide structure and a sense of place for the neighborhood and a catalyst for new development and redevelopment. It would be extended from the intersection of 11th and Myrtle along 11th and Broad streets and then along 8th Street to The Grove Plaza—creating a continuous pedestrian link from the Boise River to the CBD.

New civic spaces would be added at neighborhood focal points, along the Pioneer Corridor and in the Courthouse Corridor. These civic spaces would provide refreshing and delightful gathering places for people to enjoy and would help create a sense of community in the various subdistricts.

Figure 9 shows both the existing and proposed civic spaces in and adjacent to the River–Myrtle planning area. These include parkway streets, primary pedestrian streets, parks, plazas and promenades and pedestrian links.

The guidelines that follow focus on general principles regarding street character, civic spaces and pedestrian ways. They also include specific guidelines for the Connector and Capitol Boulevard. Design parameters for these two streets appear here because they are located in more than one subdistrict. (Section 5.0 includes additional guidelines and action steps regarding these streets from a transportation standpoint.) The subdistrict plans describe in more detail where new civic spaces should be added to the River–Myrtle planning area and action steps for getting them built.

Design and Development Guidelines

Street Character

4.1 Create a hierarchy of streets that serves the needs of the River–Myrtle planning area not only by traffic capacity but also by design treatment of sidewalks, crosswalks, streetscapes, lighting and furnishings.

4.2 Use streetscaping to create a street character that is compatible with desired land uses so street design supports implementation of the preferred development concept.

4.3 Use the Streetscape Character map (Figure 8) to identify the type of streetscape treatment required for the streets in the River–Myrtle planning area. Apply the streetscape design standards from the 2025 Downtown Boise Redevelopment Plan when streetscapes are improved in the River–Myrtle area. Implement these standards at the time properties are developed or redeveloped and as part of the capital improvement program for the River Myrtle–Old Boise Urban Renewal District.

4.4 Use streetscapes to create pedestrian-friendly streets with continuous tree coverage especially in areas planned to be residential neighborhoods.

4.5 Find an appropriate balance between the Connector’s roles as a vehicular route through downtown and as an element of the public realm. Minimize the tendency of the Connector to divide downtown and interrupt pedestrian and bicycle routes. Make design and streetscaping improvements to the Connector to create a more people-friendly environment along its edge and at crossing points.

4.6 Transform the Connector into a set of boulevard-style streets through improved roadway design and streetscaping that provide a calmer, more enjoyable driving experience and improve the pedestrian environment along their edges.

4.7 Work with property owners along the Connector, and consider acquisition of additional right-of-way, to create a series of open spaces along its length that will vary the visual width of the corridor, add visual interest and slow driving speeds so the Connector becomes more integrated with the surrounding urban environment.

4.8 Improve Capitol Boulevard as a grand, ceremonial-style street consistent with the Capitol Boulevard Plan and Special Design District.

Civic Spaces & Pedestrian Ways

4.9 Create civic spaces in each of the subdistricts in the River–Myrtle planning area that will become focal points, promote urban vitality and serve as catalysts for development and redevelopment. Locate, design and furnish public spaces so they attract people to use and enjoy them.

4.10 Focus on the locations identified for civic spaces in the subdistrict plans as having highest priority for design and implementation. Refer to the guidelines in the subdistrict plans for specific guidance on the purpose of these spaces, and how they should be designed and developed (see Chapter 7).

4.11 Wherever feasible, convert the open spaces along the Connector described in Guideline 4.7 into civic spaces where people activity would occur. Use these

spaces to create a series of places along the Connector, moderate its linear nature and integrate it into the fabric of downtown Boise.

4.12 Make the Connector conducive to pedestrian use by adding signals that allow more frequent, protected crossing points. Place signals at the same cross streets on both Front and Myrtle so protected pedestrian and bicycle routes are created.

4.13 Encourage the inclusion of well-designed and functional public space in private developments.

Action Steps

Street Character

- Create a capital investment plan for streetscaping in the River–Myrtle planning area, including an estimated schedule for construction. Use this schedule to identify projects for inclusion in CCDC’s annual budget and proceed with construction as funding permits.
- Determine if current setback requirements along the Connector allow a boulevard-style treatment to be implemented, and modify them if necessary to achieve this objective.
- Evaluate requiring a minimum 15- to 20-foot building setback on Front Street to allow for an 8-foot sidewalk flanked on either side by a row of street trees in order to create a more appealing walking environment for pedestrians. Increase sidewalk widths in areas having high pedestrian volumes.

- Evaluate requiring a minimum 15- to 20-foot building setback on Myrtle Street to allow for a 6-foot sidewalk flanked on either side by a row of street trees in order to create a more appealing walking environment for pedestrians. Increase sidewalk widths in areas having high pedestrian volumes.
- Convert setbacks along the Connector to build-to lines to assure that development is placed at the edge of the streetscaping treatment on these streets. (See the 2025 Downtown Boise Redevelopment Plan for specific streetscaping standards for the Connector.) Allow for exceptions to create plazas and other outdoor civic spaces that enhance the people environment.
- Work with Boise City to add the revised Capitol Boulevard Design Standards into the Capitol Boulevard Design District regulations and obtain approval for a memorandum of agreement among Boise City, CCDC and Ada County Highway District (ACHD) to implement these standards. Create an implementation plan that includes identifying funding sources and a schedule for construction.

Civic Spaces & Pedestrian Ways

- Create a capital investment plan for civic spaces in the River–Myrtle planning area, including an estimated schedule for construction. Use this schedule to identify projects for inclusion in CCDC’s annual budget and proceed with construction as funding permits.

- Refer to subdistrict plans for specific action steps regarding civic spaces and pedestrian ways (see [Chapter 7](#)).

5.0 Transportation & Parking

Transportation systems provide the means by which people make connections between where they live, work and carry out other activities in their lives. Parking becomes an essential part of the transportation system when people travel by car. How these systems function affects the quality of life for people, costs to taxpayers and consumers and the health of the environment. This section describes the existing situation and the desired future outcomes related to transportation and parking for the River–Myrtle planning area. These descriptions are followed by objectives, guidelines and action steps designed to guide future decisions regarding transportation and parking in this area of downtown so needs for these services and facilities are well-served.

Transportation

Existing Transportation Conditions

The River–Myrtle planning area has an extensive road system, ranging from neighborhood streets to the Connector, which consists of two five-lane roadways and is part of the federal highway system. The Idaho Transportation Department is responsible for design, construction and maintenance related to the Connector, but has delegated traffic operations on this thoroughfare to ACHD. All other streets are owned, operated and maintained by ACHD.

In addition to the Connector, there are several major streets capable of carrying higher volumes of traffic and providing direct routes to, from or through the planning area. These include:

- Capitol Boulevard and 9th Street, which run north and south through the center of the planning area and are paired as a couplet
- Americana and 15th, which run along the western edge and are paired as a couplet
- Broadway along the eastern edge
- River Street, which runs east and west along the lower edge of the planning area

Within portions of the West Connector and the River Street Neighborhood subdistrict and in the entire Warehouse/Cultural District and the East Connector/Parkside subdistrict, there is a grid street system that goes back to the earliest recorded plats in Boise. Within this system, there are several local streets that provide through routes that are much quieter and more pleasant than the higher-volume streets such as the Connector and 9th and Capitol. These include 3rd, 8th, 11th, 13th, Grand Avenue and Miller, Fulton and Broad streets. The River Street–Myrtle Street Master Plan envisions these people-friendly streets forming a network that would allow local traffic to flow within and between subdistricts without drivers having to use the larger, more congested streets. They would also serve as pedestrian and bicycle routes that would encourage people living and working in the River–Myrtle planning area to travel by these alternative modes. (See Figure 9 for the location of existing and proposed primary pedestrian streets and an illustration of the network that they form.)

Levels of Service (LOS)

Level of Service or LOS is a term used by traffic engineers to qualitatively describe operating conditions for automobile travel on existing or planned roadways. Typical traffic characteristics described by LOS include: speed, travel time, maneuverability, traffic interruptions, driver comfort and driver convenience. Similar to a report card, LOS grades roadway conditions from "A" to "F" with "A" representing the best driving conditions and "F" the worst. LOS E represents the capacity threshold of a given roadway. The LOS grades for roadway facilities are generally defined as follows:

- LOS A represents free-flow travel with an excellent level of comfort and convenience and the freedom to maneuver.
- LOS B has stable operating conditions, but the presence of other road users causes a noticeable, though slight, reduction in comfort, convenience and maneuvering freedom.
- LOS C has stable operating conditions, but the operation of individual users is substantially affected by the interaction with others in the traffic stream.
- LOS D represents high-density, but stable flow. Users experience severe restriction in speed and freedom to maneuver, with poor levels of comfort and convenience.
- LOS E represents operating conditions at or near capacity. Speeds are reduced to a low but relatively uniform value. Freedom to maneuver is difficult with users experiencing frustration and poor comfort and convenience. Unstable operation is frequent, and minor disturbances in traffic flow can cause breakdown conditions.
- LOS F is used to describe forced or breakdown conditions. This condition exists wherever the volume of traffic exceeds the capacity of the roadway. Long queues can form behind these bottleneck points with queued traffic travelling in a stop-and-go fashion. Signalized intersections do not clear in one signal cycle.

Desired Transportation Outcomes

The long-term forecast for future development in downtown Boise anticipates a substantial increase in office, retail and residential development in the River-Myrtle planning area by 2025 (see Chapter 4). This forecast was prepared as part of the Downtown Boise Mobility Study (DBMS) in 2004 and used to model future traffic demand on the downtown street system.

The DBMS proposes a policy that a level of service (LOS) of D is acceptable during peak hours on downtown streets (see sidebar). This policy recognizes that downtown is more congested than is typical in other areas of Boise, and a certain level of congestion is an acceptable trade-off in order to maintain downtown Boise as the region's urban center. The traffic analysis showed that a number of intersections in the River-Myrtle planning area will have a level of service (LOS) of E or F in 2025 during peak hours. These include all intersections on Front and Myrtle except 11th and Myrtle, Grove and 9th, Grove and 6th, Main and 9th, Main and 6th, Broadway and University, Lincoln and University, Capitol and University, River and 9th and Americana and Shoreline.

Most of the congestion predicted by the traffic model is associated with commuter traffic between the region and downtown since downtown is expected to continue growing as an employment center.

Creating a multimodal transportation system for downtown Boise that encourages use of transit, bicycling and walking as alternatives to the use of personal vehicles is one way to address this situation. If downtown continues to grow as the employment, educational

and cultural center of the region, it will cause more and more people to travel there. The DBMS acknowledges that for the foreseeable future, most of the people coming to downtown Boise will travel by automobile. In the short term, the DBMS advocates creating a downtown transportation system will allow people to arrive in downtown, park their car once, and travel in downtown using other modes—walking, bicycling or transit. It recommends improvements to streets so they are safer and more attractive as pedestrian and bicycle routes and establishing a wayfinding system that would encourage downtown residents, workers and visitors to walk or bicycle on these streets instead of using their cars. The DBMS also recommends the creation of downtown circulator that would connect together the geographic areas and activity centers in downtown. A portion of the proposed downtown circulator route uses Capitol Boulevard and 9th Street and goes through the River-Myrtle planning area.

Protecting bicycle commute routes from neighborhoods on the periphery of downtown is a second way to encourage the use of alternative modes. Downtown Boise is surrounded by a number of well-established neighborhoods that are within a practical distance for bicycle commuting. Creating routes that make bicycle commuting attractive is important to promoting this method of travel. Many routes are available from neighborhoods on the north and west side of downtown. Routes to neighborhoods on the east and south are much more constrained. For example, for neighborhoods south of the Boise River and on the Boise Bench, there are only three main crossing points over the Boise River into downtown: Broadway at the east end of the River-Myrtle planning area, Americana at the west end

and, in the center, the three bridges carrying 9th Street, 8th Street and Capitol Boulevard. There are also two pedestrian bridges that provide a means of crossing the Boise River from the south.¹ For people crossing at the center point, using 8th Street as the route to and from downtown is the most practical choice because of heavy traffic on both Capitol Boulevard and 9th Street. Retaining 8th Street as a bicycle route, therefore, becomes important to promoting bicycle commuting. (The same is true for retaining bicycle lanes on 13th and 15th through the North End neighborhood, on Bannock to the Veterans Park neighborhood, and on Warm Springs Avenue through the East End neighborhood.) Eighth Street runs through the center of the River–Myrtle planning area and so this plan addresses the importance of retaining 8th Street as a bicycle route in the guidelines below.

Improving the regional transit system is a third way to reduce traffic congestion in downtown. Having a more wide-reaching and effective transit system will allow commuters to leave their car at home and travel to downtown by commuter rail, light rail, commuter bus or vanpools.

Establishing a balanced mix of land uses so that people can live, work, shop, socialize and play without having to use a car is a fourth way to reduce not only traffic congestion but the amount of capital expended on transportation infrastructure. The River Street–Myrtle

Street Master Plan advocates creating a balanced land-use mix in the River–Myrtle planning area, and places particular emphasis on expanding the supply and range of housing to achieve this mix. The objective of building more housing in the River Street neighborhood, Warehouse/Cultural District and the Parkside neighborhood is both a transportation strategy and a land-use strategy.

Parking

Existing Parking Conditions

Most of the parking available in the River–Myrtle planning area is in surface lots. Extensive structured parking has been built in the Courthouse Corridor to support the Ada County Courthouse, the Civic Plaza apartments and the Idaho Water Center and additional development expected in the future. The City Centre Parking Garage at Front Street between 8th and 9th streets is in the Central Urban Renewal District, but its service area extends into the River–Myrtle planning area.

Desired Parking Outcomes

Surface parking lots tend to sap vitality from their surroundings. When there are numerous large-scale surface parking lots in an area, they tend to discourage pedestrian traffic because they are much less interesting to walk past than buildings. The parking strategy proposed in this plan is threefold:

¹ One of these bridges connects Boise State University (BSU) to Julia Davis Park about midway between Broadway Avenue and Capitol Boulevard, and the other connects Ann Morrison Park to the River Street neighborhood about midway between Americana Boulevard and Capitol Boulevard. Both of these bridges are out of the way for most commuters except that the bridge between BSU and Julia Davis Park provides a direct route for people traveling from school to downtown or living in the neighborhood south of Boise State University and working in downtown.

- Build public parking garages in areas where high intensity development or concentrations of people activity are expected. Public parking garages are generally available to any member of the public and serve short-term and monthly customers. They are best suited for use by workers, shoppers, visitors and apartment dwellers. They are not well suited to providing parking for homeowners who usually want reserved parking in a secured facility. The River Street–Myrtle Street Master Plan identifies five potential sites for additional public parking garages as shown in Figure 10. The site at on the northwest corner of Myrtle and Capitol is the most definite of these five since it is expected to serve the BoDo development. Others include the south end of the Cultural District, two sites in the West Connector subdistrict and a site on Miller Street between 10th and 11th streets .

Additional garages may be needed when the Pioneer Walkway is completed to serve the higher intensity development expected along this pedestrian boulevard, and for the mixed-use projects proposed by this plan at Broadway between Front and Myrtle and on the northeast corner of Americana and Shoreline.

- Rely on private parking garages built by developers. Private garages may be open to the public or reserved for use by people associated with a particular development.

- Rely on surface lots in outlying areas and for lower-intensity or smaller developments. It is likely that surface lots will continue to be built and used in the River–Myrtle planning area for the foreseeable future. Developers are required by Boise City to provide a certain ratio of parking with each new development unless public parking is readily available. Surface lots are the easiest and most cost effective way to meet parking requirements until land values rise to a certain point. Surface lots should be used sparingly and designed carefully so they do not sap the urban vitality of their surroundings.

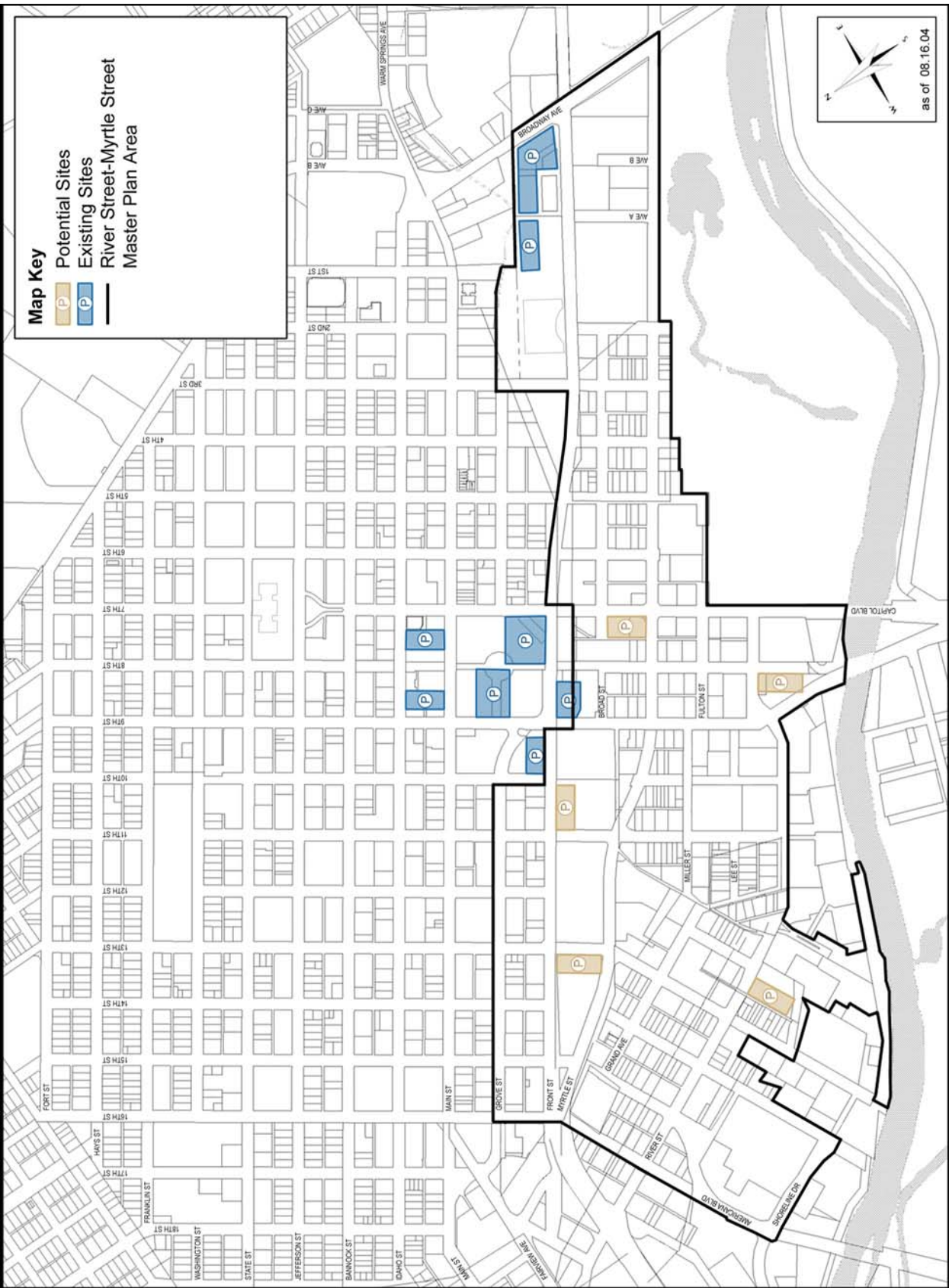
Figure 10 illustrates the existing and proposed public parking facilities in the River–Myrtle planning area and nearby vicinity. It illustrates the concentration of public parking garages in the Central Urban Renewal District and the Courthouse Corridor. Other facilities are proposed in the West Connector to serve a concentration of lodging and destination-style retail, restaurant and tourism uses, in the River Street neighborhood to serve the development along the Pioneer Corridor and in the Warehouse/Cultural District to serve the BoDo development.

Design & Development Guidelines

Transportation

- 5.1 Establish a multimodal system of transportation to serve the River–Myrtle planning area that encourages the use of transit, bicycling and walking as alternative to use of automobiles.

Figure 10: Public Parking Garages

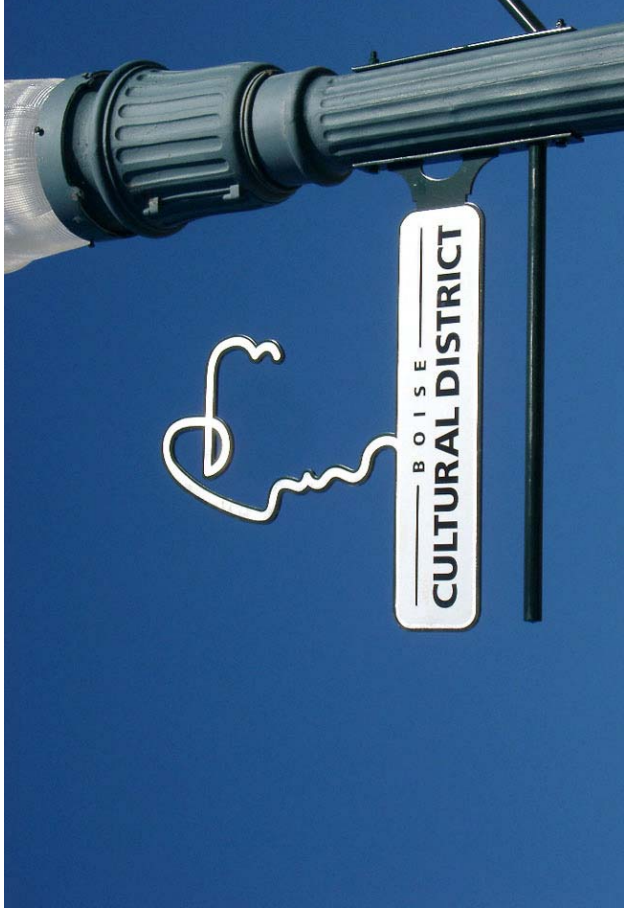


5.2	Pursue a balanced land-use mix, and increase the type and amount of housing available in the River–Myrtle planning area, to increase the likelihood that people can conduct their daily activities without having to resort to cars for transportation and can reduce the demand on transportation and parking facilities.	5.8	Evaluate Parcels A (bounded by 15 th , 13 th and the Connector) and Parcel B (bounded by 13 th , 11 th and the Connector) as a possible location for a multimodal center that could include a transfer point between regional and long-haul bus services, shuttle busses and vans, downtown and regional rail transit (if implemented), parking for commuters and other transportation services.
5.3	Focus on developing thriving residential neighborhoods in the River Street subdistrict, Warehouse/Cultural District and Parkside subdistrict to allow people both to live and work in downtown Boise and reduce demands on the downtown and regional transportation system.	5.9	Recognize that the primary function of the Connector is as a high-capacity roadway carrying traffic to, from and through downtown. Work with ITD and ACHD to include improvements to the Connector recommended by the DBMS in these agencies’ capital improvement plans.
5.4	Establish a downtown circulator that provides a transit alternative for people needing to travel in downtown, and provide service to the River–Myrtle planning area.	Parking	
5.5	Improve regional transit service to downtown Boise in general and to the River–Myrtle planning area in particular.	5.10	Emphasize the use of parking structures in areas having high-intensity development or concentrations of people activity, and whenever feasible to free land for more productive uses.
5.6	Establish a network of pedestrian-oriented streets between activity centers in the River–Myrtle planning area and in downtown, in order to encourage walking and bicycling over automobile use (see Figure 9).	5.11	Provide public parking facilities that are convenient for the public, located strategically to support desired land uses, and encourage people to park once and walk, bicycle or use transit to their various destinations in downtown.
5.7	Protect bicycle-commuting routes through the River–Myrtle planning area, with particular attention to 8 th Street, Americana and Broadway. Avoid changes in street design on these routes that would compromise their use for bicycle commuting.	5.12	Use structured rather than surface parking whenever possible in order to maximize the use of scarce land resources in the River–Myrtle planning area, create a pedestrian-oriented urban character and reduce the impact parking lots have on urban vitality.

5.13 Design parking structures so they include space for active uses along street frontages at ground level, or are screened from view from the street.	Create a route on the circulator system that provides service to the subdistricts in the River–Myrtle planning area.
5.14 Design parking structures to include architectural detailing above the street level whenever feasible so these structures contribute to the built environment in a positive way.	Support the creation of a dedicated source of funding for regional transit in order to improve service levels for downtown and for the River–Myrtle planning area.
5.15 Allow use of surface parking lots in outlying areas and for lower intensity developments with guidelines for their design and placement so their tendency to sap vitality from their surroundings is reduced.	Install a way-finding signage system that encourages downtown residents, workers and visitors to walk or bicycle in downtown rather than using cars. Use this way-finding system to link the subdistricts in downtown Boise to each other, to the CBD, Cultural District, shopping and entertainment districts, areas where lodging and convention facilities are located, sightseeing opportunities, parks and the Boise River Greenbelt.
5.16 Design surface parking lots so they are located out of view from the street whenever possible. When placement out of view is not possible, require perimeter fencing or an alternative treatment along street frontages that replicates the traditional street wall or building line on downtown blocks.	Establish a schedule for improvements to the Connector identified in the DBMS that will reduce traffic congestion on this thoroughfare, enhance the environment for pedestrians and bicyclists, and improve its interface with the urban fabric of downtown. Include the appropriate items from the schedule in the annual budgets for ITD and ACHD.
See also Design & Development Guidelines for the Connector and Capitol Boulevard under Section 4.0 and Design & Development Guidelines for parking facilities and surface parking lots in the 2025 Downtown Boise Redevelopment Plan.	
Action Steps:	
Transportation	
Pursue federal funding for implementation of the downtown circulator, and proceed with implementation as funding becomes available.	Utilize the inside lane on the West Connector as a turning lane with landscaped bulb-outs at intersections to manage street capacity and encourage lower driving speeds.

-
- Install traffic signals at 5th and Avenue B on Front; and at 3rd, Avenue A and Avenue B on Myrtle so continuous north-south routes for pedestrians and bicyclists are created through the Courthouse Corridor from Old Boise/Eastside to Julia Davis Park.
 - Install a pedestrian light at 2nd and Front to improve access to the Ada County Courthouse for people walking and bicycling and to reduce jaywalking across Front Street.
 - Time signals so they allow smooth traffic flow along the East and West Connector.
- Parking**
- Acquire sites and construct public parking garages as described in the subdistrict plans (see Chapter 7). Use the provision of public parking facilities to stimulate private development and support catalyst projects.
 - Expand the P-1 parking district to cover the service area of public parking garages when they are added to the parking system.
 - Work with owners of existing private parking lots to make them available in the evening and on weekends for public use to stretch parking resources and reduce the need to build parking facilities.
 - Work with Boise City to adopt design requirements for surface parking lots consistent with Guideline 5.16 above so surface parking lots will make a positive contribution to the urban fabric.
 - Pursue funding for and implement the downtown circulator recommended by the Downtown Boise Mobility Study including service to the River–Myrtle planning area.
 - See also Action Steps under Section 4.0 regarding the Connector and Capitol Boulevard.

7: Subdistrict Plans, Guidelines & Action Steps



The Boise Cultural District is one of five subdistricts in the River–Myrtle planning area. Each has its own character, opportunities and challenges.

The River–Myrtle planning area covers a large area—230 acres—and is the largest of the four planning areas in downtown Boise. It includes several well-defined subdistricts, each with its own character, opportunities and challenges. The River Street–Myrtle Street Master Plan was created by examining each of these subdistricts to understand its history, past planning efforts, current conditions, assets, influences at work, suitability for various land uses, availability of land for development and redevelopment and its likely role in the future of the River–Myrtle planning area. Plans for each of the subdistricts were developed based on this analysis, which are presented in this chapter. Five subdistricts have been identified:

- West Connector (Former Rail Yards)
- River Street Neighborhood
- Warehouse/Cultural District

- East Connector/Parkside Neighborhood
- Courthouse Corridor

The subdistrict plans in this chapter identify existing conditions, desired character, objectives, design and development guidelines and action steps for each subdistrict. The subdistricts are shown in Figure 11. These subdistrict plans work in concert with the overall plans that address key issues in the River–Myrtle planning area and cut across subdistrict lines. These issues include:

- Preferred Development Concept
- Preservation, Revitalization & Renewal
- Building & Site Design
- Street Character, Civic Spaces & Pedestrian Ways
- Transportation & Parking

2025 Downtown Redevelopment Plan

The 2025 Downtown Boise Redevelopment Plan is a companion document to the master plans for the downtown urban renewal districts. Each of the master plans addresses key elements such as land use, built form, street character and civic spaces for a specific part of downtown. The 2025 Downtown Boise Redevelopment Plan consolidates this information on maps that show these elements on a downtown-wide basis—thus providing a more holistic view of what the eventual character, form and structure of downtown will be. The 2025 Downtown Boise Redevelopment Plan also includes maps showing pedestrian and bicycle routes, transit routes and parking facilities on an area-wide basis.

General design guidelines and standards for building and site design, civic spaces, streetscaping and urban design that apply to all of the approved urban renewal districts in downtown Boise are set forth in the 2025 Downtown Boise Redevelopment Plan. This approach saves duplication in each master plan. These general guidelines and standards supplement the guidelines and standards contained in Chapters 6 & 7 of this master plan. CCDC considers both the 2025 Downtown Boise Redevelopment Plan and the master plans for the individual urban renewal districts when making decisions regarding public investment and evaluating development proposals.

The plans addressing area-wide issues are presented in Chapter 6.

West Connector (Former Rail Yards)

Existing Conditions

The West Connector subdistrict runs approximately from 9th Street to 15th Street and from Grove Street to Myrtle Street. It comprises two parts:

- Three superblock-sized parcels between Front and Myrtle streets running from 9th to 15th Street that were occupied by rail yards from the 1890s to 1970s until the tracks were removed. They are referred to as parcels A, B and C-D going from west to east. For the most part they are vacant except for a warehouse at 9th Street on parcel D and paved parking lots on the remainder of parcels C-D and the eastern part of parcel B. Tenth and 12th streets do connect through these parcels.

- Five blocks between Front and Grove from 11th to 16th streets which are occupied by service and auto-oriented businesses or are in parking lots or vacant. This area is viewed as a transitional area.

Both areas have changed little since 1994 when the River Street–Myrtle Street Urban Renewal District was formed.

In 2002 the Greater Boise Auditorium District (GBAD) purchased Parcel B for an expansion of the existing convention center. Plans were prepared and approved by Boise City. The GBAD Board of Directors intended to finance the expansion by selling bonds backed by the

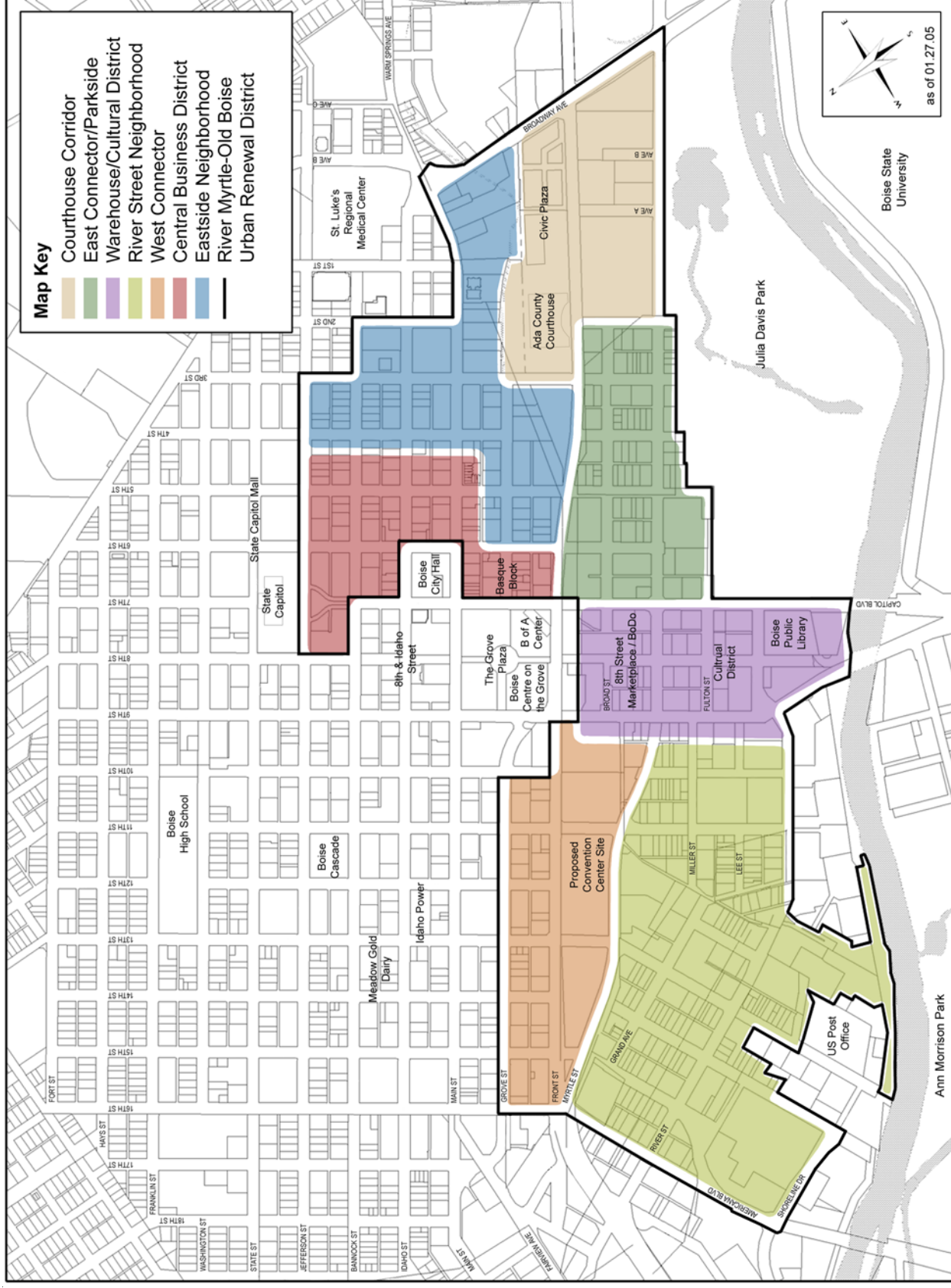
hotel/motel tax that funds the auditorium district. A ballot measure seeking voter approval for issuing the bonds did not pass. It is unknown if the property will be used for the convention center, so the catalyst effect of this facility cannot be assumed. Given this situation, it is necessary to look at alternatives for the future development of the West Connector.

Desired Character

If a new convention center is built and if it is successful, it is likely to stimulate development of hotels, restaurants, retail and other tourism-related uses in close proximity. Without a convention center, Parcel B and most likely Parcels C-D could still attract uses related to tourism given their location relative to the existing convention center and hotels, Bank of America Center, Cultural District and the destination-type retail center planned at 8th and Broad Street (BoDo). Additional destination-type retail, restaurant and entertainment businesses could also locate just west of BoDo. In either case, the challenge is how to provide strong pedestrian connections between these uses and the central business district, so this emerging activity center complements and supports the CBD rather than draining energy from it. The ideal result would be to create a sense of synergy between the CBD and this subdistrict rather than a sense of competition.

Parcel A is considered a possible location for a parking garage for incoming commuters and for uses on Parcels B and C-D, a multimodal station, or for service businesses serving the CBD and the West Connector subdistrict. The current zoning on this parcel is C-2, which allows auto-oriented commercial uses. This zoning designation should be modified to assure

Figure 11: River Myrtle–Old Boise Subdistricts



development will have an urban rather than suburban character. Attention should also be given to how buildings facing 13th Street are designed so they give a pedestrian-oriented feel to this street. Thirteenth Street has been identified as a pedestrian and bicycle route providing an important connection between the River Street Neighborhood, the Westside Downtown district and the CBD.¹

The blocks north of Front Street are quite different from Parcels A, B, and C-D because they are part of the downtown grid, with typical block sizes and street connectivity. They lie along the seam between the central business district, the proposed in-town neighborhood in Westside and the superblocks between Front and Myrtle. The three blocks between 10th and 13th include (from east to west) a full-block parking lot which is a prime development site, a block occupied by buildings originally used for auto-related businesses but which have been renovated into offices to accommodate creative and design-oriented businesses and a heating and air conditioning business. These blocks will most likely feel the ripple effects from creating an activity center focused on tourism and destination-type uses on Parcels B and C-D. There is also a possibility that this area could become a focal point for design-oriented businesses on the edge of the CBD.

The three blocks between 13th and 16th contain service businesses, vacant land and a city fire station. The Connector has a strong impact on the south half of these blocks while the properties on the north half face Grove Street, which is a relatively quiet street. This area could develop in several different ways. It could continue to provide a location for businesses needed by

downtown workers and residents but which are better located on the periphery of the CBD. Such businesses could include auto parts and repair, car wash and detailing, electrical and HVAC installation and repair, a locksmith—all of which are now present in this area. This cluster of service-oriented businesses could also extend south on properties facing 16th Street where Boise Cold Storage is currently located. The properties facing Grove Street could become part of an in-town neighborhood planned for the west end of the Westside District. It is difficult to predict what will happen in this transitional area, but is likely that development will reflect the fact that the parcels are on the edge of both the West Connector and the Westside District and are somewhat hidden by the ramp of the Connector.

The Broadway–Chinden Connector exerts a powerful influence on the types of development seeking to locate along these roadways and the connectivity between the CBD, the Westside Downtown district and the River Street Neighborhood. It also impacts the degree to which a pedestrian-friendly environment can be established along its edges. Integrating the Connector into the urban fabric of downtown while still maintaining its ability to provide access through and to downtown is a key issue addressed by the guidelines and action steps below.

Development Objectives

East of 11th Street, the West Connector will become a focal point for hotels, convention and conference facilities, tourism-related uses, destination-oriented retail, restaurants and entertainment as well as office uses. West of 11th Street, how properties develop will depend on whether the convention center expansion occurs on

¹ The Westside Downtown Urban Renewal District is located more or less between 10th and 16th streets and Washington and Grove streets.



Figure 11A: West Connector (Former Rail Yards)

Parcel B. If not, this parcel would most likely be developed in office and hotel uses related to the CBD or other destination-type uses drawn by excellent visibility from the Connector. West of 13th Street, the likely focus will be service businesses with the possibility of residential uses along the south side of Grove Street as part of a new in-town neighborhood in Westside. The West Connector could also include a design center or cluster of creative-oriented businesses in the blocks north of Front Street and west of 10th Street. Improvements will be made to the Connector to create an appealing pedestrian environment along its edges, to create better connectivity among the CBD, the Westside Downtown district and the River Street Neighborhood and to moderate driving speeds. The Pioneer Walk will become a boulevard-style pedestrian link from the Boise River Greenbelt to the Grove Plaza through the West Connector subdistrict using 11th, Broad and 8th streets. This subdistrict, with its large vacant parcels close to the CBD and the Connector, offers several logical locations for a convention center expansion including Parcels B and C-D.

Design & Development Guidelines:

Land Use & Built Form

WC-1 Utilize the Preferred Development Concept in Figure 5 to guide development patterns in the West Connector subdistrict, and in making decisions on public investments.

WC-2 Use the Built Form map in Figure 7 to guide building heights and development densities in the West Connector subdistrict. Achieve a building height and density of development that makes an appropriate transition from the CBD to the West Connector.

WC-3	Allow expansion of downtown commercial uses into the West Connector subdistrict in a manner that contributes to the synergy between the West Connector subdistrict, the CBD and the Warehouse/Cultural District.	WC-10	Discourage the construction of megastructures that fill entire superblocks unless this building type is absolutely essential to a particular use. Use building designs that break down megastructures into a series of building masses that are more human-scaled and less monumental.
WC-4	Encourage tourism-related and destination-type retail, restaurant and entertainment businesses to cluster in the West Connector subdistrict, especially between 9 th and 11 th streets. Promote an urban intensity of at least 2.0 FAR in this area.	Street Character & Connections	
WC-5	Strategically place tourism-related uses so they are within comfortable walking distance of each other and hotels, public parking and transit, and design connections so they are clearly marked, attractive and safe.	WC-11	Apply the Streetscaping Standards in the 2025 Downtown Boise Redevelopment Plan to streets in the West Connector subdistrict.
WC-6	Explore the possibility of creating a design center or cluster of creative businesses between 10 th , 13 th , Front and Grove streets.	WC-12	Apply the Design & Development Guidelines related to the Connector stated in Chapter 6, Street Character, Civic Spaces & Pedestrian Ways and Transportation & Parking.
WC-7	Encourage service businesses to locate in the West Connector subdistrict west of 13 th Street especially along Front Street.	WC-13	Re-establish the traditional street grid between Front and Myrtle by reconstructing 10 th , and Broad streets. If a street connection is not possible, establish pedestrian connections as an alternative.
WC-8	Examine the south side of Grove Street as a possible location for residential uses, which would then become the southern edge of the in-town neighborhood envisioned for the Westside Downtown district.	Pedestrian/Bicycle/Transit Connections	
WC-9	Identify appropriate sites in the West Connector subdistrict for expansion of the convention center when it is ready to proceed.	WC-14	Establish strong pedestrian, bicycle and transit connections between the West Connector subdistrict, the CBD and other subdistricts in downtown.
		WC-15	Recognize 10 th , 11 th , 13 th and Broad streets as primary pedestrian and bicycling streets.
		WC-16	Use 11 th and Broad streets to extend the Pioneer Walkway from its current terminus at 11 th and Myrtle through the West Connector subdistrict to the Warehouse/Cultural District, The Grove plaza and CBD.

Parking

WC-17 Apply the Design & Development Guidelines related to parking stated in Chapter 6, Transportation & Parking.

Action Steps:

Urban Design, Land Use & Zoning

- Work with property owners to prepare a specific plan for the superblocks between Front and Myrtle and 9th and 15th streets. Include GBAD and the developer of the BoDo retail center at 8th and Broad. Assume the mix of uses, building placement, orientation and massing, and connections between uses promote synergy and an appealing pedestrian environment.

- Use a design charrette or similar process to identify a suitable site for the convention center expansion for use when it is ready to proceed. Place the convention center expansion where it will maximize support of the CBD, other downtown retail centers, hotels, restaurants and tourism-related businesses. Consider Parcels B and C-D as possible locations to be evaluated through this design process.

- Propose rezoning for Parcel A (between Front and Myrtle west of 13th Street) from C-2 to a zoning category that assures development will have an urban rather than suburban or auto-oriented character for consideration by city council.

- Propose rezoning for the parcels fronting on Grove between 13th and 16th from C-2 to a zoning category that better supports plan goals for housing if this area is to become part of the Westside neighborhood.

Street Character & Connections

- Implement the streetscaping standards in the 2025 Downtown Boise Redevelopment Plan when street improvements are required as part of a development project or whenever improvements are made to streets in the West Connector subdistrict.

- Implement the Action Steps related to the Connector stated in Chapter 6, Street Character, Civic Spaces & Pedestrian Ways and Transportation & Parking.

- Work with property owners to re-establish 10th Street between Front and Myrtle and to install traffic signals at the Front/10th and Myrtle/10th intersections. If a street connection is not possible, establish a pedestrian connection on the 12th Street alignment.

- Work with property owners to re-establish Broad Street from 9th Street to 11th Street, and install pedestrian signals at the 9th/Broad and 11th/Broad intersections.

Pedestrian/Bicycle/Transit Connections

- Construct the Pioneer Walkway as a grand, boulevard-style pedestrian and bicycle link from the Boise River Greenbelt through the West Connector subdistrict on 11th and Broad streets to the Warehouse/Cultural District..
- Install streetscaping improvements on Broad Street that identify it as both an extension of the Pioneer Walkway and a primary pedestrian street. Design this street to encourage both walking and bicycling.
- Implement the downtown circulator to include a route along 9th Street that will link the West Connector subdistrict to the CBD and the other subdistricts in downtown.
- Evaluate Parcel A as a possible location for a multi-modal center that would serve as a connection point between the regional transit system and the downtown circulator.

Parking

- Build public parking garages to serve the west and east ends of this subdistrict, if needed to support intensive tourism-related businesses, and expand the P-1 parking district to cover the service area of these garages.
- Implement the Action Steps related to parking stated in Chapter 6, Transportation & Parking.

Economic Development

- Work with the Greater Boise Auditorium District (GBAD), Boise Convention and Visitors Bureau, Downtown Boise Association and area hotels to provide maps and information to visitors about the various subdistricts in downtown, sightseeing opportunities, and how to find shopping, dining and entertainment.

River Street Neighborhood

Existing Conditions

Much of the River Street Neighborhood subdistrict consists of a historic, turn-of-the century residential neighborhood. It is bounded by 16th Street and Americana Boulevard on the west, Myrtle Street on the north, approximately 10th Street on the east and the Boise River on the south. It currently lacks a cohesive, neighborhood image, however, as the original single-family housing stock has deteriorated and in some cases been torn down, land has been cleared and redeveloped with a scattering of fourplexes, garden-style and mid-rise apartment buildings and offices or has become parking lots or left vacant. The neighborhood is without a focal point or neighborhood shopping district. This area is bordered by warehouses with trucking and distribution uses to the east, a suburban-style office park—Forest River—on the south, a 25-35 year old auto-oriented commercial strip along Americana Boulevard on the west and the Connector on the north. Most of the single-family houses are rentals, with some landowners owning multiple properties, and many of these houses show signs of needing maintenance. They provide, however, a significant reservoir of affordable housing close to the central business district.

There is a concentration of facilities and programs in this subdistrict that address the needs of lower income households including the city's homeless shelter and soup kitchen, Head Start and several apartment developments that provide affordable housing units.

The River Street Neighborhood is in a very strategic location, given it is situated close to the recreational amenities of the Boise River Greenbelt and Ann Morrison Park but it is also within walking distance or a short bicycle ride to the Cultural District, Julia Davis Park and the CBD. It has excellent vehicular access to the Connector, River Street, Americana, 16th, 15th, 13th, 11th and 9th streets and Capitol Boulevard. Because of its size and its location south of both Front and Myrtle, which places it outside the immediate influence of the central business district, this subdistrict tends to be quieter and more conducive to re-establishing a residential neighborhood than in the subdistricts along the Connector. The River Street Neighborhood also has large tracts of vacant or underutilized land, which increase the potential for revitalization and transformation of this area.

A number of office developments have occurred in this subdistrict, exclusively on properties fronting either Myrtle or River streets, showing the importance placed on automobile access and visibility for this type of use. These include the Pioneer Plaza office buildings between 11th and 13th on Myrtle, medical office buildings between 9th and 11th streets, the Orthopedic Surgery Center at 14th and River, River Plaza office building at 13th and River, and the Forest River office park on the south side of River Street between 9th and 12th streets. The Family Fitness Center on River Street has been converted to a multipurpose facility occupied by the Anser Charter School, Bronco Elite Gymnastics and a dance academy.

Figure 11B: River Street Neighborhood



In 2001, CCDC conducted a design competition focused on the existing Pioneer Walkway and the properties surrounding it to identify ways to enhance this walkway so it becomes a prominent link between the Boise River Greenbelt, the River Street Neighborhood, the West Connector subdistrict and the CBD as well as a catalyst for development along its edges. The winning design envisions a grand, boulevard-style pedestrian and bicycle link along its current alignment from the Boise River to 11th Street where it would continue east on Broad Street (re-established through Parcels C-D) to 8th Street and then north to the Grove Plaza.

Desired Character

The Downtown Boise Plan (1993) identified this area as a prime location for additional residential development, which has been reinforced by a change in the zoning for the area to Residential-Office (R-O). The River Street–Myrtle Street Urban Design Plan (1994) includes as a guideline to re-establish a critical mass of residential uses at the core of the River Street Neighborhood. This 2004 River Street–Myrtle Street Master Plan places even greater emphasis on this idea, urging the development of a new in-town urban neighborhood with a range of housing choices. Most of the new housing is envisioned to be at a relatively high density and have a compact, urban form, in contrast to the modest single-family residences and garden apartments there today. Some opportunities may exist for housing at lesser densities at the west end of the neighborhood.

The winning design for the Pioneer Corridor identified the intersection of Grand Avenue and Miller and Ash

streets as the logical location for a plaza or similar public space to create a neighborhood focal point and as an anchor for a neighborhood-shopping district along Grand Avenue similar to Hyde Park. This design also suggests a public space for the larger community where the Pioneer Walkway intersects with the Boise River Greenbelt. The Pioneer Walkway would become an anchor for residential, office, retail and restaurant development. These ideas have been incorporated into this River Street–Myrtle Street Master Plan.

The western edge along 16th and Americana faces a major thoroughfare with high visibility to traffic and as such is more conducive to light industrial, service commercial and office uses than to residential uses. The parcels fronting on these streets would serve as a buffer to the neighborhood to the east. The property at 700 N.

Americana and 1500 Shoreline would be an exception. Taken together, these two parcels are approximately nine acres in size and represent a significant redevelopment opportunity. This site is envisioned as the location of a major mixed-use development including housing and office and possibly civic, educational and retail uses as well.

The eastern edge is somewhat fluid as to the types of uses that will develop, with different possibilities for the blocks between 9th and 11th streets. At present there are warehousing and storage businesses facing River Street. The parcel between 10th and 11th streets prevents Lee Street from continuing through to 9th Street. There are various uses including a high-tech firm, parking lot, dance club and medical offices facing 9th Street. The angle

of the Connector shortens the blocks on the south side of Myrtle between 9th and 11th streets. Most of the block between 10th and 11th south of Myrtle is vacant. This plan anticipates that office and commercial uses will continue to face 9th Street, but eventually the warehouse and storage businesses will relocate to less expensive ground west of the River—Myrtle planning area perhaps along Main or Fairview because the cost of using prime downtown real estate for such uses will become prohibitive. If these businesses relocate, the most likely scenario is that they will be replaced by offices or mixed-use development and the residential portion of the subdistrict would begin at 11th Street. It is possible that residential uses will extend eastward, in the half blocks facing Miller between 11th and 10th streets.

Office uses are expected to predominate along Myrtle Street to provide a buffer between the residential neighborhood and the Connector. If convention facilities are built in the West Connector subdistrict, it is possible that developers will seek out sites on the south side of Myrtle for hotel, restaurant and retail development. This plan discourages these uses from locating south of Myrtle and encourages them to locate between Front and Myrtle or north of Front to keep visitors in close proximity to both the CBD, the Old Boise Historic District, the Basque Block and the BoDo retail center at 8th and Broad streets. The primary reason for this approach is to prevent a drain on the vitality of the CBD. As noted above, office uses are also expected to occur along River Street, particularly between 9th and 11th streets, where they would face the Forest River office development.

Development Objectives

The central area of the River Street Neighborhood will become a thriving in-town, urban neighborhood with tree-lined streets and a wide variety of medium- to high-density housing anchored by a neighborhood commercial district along Grand Avenue and a public plaza at the intersection of Grand Avenue, Ash and Miller streets. It will have a sense of identity discernible by the residents and visitors. The Pioneer Walkway will be improved into a boulevard-style walking and bicycling route between the Boise River and downtown through this neighborhood, offering an refreshing and enjoyable park-like, public space that will also serve as a catalyst for development. The River Street Neighborhood will provide a place for people to live, shop, dine and socialize close to work places and cultural and entertainment attractions in downtown Boise. It will be possible to travel between these activity areas by walking and bicycling on people-friendly streets and by transit.

Office, service commercial and light industrial uses would continue along the western edge of this subdistrict facing 16th Street and Americana Boulevard, except that a major mixed-use development would develop on the northeast corner of Americana and Shoreline. Office and mixed-use development would occur on the east end of the district between 9th and 11th streets and along the south side of Myrtle Street. Hotel, retail, restaurant and tourism uses associated with the possible development of the convention center expansion would be encouraged north of the River Street Neighborhood along the West Connector or in the CBD, rather than allowing it to move into the River Street Neighborhood, so the vitality of the downtown core is reinforced.

Design & Development Guidelines

Land Use & Built Form

RS-1 Utilize the Preferred Development Concept in Figure 5 to guide development patterns in the River Street Neighborhood subdistrict, and in making decisions on public investments.

RS-2 Use the Built Form map in Figure 7 to guide building heights and development densities in the River Street Neighborhood.

RS-3 Establish a flourishing in-town, urban neighborhood with a wide range of housing choices in the River Street Neighborhood including both renter and owner units in different sizes and configurations and at different rent and price levels.

RS-4 Establish a distinctive identity for the residential portion of the River Street Neighborhood through gateways, public spaces, streetscaping, signage and other means.

RS-5 Encourage housing densities of 50 to 120 units per acre in order to create an urban level of intensity and to maximize the number of housing units developed in the River Street Neighborhood.

RS-6 Focus high- intensity housing and office uses along the Pioneer Walkway south of River Street and add restaurants and retail to the mix surrounding the civic space where the walkway intersects the Boise River Greenbelt.

RS-7 Focus medium- to medium-high-intensity housing development along the Pioneer Walkway north of River Street and add restaurants, retail and neighborhood-serving businesses surrounding the plaza space at the intersection of Grand Avenue and Miller and Ash streets.

RS-8 Create a neighborhood shopping district along Grand Avenue from Ash to 13th Street, and including the intersection of Grand Avenue, Miller and Ash streets.

RS-9 Prevent hotel and tourism-type uses associated with the possible development of the convention center expansion from developing south of Myrtle Street.

Historic Resources

RS-10 Assess the historic value of the existing houses in the River Street Neighborhood.

RS-11 Evaluate the possibility of creating a historic park within the River–Myrtle planning area, preferably in the River Street Neighborhood where a collection of the best examples of historic residences could be relocated, renovated and preserved.

Pioneer Corridor & Other Civic Spaces

RS-12 Use the Pioneer Walkway as an urban design element that provides structure to the River Street neighborhood, creates a public gathering place and linear park and serves as a catalyst for development along its edges.

RS-13 Establish a civic space at the intersection of 11th and Myrtle which celebrates this location as a gateway to downtown and a node on the Pioneer Walkway.

RS-14 Establish a neighborhood focal point using a plaza or similar civic space at the intersection of Grand Avenue and Miller and Ash streets.

RS-15 Establish a community-sized civic space at the intersection of the Pioneer Walkway and the Boise River Greenbelt. Encourage the development of activating uses such as restaurants in and along the edges of this public space.

Street Character & Connections

RS-16 Apply the Streetscaping Standards in the 2025 Downtown Boise Redevelopment Plan to streets in the River Street Neighborhood subdistrict.

RS-17 Apply the Design & Development Guidelines related to the Connector stated in Chapter 6, Street Character, Civic Spaces & Pedestrian Ways and Transportation & Parking.

Pedestrian/Bicycle/Transit Connections

RS-18 Establish strong pedestrian, bicycle and transit connections between the River Street Neighborhood, the CBD and other subdistricts in downtown.

RS-19 Improve Grand Avenue and Miller Street so they become part of a network of people-oriented, slow streets that carry local neighborhood traffic and serve as safe and attractive routes for walking and bicycling. Encourage the use of alternative transportation modes through improving these streets.

Parking

RS-20 Apply the Design & Development Guidelines related to parking stated in Chapter 6, Transportation & Parking.

Action Steps

Urban Design, Land Use & Zoning

- Use the Pioneer Walk as a focal point for housing, office and destination retail, restaurants and entertainment uses, with the objective of creating a critical mass of residents, workers and visitors to activate the corridor and support other land uses in the River–Myrtle planning area.
- Create marketing materials on the Pioneer Walkway geared toward developers and property owners to interest them in building projects along its edges consistent with the Pioneer Corridor design.
- Acquire land at strategic locations along the Pioneer Walkway to help stimulate catalyst projects through public-private partnerships.
- Acquire land suitable for housing development, enter into development partnerships and write down land prices when necessary to stimulate the supply of housing in the River Street Neighborhood.
- Work with Neighborhood Housing Services to access resources that can be used to develop affordable housing options in River Street.

■ Conduct information sessions with property owners about plans for the River Street Neighborhood and prospects for urban housing to encourage them to choose to develop housing on their properties. ■ Exclude hotel and tourism-type uses from zoning districts in place south of Myrtle Street.	■ Work with property owners, Boise City and ACHD to design and implement a neighborhood-oriented civic space at the intersection of Ash, Miller and Grand Avenues. Include this improvement in the capital investment plan for 2006-2010. ■ Initiate the design process for a plaza-like space at the intersection of 11th & Myrtle and construct this improvement in 2005-2006. ■ Create gateways into the residential area of the River Street Neighborhood that give it identity and a desirable sense of place. Locations to consider for gateways include 15th/River, 15th/Grand, 13th/Myrtle, 11th/Myrtle, 9th/Miller and 9th/River or 11th/River depending on what happens in the blocks between 9th and 11th.
Civic Spaces	
■ If feasible, implement the design for the Pioneer Walkway by realigning and vacating portions of Ash Street and acquiring sufficient width to construct the walkway through acquisition, easements, licenses or dedications from property owners and Boise City. ■ Construct the Pioneer Walkway as a grand, boulevard-style pedestrian link from the Boise River Greenbelt through the River Street neighborhood to 11th and Myrtle. (See the West Connector subdistrict and the Warehouse/Cultural District for an action steps regarding continuing the Pioneer Walkway on Broad to the Warehouse/Cultural District and on 8th Street to the Grove Plaza and the CBD.) ■ Work with property owners and Boise City Parks & Recreation to design and build a community-oriented civic space at the intersection of the Pioneer Corridor and the Boise River Greenbelt. Include this improvement in the capital investment plan for 2006-2010.	■ Implement the action steps related to the Connector stated in Chapter 6, Street Character, Civic Spaces & Pedestrian Ways and Transportation & Parking.
Street Character & Connections	
■ Implement the Streetscaping Standards in the 2025 Downtown Boise Redevelopment Plan when street improvements are required as part of a development project or whenever improvements are made to streets in the River Street Neighborhood. ■ Implement the action steps related to the Connector stated in Chapter 6, Street Character, Civic Spaces & Pedestrian Ways and Transportation & Parking.	■ Implement the Streetscaping Standards in the 2025 Downtown Boise Redevelopment Plan when street improvements are required as part of a development project or whenever improvements are made to streets in the River Street Neighborhood.
Pedestrian/Bicycle/Transit Connections	
■ Install streetscaping improvements on Grand Avenue and Miller Street that identify them as primary pedestrian streets and encourage the use of these streets for walking and bicycling.	■ Install streetscaping improvements on Grand Avenue and Miller Street that identify them as primary pedestrian streets and encourage the use of these streets for walking and bicycling.

- Implement the downtown circulator to include a route along 9th Street that will link the River Street Neighborhood subdistrict to the CBD and the other subdistricts in downtown.

Parking

- Construct at least one public parking garage in the vicinity of the Pioneer Walkway to serve the River Street Neighborhood and visitors. Expand the P-1 parking district to include the service area for this garage.
- Implement the Action Steps for parking stated in Chapter 6, Transportation & Parking.

Economic Development

- Use the Pioneer Walkway as a draw to bring visitors downtown.

Warehouse/Cultural District

Existing Conditions

Boise's historic warehouse district developed with the coming of the railroad to downtown Boise in the 1890s. A large portion of this warehouse district is located in the center of the River-Myrtle planning area and has been identified as the Warehouse/Cultural District

subdistrict in this plan. It extends from Front Street to the Boise River and from Capitol Boulevard to 9th Street. Turn-of-the-century brick buildings line 8th, 9th and other streets in the area. Boise City established a historic district in a portion of this area in 19__ (see Figure 6).

In the late 1970s, W.H. Moore Company began redeveloping the historic buildings in the blocks between Front, Capitol, Myrtle and 9th streets, creating a specialty retail, office and entertainment district known as the 8th Street Marketplace. It met with only limited success. In 2002, these properties were bought by Brix and Company, which is constructing an expanded destination-style retail and entertainment development known as BoDo using the historic buildings as a basis and adding 34,000 square feet of office, 80,000 square feet of retail and restaurant space, an 11-screen movie theater and a 168-room hotel. As part of this project, the developer is re-establishing Broad Street from Capitol Boulevard to 9th Street, which restores the traditional street grid at this location.

This subdistrict has also become known as the Boise Cultural District because of the high concentration of cultural and entertainment facilities that are located within its boundaries or in close proximity.¹ The most recent additions are the Fulton Street Theater that opened in 2000 and the Big Easy Concert House that opened the following year.

¹ Included within the subdistrict are the Big Easy Concert House; the Funny Bone comedy club; Knock 'Em Dead Dinner Theater; Esther Simplot Performing Arts Academy, which provides classes and office and rehearsal space for Opera Idaho, Boise Philharmonic Orchestra and Ballet Idaho; the Fulton Street Theater which is home to Boise Contemporary Theater and Balance Dance Company and leases office space to Idaho Shakespeare Festival and Idaho Theater for Youth; the Kulture Klatsch; the Anne Frank Memorial; World Sports Humanitarian Hall of Fame; Boise City Library and Log Cabin Literary Center. The Foothills School of Arts and Sciences, the Idaho Urban Research and Design Center and part of the Boise State University studio art program are co-located in a warehouse building at 8th and River streets. Julia Davis Park is located immediately east of Capitol Boulevard and contains an amazing collection of cultural venues including the Boise Art Museum, Idaho State Historical Museum, Idaho Black History Museum, Discovery Center of Idaho and Zoo Boise. Also across Capitol Boulevard is the Basque Block, where the Basque Museum and Cultural Center is located, and the Flicks Theater. The Egyptian Theater is located at Main Street and Capitol Boulevard.

Recently there has been strong interest in building housing in the vicinity of 8th and Fulton near the Boise City Library. A design workshop was held to explore how this area could become a pulse point of housing clustered at the lower end of the Cultural District that would lead to the establishment of an urban neighborhood.

Desired Character

Many positive changes have occurred in the Warehouse/Cultural District since 1994, and at the same time, the historic character has remained more or less intact. Historic buildings are being renovated and reused as part of new development projects. The plan supports these efforts and encourages them to continue, keeping in mind that there may be an occasional older building that is obsolete or beyond repair and needs to be removed. In these situations, the plan recommends that new infill buildings in areas dominated by historic buildings respect the current scale and massing of these buildings although the new buildings may have a variety of architectural designs. Elsewhere in the Warehouse/Cultural District more intensive development may be acceptable especially if it results in housing

growth to the west onto Parcels C-D. As this activity center grows, it is essential that connections be created between the BoDo development and the CBD to promote synergy between them rather draining energy from the CBD.

The Warehouse/Cultural District is viewed as a prime location for the development of medium- to high-density housing as part of BoDo and in a compact, urban neighborhood in the vicinity of 8th and Fulton streets.

The plan supports creating a distinct identity for the Cultural District through not only concentrating cultural venues in this area but also holding a variety of cultural events and using it as the focal point for Fall for Boise, the city's annual month-long celebration of the arts in the fall. The goal is to breathe life into the concept of the Cultural District as a lively, happening place in the public's mind. The plan calls for adding a mid-sized theater to the cultural offerings in the Cultural District and to use signage and urban design features to communicate the existence of the district.

Development Objective

A vigorous cultural district will be established between 9th and Capitol and Front and the Boise River, and will also extend to include the museums and cultural attractions in Julia Davis Park, the Basque Block and the Egyptian and Flicks movie theaters. This area will have a distinct identity and will become known as a place where a wide variety of cultural and entertainment events happen. It will add to the quality of life in the region and stimulate an increase in both planned and spontaneous visits to downtown Boise.

The cultural district will be anchored on the north by a destination-retail and entertainment center at 8th and Broad, which will build on the historic character of the area and complement rather than compete with the central business district. Strong connections that encourage pedestrians to travel between the Cultural District and the CBD will be established using transit, streetscaping, wayfinding, public art and other features. The retail and entertainment center will serve as a regional draw to bring people to downtown Boise, and will enhance the desirability of downtown as a place to live, work and shop. It will serve as a catalyst for additional retail, restaurant and entertainment development.

The cultural district will be anchored on the south by a compact, urban neighborhood. This neighborhood would be marked by entry treatments on 8th Street at River and Myrtle streets and on Fulton Street at 9th Street and Capitol Boulevard as well as distinctive streetscaping that would create a sense of place.

Design & Development Guidelines

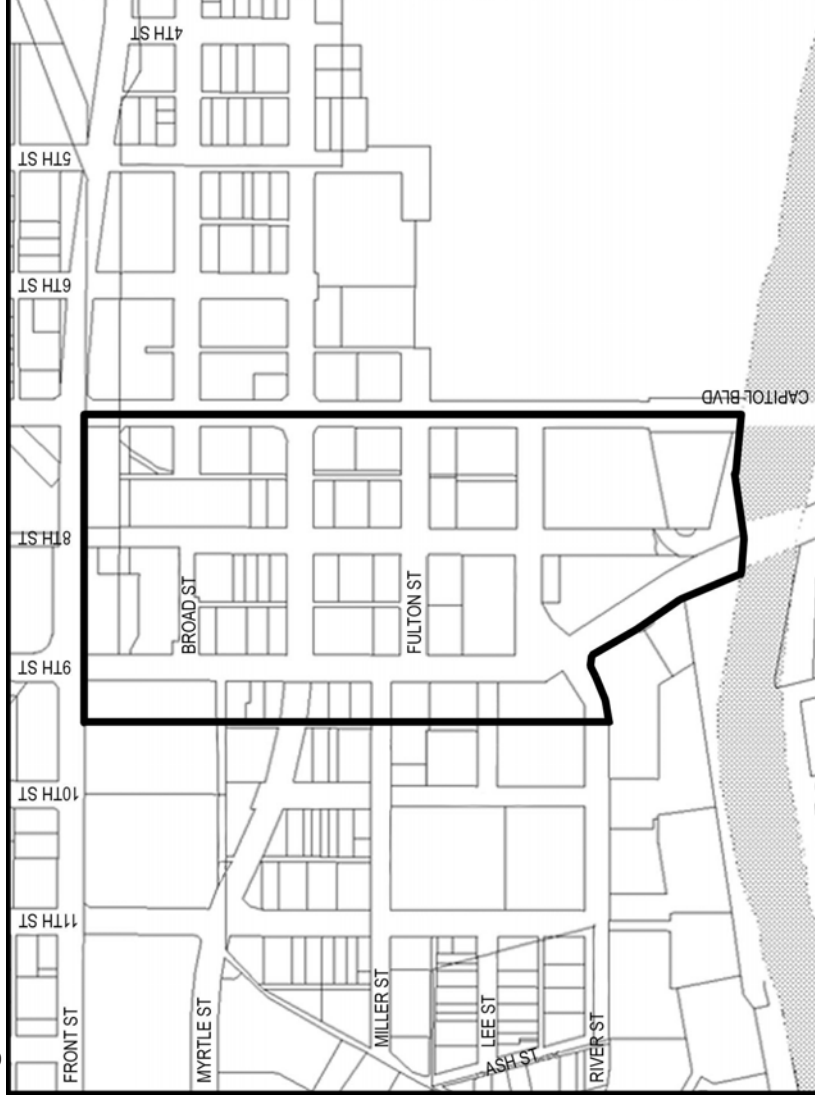
Land Use & Built Form

CD-1 Utilize the Preferred Development Concept in Figure 5 to guide development patterns in the Warehouse/Cultural District, and in making decisions on public investments.

CD-2 Use the Built Form map in Figure 7 to guide building heights and development densities in the Warehouse/Cultural District.

CD-3 Establish a vigorous destination-type retail and entertainment center at the north end of the Warehouse/Cultural District between Front and Myrtle, and 9th and Capitol.

Figure 11C: Warehouse/Cultural District



CD-4	Encourage spin-off retail uses to occur on the superblock between 9 th and 11 th and Front and Myrtle streets in the West Connector subdistrict.	structure to the historic fabric of the Warehouse/Cultural District and to the community's record of its history with the benefits derived from redevelopment proposals. Give weight to proposals that will result in increasing the supply of housing in the Warehouse/Cultural District.
CD-5	Work to create a compact, urban neighborhood in the area bounded by Capitol Boulevard and River, 9 th and Myrtle streets. Make public investments to create a sense of place.	
CD-6	Encourage housing densities of 80 to 120 units per acre in order to create an urban level of intensity and to maximize the number of housing units developed in the Warehouse/Cultural District.	CD-11 Encourage a range of architectural styles in new buildings in a way that does not detract from the historic character of the Warehouse/Cultural District.
CD-7	Evaluate whether the conversion of existing warehouse buildings to housing will make more affordable housing options available. Where feasible, encourage the renovation of warehouses into unique types of housing units such as lofts or artists' housing.	CD-12 Design new infill buildings in areas dominated by historic buildings so they respect the current scale and massing of these buildings although the new building may have a variety of architectural designs.
Cultural District Development		
CD-8	Preserve the historic character of the Warehouse/Cultural District while encouraging development of underutilized property.	CD-13 Encourage the growth of performing arts and other cultural uses in the Warehouse/Cultural District with emphasis on the area south of Myrtle Street.
CD-9	Retain and build upon the historic flavor of the Warehouse/Cultural District. Facilitate the renovation and reuse of historic buildings whenever feasible.	CD-14 Evaluate the need for new cultural facilities and the desirability of siting them in the Cultural District. Advocate for retaining the Cultural District as a focal point for cultural facilities.
CD-10	Where a historic structure is in poor repair or obsolete, evaluate alternative approaches that would include reuse of the structure, reuse of the façade or replacement. Weigh the contribution made by the	CD-15 Continue the installation of signage and design elements to strengthen the identity of the Cultural District.
		CD-16 Make the Cultural District a significant draw for downtown visitors.

Action Steps	
Street Character & Connections CD-17 Apply the Streetscape Standards in the 2025 Downtown Boise Redevelopment Plan to streets in the Warehouse/Cultural District.	Urban Design, Land Use & Zoning - Form development partnerships and invest in public improvements to stimulate retail development in the north end of the Warehouse/Cultural District. - Acquire land suitable for housing development, enter into development partnerships and write down land prices when necessary to stimulate housing development in south end of the Warehouse/Cultural District. - Conduct information sessions with property owners about plans for the Warehouse/Cultural District and prospects for urban housing to encourage them to choose to develop housing on their properties.
Pedestrian/Bicycle/Transit Connections CD-20 Because of its unique role as a cultural and retail hub for downtown and the region, establish strong pedestrian and transit connections to the region.	Historic Resources - Work with the Boise City Historic Preservation Commission (HPC) to create a method of evaluating redevelopment proposals affecting historic structures. Weigh the assets being gained, preserved and lost, and the degree to which redevelopment will achieve the desired character and preferred development concept stated in this plan. - Consult with the HPC when redevelopment proposals involving historic structures are initiated to obtain early input on ways to meet preservation and redevelopment goals.
CD-21 Establish strong pedestrian, bicycle and transit connections between the Warehouse/Cultural District, the CBD and other subdistricts in downtown. Recognize 8th, Broad and Fulton streets as primary pedestrian and bicycling streets.	
CD-22 Use Broad and 8 th streets to extend the Pioneer Walkway from its current terminus at 11 th and Myrtle through the West Connector and Warehouse/Cultural District to the Grove Plaza and CBD.	
Parking CD-24 Apply the Design & Development Guidelines related to parking stated in Chapter 6, Transportation & Parking.	

Cultural District Development

- Work with the Office of the Mayor, Boise City Arts Commission, arts patrons and the arts community to evaluate if a mid-sized theater (400-1200 seats) is needed and whether locating it in the Warehouse/Cultural District is appropriate. If appropriate, identify potential locations and funding sources and create an implementation plan that would lead to construction.
- Hold a variety of cultural events in the Cultural District and use it as the focal point for Fall for Boise, the city's annual month-long celebration of the arts, to breathe life into this concept.
- Include references to the Cultural District in marketing cultural and entertainment events occurring in this district. Use the Cultural District as a draw to bring visitors downtown.

Street Character & Connections

- Implement the Action Steps related to the Connector as stated in Chapter 6, Street Character, Civic Spaces & Pedestrian Ways and Transportation & Parking.
- Implement the Action Steps related to Capitol Boulevard as stated in Chapter 6, Street Character, Civic Spaces & Pedestrian Ways.
- Implement the Streetscaping Standards in the 2025 Downtown Boise Redevelopment Plan when street improvements are required as part of a development project or whenever improvements are made to streets in the Warehouse/Cultural District.

- Install streetscaping improvements along Myrtle, Fulton and River streets to create a distinctive sense of place in the area planned to be an urban neighborhood. Design and construct distinctive gateway treatments at the entry points to this neighborhood.
- Install streetscaping improvements along 8th Street from Front to the Boise River consistent with its role as the primary pedestrian connection between the CBD and the Boise River. These improvements should be similar in character to the improvements on 8th Street between Main and Bannock.

Pedestrian/Bicycle/Transit Connections

- Construct the Pioneer Walkway as a grand, boulevard-style pedestrian link from the Boise Greenbelt through the Warehouse/Cultural District on Broad and 8th streets to The Grove Plaza and CBD. Install streetscaping improvements along Broad consistent with this objective.
- Implement the downtown circulator to include a route along Capitol Boulevard and 9th Street that will link the Warehouse/Cultural District to the CBD and other subdistricts in downtown. Create transit stops in the Cultural District.

Parking

- Implement the Action Steps related to parking stated in Chapter 6, Transportation & Parking.

- Construct a public parking garage to serve the destination retail center at the north end of Warehouse/Cultural District. Expand the P-1 parking district to include the service area for this garage.
- Consider construction of a public parking garage to serve the south end of the Warehouse/Cultural District if needed. Expand the P-1 parking district to include the service area for this garage if it is built.

East Connector/Parkside

Existing Conditions

The East Connector /Parkside subdistrict runs approximately from Capitol Boulevard to 2nd Street and from Front Street to the edge of Julia Davis Park. It includes parcels located between Front and Myrtle, which form the Broadway-Chinden Connector, and parcels south of Myrtle Street bordering the park. This area between the two sides of the connector is wider than it is west of 11th Street. Broad Street runs east-west between Front and Myrtle and serves as a people-oriented, slow street much more suitable for pedestrian and bicycle travel than the Connector.

Development of this area began with the establishment of an elegant residential neighborhood in the late 1890s and early 1900s. The land for Julia Davis Park was donated to Boise City in 1907, which resulted in the creation of an exceptional urban park along the neighborhood's south edge. The coming of the railroad to downtown Boise in 1894, however, meant tracks were

eventually extended along Front Street cutting this neighborhood off from the rest of downtown. Later warehouses were built along the tracks and the neighborhood began to slide. Houses built during the Depression were much more modest. Many of the houses have since become rentals and are in deteriorating condition.

The 1990s were marked by an influx of low-rise, suburban style office buildings, encouraged by a change in the zoning to Residential-Office in 199_. The neighborhood is now a mixture of older, single family houses, warehouses, offices and vacant land where buildings have been cleared.

The Connector exerts a strong influence on this area as it does in the West Connector subdistrict. It works against the development of housing along its edges because the current design and traffic speeds create a harsh environment. At the same time, the Parkside neighborhood has assets that make for a desirable residential neighborhood. These include close proximity to Julia Davis Park, Boise State University and a large-scale grocery store, several major employers within walking or bicycling distance, and close proximity to restaurants, shopping and cultural offerings in Old Boise and the downtown core. The plan recommends that steps be taken to encourage housing as part of the land use mix in this subdistrict even though the tendency of the market will be toward office.

Desired Character

The area east of 6th Street between Broad Street and the northern edge of Julia Davis Park will become a compact, urban residential neighborhood interspersed

with low- to medium-rise office buildings. Where possible, housing should face housing along Broad Street. Broad Street will become a delightful neighborhood street with neighborhood-oriented retail uses clustered on the west end close to Capitol Boulevard or in the center section around a neighborhood node on Broad Street between 3rd and 4th streets. Higher intensity office uses will line the Connector and Capitol Boulevard, with the possibility of ground floor retail on Capitol. The Connector is not expected to become a shopping street given its characteristics with high volumes of commute traffic. Fourth Street will be re-established as a through connection to Old Boise–Eastside. The entrance to Julia Davis Park at 3rd Street will become much more prominent and 3rd Street will become a strong pedestrian and bicycle link from the park to the Ada County Courthouse and then to Grove, Idaho and Main streets which will provide connections to the Old Boise commercial district and the downtown core.

Development Objectives

Create a balance between office, retail and residential uses in this subdistrict and prevent office development from overtaking the entire area. Focus office development along the Connector and Capitol Boulevard. Locate housing along Broad Street, to take advantage of this pedestrian-oriented street, and in the area between Broad and Myrtle streets because of its proximity to Julia Davis Park. Create a neighborhood node on Broad Street between 3rd and 4th streets, and pedestrian and bicycle connections on 3rd and 4th to Old Boise–Eastside and the downtown core. Encourage neighborhood-oriented retail uses to cluster at the neighborhood node and downtown-oriented retail uses to locate on Capitol Boulevard.

Design & Development Guidelines

Land Use & Built Form

EC-1 Utilize the Preferred Development Concept in [Figure 5](#) to guide development patterns in the East Connector/Parkside Neighborhood subdistrict, and in making decisions on public investments.

EC-2 Use the Built Form map in [Figure 7](#) to guide building heights and development densities in the East Connector/Parkside subdistrict.

EC-3 Work to create a compact, urban neighborhood in the area bounded by 6th, Broad, 3rd and Myrtle Streets. Encourage locating housing developments so they face each other across Broad Street or are oriented to Julia Davis Park.

EC-4 Encourage housing densities of 80 to 120 units per acre in order to create an urban level of intensity and to maximize the number of housing units developed in the East Connector/Parkside subdistrict.

EC-5 Focus office development along the Connector and Capitol Boulevard.

Historic Resources

EC-6 Assess the historic value of the existing houses in the Parkside Neighborhood.

EC-7 Evaluate the possibility of creating a historic park within the River–Myrtle planning area, preferably in the River Street Neighborhood where a collection of the best examples of historic residences could be relocated, renovated and preserved.

Civic Spaces
EC-8 Create a neighborhood node on Broad Street between 3rd and 4th streets similar to the Basque Block. Encourage neighborhood-oriented retail to cluster around this node.

Street Character & Connections
EC-9 Apply the Streetscape Standards in the 2025 Downtown Boise Redevelopment Plan to streets in the East Connector/ Parkside District.

EC-10 Apply the Design & Development Guidelines related to the Connector stated in Chapter 6, Street Character, Civic Spaces & Pedestrian Ways and Transportation & Parking.

EC-11 Apply the Design & Development Guidelines related to Capitol Boulevard stated in Chapter 6, Street Character, Civic Spaces & Pedestrian Ways.

Pedestrian/Bicycle/Transit Connections
EC-12 Establish strong pedestrian, bicycle and transit connections between the East Connector/ Parkside subdistrict, the CBD and other subdistricts in downtown. Recognize 3rd and Broad Streets as primary pedestrian and bicycling streets.

EC-13 Create a more prominent entrance to Julia Davis Park at 3rd Street. Improve 3rd Street so it becomes a strong pedestrian and bicycle link from the park to the Ada County Courthouse and then to Grove, Idaho and Main streets, which provide connections to the Old Boise commercial district and the downtown core.

Figure 11D: East Connector/Parkside



Parking

- EC-14 Apply the Design & Development Guidelines related to parking stated in Chapter 6, Transportation & Parking.

Action Steps

Urban Design, Land Use & Zoning

- Acquire land suitable for housing development, enter into development partnerships and write down land prices when necessary to stimulate housing development in the East Connector/Parkside subdistrict.
- Conduct information sessions with property owners about plans for the East Connector/Parkside subdistrict and prospects for urban housing to encourage them to choose to develop housing on their properties.

Civic Spaces

- Initiate the design process for a civic space on Broad Avenue between 3rd and 4th Streets and include this improvement in the five-year capital plan for 2006-2010.
- Work with Boise City Parks Department to make improvements to the 3rd Street entrance to Julia Davis Park.

Street Character & Connections

- Implement the Action Steps related to the Connector as stated in Chapter 6, Street Character, Civic Spaces & Pedestrian Ways and Transportation & Parking.
- Implement the Action Steps related to Capitol Boulevard as stated in Chapter 6, Street Character, Civic Spaces & Pedestrian Ways.
- Implement the streetscaping standards in the 2025 Downtown Boise Redevelopment Plan when street improvements are required as part of a development project or whenever improvements are made to streets in the East Connector/Parkside subdistrict.
- Install streetscaping improvements on 3rd Street and Broad Streets that identify them as primary pedestrian streets and encourage the use of these streets for walking and bicycling.

Pedestrian/Bicycle/Transit Connections

- Implement the downtown circulator to include a route along Capitol Boulevard that will link the East Connector/Parkside subdistrict to the CBD, Old Boise Eastside and the Warehouse/Cultural District.

Parking

- Implement the Action Steps related to parking as stated in Chapter 6, Transportation & Parking.

Courthouse Corridor

Existing Conditions

The Courthouse Corridor is bounded on the north side by property owned by Ada County, Broadway Avenue on the east, Julia Davis Park on the south and 3rd and 2nd streets on the west.

Development of the Ada County property is the most substantial event to happen to the River–Myrtle District since 1994. In 1994, this property was vacant except for a warehouse building at the east end—a vestige of the railroad era. Construction of the 322,825 square foot Ada County Courthouse and 424-space Courthouse parking garage was completed in January 2002. It was followed by complete excavation of the parcels between Avenue B and Broadway to construct a one-level underground parking garage with 381 spaces, development of 399 affordable housing units and retail space on Parcels 2 and 3 (referred to as PA2 and PA3), and construction of the 207,300 square foot Idaho Water Center, which provides office and lab space for the University of Idaho, Idaho Department of Water Resources and US Forest Service. CCDC made a significant investment in creating a pedestrian promenade that runs east-west between the Courthouse Corridor and the Idaho Water Center and along the front face of the Civic Plaza Apartments. It has been furnished with brick paving, planters and seating which create a high quality public space. The sidewalks along Front Street have also been upgraded with brick paving and lined with trees to provide a more protected pedestrian environment.

The developments in the Courthouse Corridor have raised the level of intensity at the east end of the River–Myrtle planning area and created more ambitious expectations as to what will happen in the future. The scale of the Ada County Courthouse, Civic Plaza Apartments, Idaho Water Center and the Kreizenbeck office building sends a clear message that downtown is moving outward and that the intersection at Broadway and Front has become a gateway to downtown.

The current mix of uses includes larger-scale office buildings, a warehouse-style grocery store, the Civic Plaza Apartments and ground-floor retail space in the courthouse and Kreizenbeck building. More retail space has been built as part of the Civic Plaza Apartments project but has there have been difficulties leasing it. The original development program for the Ada County-owned property envisioned an additional 800,000 square feet. In April 2004, CCDC, Ada County, University of Idaho and Civic Partners conducted a mid-course review of the project, which resulted in a number of suggestions for completion of the development. These included:

- Addressing the challenges created by the Connector for adjacent development and the need to tame these streets if development is to be successful.
- Recognizing the difficulties facing retail uses and the need to broaden the mix of tenants sought for the retail space.
- Adding more residential to create a stronger sense of community for the existing Civic Plaza apartments.

■ Adding a hotel or educational uses to the mix. ■ Retaining open space on the parcel in front of PA3, and clustering development on either end to create a sense of place. ■ Softening the current institutional look to the Corridor as new buildings are added.	County Courthouse in downtown was an important objective of the original River Street–Myrtle Street Urban Design Plan, which has been met with the completion of the new courthouse.
These recommendations have been incorporated into the development guidelines for the Courthouse Corridor subdistrict where applicable.	Challenges remain in linking the Courthouse Corridor to the CBD and to other activity centers in downtown. The Downtown Boise Mobility Study is recommending that a downtown circulator be put in place that would have two loops, one of which would run on Main and Idaho and connect to the Courthouse Corridor.
The Winco grocery store is located on a site equivalent to four city blocks with approximately 1/3 of the site occupied by the building and 2/3 occupied by the parking lot. It departs from the urban character that this plan encourages; however, it is a significant asset in the effort to increase the amount of housing in downtown.	Desired Character The Courthouse Corridor will continue to be a focal point for larger-scale development because of its location on Front and Myrtle streets and Broadway Avenue—all of which are high volume thoroughfares. Much of the recent development is government-related or private offices. As new development occurs, it should include a broader mix of uses—more housing, retail, hotels and educational facilities. Otherwise there is a possibility that the corridor will be lifeless after hours and on weekends. Creating a mix of uses will also make better use of the abundant parking facilities now in place along the north side of the corridor.
There is a significant opportunity site on the southwest corner of Broadway and Front. The University of Idaho Foundation purchased the property to build Idaho Place—where University of Idaho, Boise State University and Idaho State University had hoped to create an educational center serving the State of Idaho and beyond. This project is on hold at the present time, which creates uncertainty about what might occur on this property. The River Street–Myrtle Street Master Plan envisions a significant mixed-use project at this location.	Care should be exercised in how these larger-scale buildings are designed to avoid them becoming too monumental, too far from human scale and too disconnected with the street level. The visual impact of the existing Civic Plaza buildings on the north side of Front Street needs to be softened. The vacant land situated between these buildings and Front Street offers the opportunity to create a public space around which additional buildings could be clustered—buildings with more varied massing and more street-level presence. A
Moving the Ada County Courthouse from the CBD to its current location on Front Street between 1st and 3rd streets has changed traffic patterns; the degree to which the County work force uses the downtown core for shopping and dining at lunch time; and the downtown office market as legal and other related office tenants move to be closer to the Courthouse. Retaining the Ada	

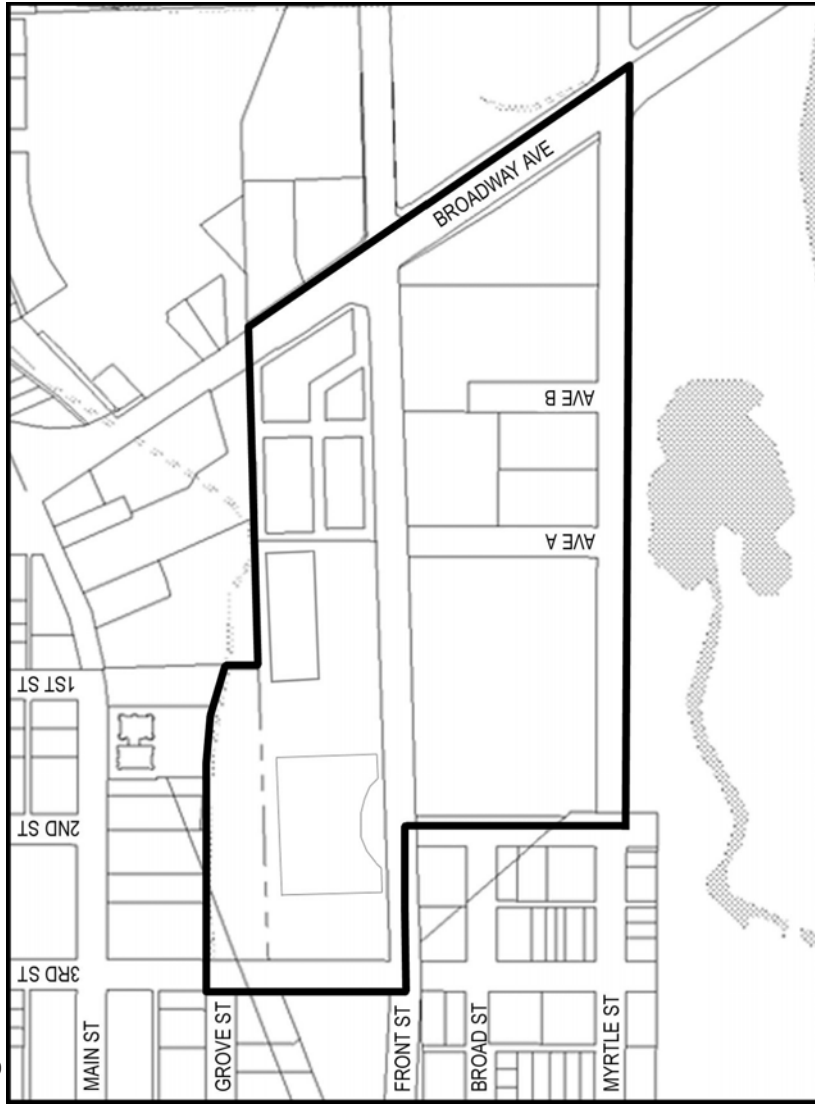
public open space would also provide a gathering place for people and serve as the living room to the apartments already in place.

Adding housing to the Ada County property and to the Courthouse Corridor in general will benefit the existing apartments by creating a stronger sense of neighborhood in the area.

The property on Broadway Avenue between Front and Myrtle represents one of the most significant development opportunities between Portland and Salt Lake City. It is most important that it not be underdeveloped. It is ideally suited for a major mixed-use development including residential, office, retail and a hotel at a scale to match or exceed the Idaho Water Center. Such development will help address the balance between different types of uses in the Courthouse Corridor. At the same time, the building form should be approachable and connected to the street level. It should not be self-contained and separated from the surrounding city.

The idea of creating a higher education center, with representation from the three major universities in the state still has merit given the growing population in the Treasure Valley and the importance of education to keeping Idaho competitive in the world economy. The Courthouse Corridor is a logical location for such facilities although the size and configuration may differ substantially from the original concept. Other possible locations are in the West Connector and East Connector/Parkside subdistricts, the CBD and the Westside Downtown district. If such a center is undertaken, it should be located so it becomes a part of the downtown community and not a separate campus-style facility.

Figure 11E: Courthouse Corridor



As noted earlier, the Broadway–Chinden Connector exerts a powerful influence on the types of development seeking to locate along its edges. It also affects the connectivity between the north side and the south side of downtown Boise and how comfortable people feel walking or bicycling on or across these roadways.

Development Objectives

Given that the Courthouse Corridor is a focal point for high intensity development, attention should be given to assuring that this development is well-designed, approachable and not overwhelming. New developments should incorporate elements that reduce the monumental effect of massive buildings and retain a human scale at the street level. Use of design techniques that make large-scale buildings appear to be a collection of smaller buildings will be encouraged. Incorporating open space in high intensity development to create a sense of place and stimulate more dynamic activity at the street level will also be encouraged.

New development should incorporate a mix of uses that encourages a 24-hour level of activity, and avoids the Courthouse Corridor becoming a collection of office buildings. Creation of a major mixed-use project on the west side of Broadway between Front and Myrtle is the most significant plan proposal related to increasing the mix of uses in the corridor. It is most important that the development potential of this site, and its prospects for contributing to the downtown and regional economy not be underestimated. An emphasis should also be placed on adding more housing to create a neighborhood for both new and existing residents. Additional

housing would also reinforce efforts to create an in-town neighborhood in the East Connector/Parkside subdistrict to the immediate west of the corridor.

Every effort should be made to moderate the impact of Front and Myrtle on adjacent properties by creating a more appealing pedestrian environment along their edges. This plan envisions the Connector transformed into a set of boulevard-style streets that continue to provide a high capacity vehicular route to, from and through downtown, but are also compatible with a downtown setting. More intense streetscaping will be used to create a buffer between the sidewalk and the roadway to encourage walking and bicycling and allow a wider range of uses to develop along its edges. Public spaces should also be placed at strategic locations along Front and Myrtle Streets to vary the visual width of the corridor as people travel through it and to create more visual interest and a stronger sense of place.

Connectivity for pedestrians and bicyclists should be improved by establishing a pedestrian connection on the Avenue A alignment from Front Street to Warm Springs Avenue and on the Broad Street alignment from 2nd Street to Avenue B, and by adding traffic signals at selected intersections on the Connector. The Courthouse Corridor should be better served by transit. Because the Courthouse Corridor is being developed at higher intensities, it will have a concentration of workers and residents. It is important to provide connections for these people to the retail districts in the CBD, Old Boise commercial district and the Warehouse/Cultural District.

Design & Development Guidelines

Land Use & Built Form

CC-1 Utilize the Preferred Development Concept in Figure 5 to guide development patterns in the Courthouse Corridor subdistrict, and in making decisions on public investments.

CC-2 Use the Built Form map in Figure 7 to guide building heights and development densities in the Courthouse Corridor subdistrict.

CC-3 Encourage the development of a major, mixed-use project west of Broadway between Front and Myrtle, to include residential, office, retail and possibly a hotel at a scale to match the Idaho Water Center.

CC-4 Broaden the mix of uses in the Courthouse Corridor subdistrict to include more housing, retail, hotels and educational facilities and reduce the concentration of office uses in order to add vitality to the area after hours and on weekends.

CC-5 Add housing to the Courthouse Corridor subdistrict to create more of a neighborhood feeling for existing housing.

CC-6 Encourage housing densities of 80 to 140 units per acre in order to create an urban level of intensity and to maximize the number of housing units developed in the Courthouse Corridor subdistrict.

CC-7 Discourage the construction of mega-structures that fill entire super-blocks unless this building type is essential to a particular use. Use building designs that break down mega-structures into a series of building masses that are more human-scaled and less monumental, and appear to be a series of smaller buildings.

Civic Spaces

CC-8 Work with developers to incorporate public spaces into private development projects and to use these spaces as a focal point in arranging buildings. Use these spaces to create a sense of place, create connections between buildings, enliven the street level and provide a counterpoint to high intensity development.

Street Character & Connections

CC-9 Apply the Streetscape Standards in the 2025 Downtown Boise Redevelopment Plan to streets in the Courthouse Corridor subdistrict.

CC-10 Apply the Design & Development Guidelines related to the Connector stated in Chapter 6, Street Character, Civic Spaces & Pedestrian Ways and Transportation & Parking.

CC-11 Work with property owners to improve the Broad Street alignment from 2nd Street to Broadway Avenue as a pedestrian promenade, which provides a walking and bicycling connection through their properties. Recommend this connection be incorporated into the design of new development projects when the Broad Street alignment crosses or abuts these projects. Reconstruct a street on the Broad Street alignment where feasible.

Pedestrian/Bicycle/Transit Connections

CC-12 Establish strong pedestrian and transit connections between the Courthouse Corridor subdistrict, the CBD and other subdistricts in downtown. Recognize 3rd, Avenue A and Avenue B as primary north-south pedestrian and bicycle connections.

Parking

CC-13 Apply the Design & Development Guidelines related to parking stated in Chapter 6, 'Transportation & Parking.'

Actions Steps

Urban Design, Land Use & Zoning

- Work with Ada County, University of Idaho and Civic Partners to implement the recommendations from the Courthouse Corridor mid-course review. Refine the urban design plan for the remaining development to be built in the corridor.

- Work with the University of Idaho to conduct a market analysis of the development possibilities for the property on Broadway Avenue between Front and Myrtle streets. Conduct a design charrette to develop scenarios for building massing and urban design elements. Give attention to creating a pedestrian connection through the property from Broadway to Avenue B along Broad Street alignment.

- Collect examples of large-scale buildings designed to appear as a series of smaller buildings, ways in which to keep large-scale buildings approachable and ways to retain a human scale at the street level regardless of the massiveness of the building. Include examples of developments that use open space as a way to arrange high-density development into attractive and welcoming places. Use these ideas in working with developers proposing projects in the Courthouse Corridor subdistrict.

- Develop an overall urban design scheme for the Courthouse Corridor subdistrict that shows how to coordinate site layout between developments so they create a series of places rather than stand-alone projects.

- Evaluate the R-O zoning district to determine if it remains appropriate for the Courthouse Corridor subdistrict given the intensity of development being built in this subdistrict. Consider rezoning this area to C-5 or a mixed-use zoning district created to better address the type and intensity of development occurring in the Courthouse Corridor subdistrict.

Civic Spaces

- Work with Ada County and the developer in the Courthouse Corridor subdistrict to create a public open space between the Civic Plaza Apartments and Front Street.

Street Character & Connections

- Implement the Action Steps related to the Connector as stated in Chapter 6, Street Character, Civic Spaces & Pedestrian Ways and Transportation & Parking.
- Implement the streetscaping standards in the 2025 Downtown Boise Redevelopment Plan when street improvements are required as part of a development project or whenever improvements are made to streets in the Courthouse Corridor subdistrict.
- Work with property owners to re-establish Broad Street from 2nd Street to Avenue B. If a street connection is not possible, establish a pedestrian connection on the Broad Street alignment.

Pedestrian/Bicycle/Transit Connections

- Implement the downtown circulator to include a route through the Courthouse Corridor subdistrict that will link this subdistrict, the CBD and other subdistricts in downtown. Create transit stops to serve the Idaho Water Center and Ada County Courthouse as well as other destinations.

Parking

- Implement the Actions Steps related to parking stated in Chapter 6, Transportation & Parking.

Old Boise–Eastside

The subdistrict map in [Figure 11](#) shows two additional areas: a portion of the Central Business District and the Eastside Neighborhood. These two areas comprise the Old Boise–Eastside planning area.¹ The creation of the River Myrtle–Old Boise urban renewal district in 2004 added the Old Boise–Eastside study area to the original River Street–Myrtle Street Urban Renewal District, which has been renamed the River Myrtle–Old Boise Urban Renewal District. In addition the City Council approved master plans for this combined urban renewal district: one covering the River–Myrtle planning area and one covering the Old Boise–Eastside planning area. Please refer to the Old Boise–Eastside Master Plan for information about specific plans, guidelines and actions steps that pertain to these two subdistricts.

¹ The original River Street–Myrtle Street Urban Design Plan included a subdistrict called the Old Boise Expansion Area. This area was located between Capitol Boulevard and Grove, 3rd and Front streets. It is now included within the Old Boise–Eastside planning area and so is covered by the Old Boise–Eastside Master Plan.

8: Implementation



Construction of the Ada County Courthouse and Administrative Offices at Front and 2nd streets fulfilled an objective in the original River Street–Myrtle Street Urban Design Plan to maintain downtown Boise as the center of federal, state, county and city government institutions. It also served as a catalyst for additional development at urban intensities and expanded the commonly accepted boundaries for downtown Boise.

However good a plan may be, its benefits will not be realized until it has been implemented. Implementation occurs through many avenues including private and public investments, grant programs, the use of regulatory tools such as zoning and design review, construction of catalyst projects and other actions. Boise has experienced successful redevelopment in the Central District, where investments of public funds have precipitated a five-fold investment of private funds. Construction of the Grove Plaza, reconstruction of downtown streets and installation of brick sidewalks, street trees and planters, turning 8th Street into a pedestrian-oriented street where restaurants and shops thrive, creating the transit mall and building public parking garages have brought about a transformation of

Boise's downtown core. Entering into partnerships with private developers and using land write-downs has resulted in renovation and reuse of historic buildings and new development.

This success has continued in the River Street–Myrtle Street Urban Renewal District with development of the River Plaza Apartments and office building and construction of the Ada County Courthouse, Civic Plaza apartments and parking garages in the Courthouse Corridor.

The River Street–Myrtle Street Master Plan sets forth objectives, design and development guidelines and action steps that describe how this area should develop over the next 20 years.

This plan will help guide public and private actions as development proceeds so that they complement each other and yield greater results than what otherwise would occur if no plan were in place. These results will not be achieved, however, without commitment, dedicated effort and investment from both the public and private sectors. This section of the plan describes what resources are available and what steps must be taken in order to implement the River Street–Myrtle Street Master Plan.

- Improvement districts
- Federal and state funding programs for transportation and transit projects, housing, community development, business development and other funding programs
- Federal tax credit programs
- Grants and donations

River Street–Myrtle Street Financial Plan

The initial financial analysis for the Old Boise–Eastside study area showed that it might not generate sufficient tax increment revenue to pay for anticipated improvements. As a result, the CCDC Board of Commissioners evaluated combining the Old Boise–Eastside area with the existing River Street–Myrtle Street Urban Renewal District as part of this master planning process. The advantages of having a larger land area in an urban renewal district are discussed in Chapter 1.

At the conclusion of the master planning process for both the River–Myrtle planning area and the Old Boise–Eastside planning area, Keyser Marston Associates prepared a financial analysis for a combined River Myrtle–Old Boise Urban Renewal District and for Old Boise–Eastside as a stand-alone district. In both cases, the financial analysis projects that sufficient resources from tax increment revenue and other sources will be available to pay for the proposed improvements. In the intervening time between the preliminary and final analysis for Old Boise–Eastside, the assumptions regarding the

Resources for Public Investment

Tax increment financing (TIF) is the primary means for funding public improvements in an urban renewal area. Idaho State Code allows for the establishment of urban renewal districts and the creation of revenue allocation areas to provide a source of TIF funding for urban renewal activities. When a revenues allocation area is created, a portion of the property taxes generated from increases in assessed valuation are allocated to the urban renewal agency. (See sidebar on p. 12 for more information on TIF.) These funds must be used in the urban renewal district for activities that are intended to redevelop and revitalize the area.

Other sources of funding for redevelopment activities include:

- Revenues from public parking and transit systems operated by the urban renewal agency
- Revenue bonds

value of future development and the cost and extent of improvements were refined for this area and led to a positive result. (The financial analyses for the combined River Myrtle–Old Boise District and Old Boise–Eastside are available from CCDC.) As noted in Chapter 1, during the public review of this plan, property owners in the existing River Street–Myrtle Street district expressed concerns that adding Old Boise–Eastside would drain resources from the River Street–Myrtle Street district and leave some areas of this district without resources. To address this concern, this plan includes CCDC’s commitment to work with a stakeholder committee to prepare a capital investment plans for each successive five years remaining in the life of the district. The stakeholder committee would include property owners, business owners and residents in the district. This committee would make recommendations to the CCDC Board of Commissioners on the type and timing of public investments in the various subdistricts of River Myrtle–Old Boise Urban Renewal District. These subdistricts are shown in Figure 11. The CCDC Board of Commissioners recommended and the Boise City Council elected to proceed with a combined River Myrtle–Old Boise Urban Renewal	District because of the advantages of larger district described earlier. This financial analysis includes the following: ■ Assumptions about the amount, type and timing of development that would occur during the planning period. Development assumptions are used to estimate the amount of property tax revenue that would flow to CCDC from the revenue allocation area. ■ Assumptions about the number and size of parking garages that would be built and estimates of revenue that would be generated. ■ Estimates of other likely sources of funding. ■ List of potential projects CCDC would undertake to revitalize both Old Boise/Eastside and River Street–Myrtle Street areas during the next 20 years. These projects are intended to be catalysts that will trigger private investment in the district. The table below lists the type and estimated costs for proposed revitalization projects expected to be built in River–Myrtle during the 20-year planning horizon. It provides a description of the anticipated public investments in the River–Myrtle area.
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Revitalization Projects & Costs

Parking Garages (4)	\$19,312,500
Transit Facilities/Downtown Circulator	\$20,400,000
Local	\$ 4,080,000
Federal	\$16,320,000
Pioneer Corridor	
Streets	\$ 5,522,000
Broad Street Extension	\$ 1,074,400
10 th Street Extension	\$ 638,000
Streetscaping/Sidewalks/Curb & Gutter	
Broad Street	\$ 867,680
10 th Street Extension	\$ 237,600
Capitol Boulevard	\$ 2,000,000
BoDo	\$ 1,100,000
Grove Street (6 th to 5 th)	\$ 420,000
Other Streetscape	\$ 3,937,500
Property Acquisition	\$ 1,800,000
Sewer/Storm Drainage	\$ 750,000
Water Quality Treatment	\$ 300,000
Utility Undergrounding	\$ 1,000,000
Telecommunications Facilities	\$ 800,000
Geothermal Capacity Increase	\$ 600,000
Traffic Signals	\$ 800,000
Road Re-surfacing	\$ 200,000
Julia Davis Park Expansion	\$ 480,000
Other Park/Open Space	\$ 200,000
Housing	\$ 600,000
Facade Easements/Improvements	\$ 100,000
Improve Vehicular/Pedestrian Access	\$ 300,000
Special Projects	\$ 400,000
Cultural Investments	\$ 169,438
Total	\$64,009,118

It should be noted that the financial plan does not guarantee that all of these improvements will be made. Over time, the amount of tax increment produced will depend on the development activity in the River Myrtle–Old Boise Urban Renewal District. The flow of revenues may be higher or lower than projected in the financial plan, which would lead to adjustments in the improvements undertaken. The amount of funding allocated to each type of improvement may be adjusted depending on opportunities that arise or needs that develop. The financial plan is a approximate picture of the types and amount of investment that will occur given what is now known, based on assumptions about the amount of development activity and its value, tax rates, financing, construction and operating costs and other factors.

River Street–Myrtle Street Master Plan as Part of Boise City Comprehensive Plan

City decision makers use the Boise City Comprehensive Plan to evaluate development applications. The Boise City Zoning Ordinance states that approval for zoning revisions, land subdivisions, conditional uses and design review requires a finding that the proposal under consideration be consistent with the comprehensive plan. Existing urban renewal plans for the Central, River Street–Myrtle Street and Westside Downtown districts have been adopted by reference as part of the comprehensive plan, and thus are used in determining consistency when a development proposal is located in one of those districts. Initiating an amendment to the compre-

hensive plan to include the River Street–Myrtle Street Master Plan update is one of the important next steps in implementing this plan, so it will also be used in making consistency findings.

Zoning Revisions

Zoning is one of the key tools for implementing any long-range plan for revitalization and development. The River Street–Myrtle Street Master Plan establishes a vision and desired direction for this area, expressed through plans, objectives, design and development guidelines and action steps. The Boise City Zoning Ordinance actually regulates how property owners can use their property. Plan implementation is more effective when there is consistency between urban renewal plans and the zoning regulations.

Current zoning designations in River Street–Myrtle Street District are shown in Appendix B. A majority of the property in the district is in the C-5D, C-5HD and R-OD districts. A minor amount of land is in the C-2 and C-3 districts. The City of Boise has achieved some success in implementing the Downtown Boise Plan by establishing the Central Business (C-5) and the Residential Office (R-O) zoning classifications. Development has responded

as planned for the most part, although there have been some intrusions of suburban-style development along the Connector. Typically these developments place surface parking lots between the building and the street and densities are lower than desired.

This section discusses the current zoning districts in place in the River–Myrtle planning area that may need attention in order to create greater consistency between this plan and zoning regulations.

C-5 and R-O Districts

The C-5 district encourages urban density, pedestrian-oriented development while R-O is oriented toward a mix of lower intensity residential and office development.

For the most part, C-5 is in the center of the planning area between 11th and 5th streets. R-O fills the wings of the planning area extending east from 5th to Broadway and west from 11th to Americana. C-5 more or less coincides with the West Connector, Warehouse/Cultural District and part of the East Connector/Parkside subdistrict west of 5th Street and R-O with the River Street Neighborhood, East Connector/Parkside east of 5th Street, and Courthouse Corridor subdistricts.

One of the issues identified during the planning process was the effect of the R-O district on the housing situation in the River–Myrtle planning area. This zoning district treats residential and office uses as equal, but in reality market pressures in the River–Myrtle planning area encourage converting houses to offices and replacing residential structures with office buildings. The River Street–Myrtle Street Master Plan includes development guidelines to retain and increase the supply of housing in the River Street and Parkside neighborhoods, and to encourage the development of more housing in the Courthouse Corridor, all three of which are under R-O zoning. Possible revisions to the R-O district, or development of an urban residential district should be considered to address how to encourage housing development in downtown.

Another issue related to the R-O zoning district pertains to the Courthouse Corridor. The level of development intensity being built in the corridor is much greater than is typical for the R-O district. The typical height limit for R-O is between 35 and 47.5 feet; the Ada County Courthouse is 100 feet and the Idaho Water Center is 75 feet in height. These developments received approval for height exceptions which are allowed by City Code with approval by the Planning & Zoning Commission. The

allowance for these larger buildings was in keeping with the location of the corridor on the Connector, which is a major thoroughfare. These projects have now set a benchmark, and developers are likely to seek height exceptions for future projects. Whether the Courthouse Corridor should continue under R-O zoning or whether it should be changed to C-5 or a new zoning district allowing higher intensity, mixed use development needs to be evaluated.

C-2 and C-3 Districts

The C-2 district provides for commercial areas that fulfill needs for travel-related service and retail sales areas along travel routes and within community shopping centers. Allowed uses include automobile sales lots, service stations, minor auto repair shops, car washes and convenience stores. The C-3 district provides for service businesses that are more intensive than those permitted in other commercial zones and which are semi-industrial in character. Examples of businesses allowed in this district include auction establishments, automobile-related businesses including sales lots, services stations, minor and major auto repair and car washes, construction businesses, heavy machinery supply, seed and garden supply, storage businesses, warehouses, truck stops, transit terminals and convenience stores.

The overall intent of this master plan is to promote urban intensities and a pedestrian-oriented character in the River–Myrtle planning area. C-2 and C-3 promote a lower intensity, auto-oriented character more typical of outlying areas. Many of the allowed uses in these districts would be inappropriate in a downtown setting and are prohibited in C-5 and R-O, which cover much of the downtown area.

This plan recommends that the C-2 and C-3 be replaced by a different zone category more conducive to achieving the goals of the plan.

Evaluating and possibly revising the zoning designations in River–Myrtle planning area is an important step in creating consistency between the plan, existing land use patterns and development regulations. The plan includes action steps regarding the C-5, R-O, C-2 and C-3 zoning designations in the subdistrict plans for the West Connector, River Street Neighborhood, East Connector/Parkside and Courthouse Corridor.

DD and HD Overlay Districts

The Boise City Zoning Ordinance includes a “down-town design review” or DD designation that applies to properties located in an urban renewal district. This designation requires that development applications be referred to CCDC for comment prior to the city taking action. CCDC provides comments on the degree to which the development proposal implements the plan for that district. The Planning & Zoning Commission, Design Review Committee and City Council take such comments into consideration when making decisions on development proposals.

The zoning ordinance also includes a “historic design review” or HD designation that applies to properties in city-adopted historic districts. In some cases, properties are included in both a historic district and an urban renewal district. Where such overlaps occur, the properties are shown on the zoning map with an HD designation rather than both an HD and DD designation. The regulations governing the HD designation do not include a requirement that development applications for properties that are in both a historic district and an urban renewal district be referred to CCDC for comment. In actuality, Boise City staff refers such cases to CCDC for comment in order to maintain a consistent process for all properties in urban renewal districts. Revising the zoning ordinance to include referral of development applications to CCDC when properties with an HD designation are also in an urban renewal district would recognize this practice.

Appendix A: River Street–Myrtle Street Urban Design Plan - Excerpts

A. OVERALL GOALS AND OBJECTIVES

- 1. New, mixed-use redevelopment should reinforce activities in greater Downtown Boise:**
 - Expand the focus of regional employment, services, entertainment, cultural, government uses with additional residential, office, and retail uses—including neighborhood and community shopping uses;
 - Re-establish River Street and Myrtle Street areas as mixed use neighborhoods;
 - Capitalize on the recently adopted R-O zoning to expand the choices of housing types for Boiseans;
 - Ensure an affordable component in new housing projects;
 - Limit the concentration of social service programs within the study area;
 - Maintain Downtown Boise as the center of Federal, State County and City institutions particularly Administrative, Judicial, Legal and public uses;
 - Accommodate long term expansion of Central Business District uses primarily on large, undeveloped parcels west of Capitol Boulevard;
 - Encourage pedestrian activity and increase pedestrian safety.
- 2. Build on existing attributes within the area:**
 - Extend the character of the Greenbelt and Julia Davis Park into adjacent subdistricts;
 - Expand the range and strength of cultural uses in the 8th Street area. Incorporate public art to identify, define and enhance streetscapes, public spaces and neighborhoods;
 - Reinforce established uses that contribute to the diversity of activities in Downtown Boise;
 - Preserve and reinforce areas of existing urban character.
- 3. Foster an urban character in redevelopment:**
 - Respect existing urban neighborhood development patterns: blocks, grid streets, mid-block parking, street oriented buildings and detached sidewalks with street trees;
 - Encourage a pedestrian orientation of new development in the design and location of buildings, streets, parking and open space;
 - Provide flexibility in the design of buildings for the changing nature of uses, anticipating the influences of information technology, live-work, etc.;
 - Provide a continuity of pedestrian connections such as sidewalks, entry paths and signalized crossings;

-
- Develop and overall parking strategy for the greater Downtown area that includes the following:
 - Maximum as well as minimum parking requirements for various uses (the initial phase of a metropolitan area plan);
 - Neighborhood protection ordinances for spill-over parking;
 - A transportation management plan for larger projects with incentives for shared parking, car pooling, transit use, bike use, shuttle service and pedestrian orientation of development;
 - Incentives for below grade and structured parking as well as air rights development over surface parking and/or structured parking.

4. Create memorable and dignified streets that complement the activity and design of adjacent uses:

- Reinforce a hierarchy of streets in Downtown Boise not only by traffic capacity but also by design treatment of sidewalks, crosswalks, street trees, lighting and furnishings:
 - The five distinct design treatments are Urban Street, Parkway, special Parkway at Capitol Boulevard, Neighborhood Street and Visual/Circulation Access Corridor (see *Street Character Framework* for detail description);
 - Ensure coordination between the pedestrian and bicycle route system and the public open space system;
- Incorporate public art to identify, define and enhance streetscapes, public spaces and neighborhoods.

B. THE CONCEPT

1. A vital central city made up of urban, mixed use subdistricts will be strengthened and diversified.
2. A framework of streets, paths and open spaces will build on existing Downtown Boise networks and strengthen connections to the river.
3. Residential uses with an affordable component will be re-established in River Street and Myrtle Street areas.
4. The character and benefits of the Boise River Greenbelt and Julia Davis Park will be extended into and connected to the subdistricts.
5. An emerging cultural focus in the Eight Street subdistrict will be reinforced and expanded.

Map Key

R-1C	Single Family Residential	A	Open Land
R-2	Combined Residential	U	University
R-3	Multi Family Residential	C	Capitol Boulevard Overlay
N-O	Neighborhood Office	H-D	Design Review (DR) Overlay
L-O	Limited Office	DD	Historic DR Overlay
R-O	Residential Office	DD	Downtown DR Overlay
C-1	Neighborhood Commercial	H	Historic Overlay
C-2	General Commercial	CD	Conservation District Overlay
C-3	Service Commercial	DA	Development Agreement
C-5	Central Business District	- - -	Zoning Boundaries
H-S	Health Service	- - -	Urban Renewal Districts

Appendix B: Supplemental Changes

RIVER STREET-MYRTLE STREET MASTER PLAN SUPPLEMENTAL CHANGES MADE TO AUGUST 23, 2004 DRAFT ADOPTED BY THE CCDC BOARD OF COMMISSIONERS AND TRANSMITTED TO THE BOISE CITY COUNCIL IN THE OCTOBER 12, 2004 FINAL DRAFT

This list contains changes that were made to the August 23, 2004 Draft of the River Street–Myrtle Street Master Plan adopted by the CCDC Board of Commissioners, which were accepted by the Boise City Council as part of its action to approve the River Myrtle–Old Boise Urban Renewal Plan, except as otherwise noted. Text that was added is underlined; text that was removed is shown by strikeouts. The reason for each change is noted in parentheses and italics at the end of each item. The plan document titled Final Draft for City Council–October 12, 2004 incorporated these changes. A separate list identifying these changes was entered into the record at the October 12, 2004 public hearing on the River Myrtle–Old Boise Urban Renewal Plan.

In the August 23, 2004 draft, there were comments highlighted in blue that explained where text might need to be modified, added or deleted based on Boise City Council action or when information was pending. Where these comments were resolved prior to October 12, 2004, they were removed without a note being included in the list below.

A limited number of very minor changes to the August 23, 2004 Draft were not included in the list sent to the Boise City Council. Examples include adding a horizontal line at the top of a page consistent with the page format used throughout the plan, bolding subheadings, removing background street names on maps to reduce clutter, and capitalizing words that are consistently capitalized elsewhere but got missed in a particular instance. (The Boise City Council was made aware of this caveat in the transmittal of the list.)

Please Note: After City Council action on November 30, 2004 to approve the River Street–Myrtle Street Master Plan, minor changes were made to the plan document to prepare it for publication. These changes do not affect the substance of the document. They are listed at the end of this appendix.

Cover

1. Final Draft for City Council – October 12, 2004

Credits

2. Corrected Zimmer-Gunsul-Frasca to Zimmer Gunsul Frasca Partnership (*remove hyphens; use correct name*)

Table of Contents

3. p. ii, List of Figures: 11:A: West Connector (Former Raily Yards) (*maintain consistency with how "rail yards appears elsewhere in plan)*

Chapter 1: Introduction

4. p. 6, 1st column, 1st bullet: ...Broadway and Avenue B on the east to 17th Street... (*change th to superscript to maintain consistency with format used throughout plan)*
5. p. 14, 2nd column, 2nd paragraph: Recognizing that property owners, business owners and residents in Old Boise/Eastside have a desire for a master plan that focuses on their area, t The urban renewal plan for the combined River-Myrtle/Old Boise district has been structured so it includes two master plans: one for the original River Street-Myrtle Street area urban renewal district and one for the Old Boise/Eastside study area, with some modification of the boundary between them. The map in Figure t Figure 11 on page 63 shows how the two areas relate to one another. The Central Business District and Eastside neighborhood shown on the map are in the Old Boise/Eastside planning area and the remaining subdistricts are in the River-Myrtle planning area. This approach maintains continuity in the planning work done for the River-Myrtle area. tPreparing two master plans also recognizes that the property owners, business owners and residents in Old Boise/Eastside who participated in the development of the initial development concept and development goals and policies for this area have a desire for a master plan focused on Old Boise/Eastside. (*Text that appears in August 25, 2004 plan was taken from the Old Boise/Eastside Master Plan and needed to be rewritten so it fits in the River-Myrtle Master Plan.*)

Chapter 2: Eligibility for Urban Renewal Activities

6. p. 16, 1st column, 1st paragraph: ...dipicteddepicted in Figure 1: (*correct spelling*)

Chapter 3: Planning & Policy Context

7. p. 18, 1st column, 1st bullet: The Downtown Boise Plan indicates anticipates that the CBD will extend into the area... (*better wording given this is a plan*)
8. p. 18, 1st column, 1st bullet: Regardless of the differences between these plans on the extent of the CBD, These plans are in agreement with the Downtown Boise Plan, except for the nature when describing the type of development expected in the CBD, which is expected to have i.e. high intensity, compact development; with a diverse mix of uses, high level of pedestrian use and 24-hour activity. (*Clearer wording contrasting the differences in the plans re: the extent of the CBD and agreement between the plans on the types of uses anticipated in the CBD.*)
9. p. 20, 2nd column, last paragraph to p. 21: During the preparation of the River-Myrtle Master Plan, it became obvious that describing a vision and writing goals and objectives, and doing mapping for each individual district often causes duplication. It also tends to obscure the message that there is one unified vision for the long-term revitalization of downtown Boise: - the vision and many of the design and development

guidelines for downtown redevelopment are common to all of the urban renewal districts. *(better wording; no change in meaning)*

10. p. 21, 1st column, 1st paragraph: In addition, it is helpful to look at certain elements such as land uses, streetscaping, ~~public~~ civic spaces, bicycle and pedestrian routes, and transportation and parking on a downtown-wide basis to see the overall picture. *(civic is term used consistently in plan)*
11. p. 21, 1st column, 1st paragraph: One of the goals of downtown redevelopment is to arrive at a point where these districts function as interconnected places, ~~and where~~ land uses and activity centers in one district complement and support the other districts and downtown functions as an organic whole. *(better sentence construction)*
12. p. 21, 1st column, 2nd paragraph: The 2025 Downtown Boise Redevelopment Plan was created to illustrate the seamless nature of downtown and to describe both the overall vision and goals for its revitalization and the design and development ~~objectives and~~ guidelines shared by all the districts. It depicts the common elements on maps that show all of the districts and the periphery around them so these elements are put into context. Plans, ~~development~~ objectives, ~~and~~ design and development guidelines and action steps that apply to a particular district are stated in the master plan for that district. *(better match text in 2025 Plan; no change in meaning)*
13. p. 21, 2nd column to p. 22, 1st column; list of overall goals for downtown revitalization:

Urban form: ... Develop subdistricts, activity centers and in-town neighborhoods so ~~that each have their own unique~~ identity has its own character and sense of identity. *(better sentence construction; subject-verb agreement)*

Transportation system: ~~Transform the existing~~ Create an effective multimodal transportation system into a well-designed, integrated and effective multimodal system in downtown Boise that promotes walking, bicycling, carpooling and transit, ~~as alternatives to the use of automobiles and~~ reduces dependence on automobiles. Ease traffic flow in downtown by improvements to street design and signalization and by traffic demand management. Develop a multimodal transportation system for the region that addresses potential traffic impacts caused by future growth in downtown, and makes travel to, from and within downtown by alternative modes practical and attractive. *(2025 plan includes revised version of this goal; revisions make transportation goal more comprehensive; recognize automobiles as a factor in transportation; address traffic flow, traffic demand management and regional transportation system)*

Parking system: Provide an adequate supply of parking while minimizing the use of land for surface parking lots. Achieve maximum utilization of parking by developing land uses that have offsetting parking demands, making private parking available to the public when not needed otherwise and ~~connecting parking facilities to~~ activity centers by ~~alternative means of transportation~~ making public parking facilities easily accessible from the various subdistricts and activity centers in downtown. *(clearer wording; correct typo in facilities)*

Chapter 5: Preferred Development Concept

14. p. 27, Figure 4: add Americana Blvd to map *(helpful reference for reader)*
15. p. 32, footnote: In spring 2004, CCDC conducted a consumer survey of people living in Boise and in Ada County to measure the level of interest in downtown living. Two of the questions focused on what areas of downtown were considered most desirable as a place to live. ~~The Cultural District ranked third after the Heart of Downtown and Old Boise. The River Street and Parkside districts ranked fourth and fifth ahead of the Westside District, which ranked last.~~ When respondents were asked whether they were interested in living in each of six different areas of downtown, they chose the Cultural District more frequently than River Street, Parkside and Westside districts but less frequently than the Heart of Downtown or Old Boise. When asked to pick their top choice among the six areas, approximately 12% of the respondents chose the Cultural District, 12% chose the River Street District, and 9.7% chose the Parkside District as their first choice. *(use more accurate and more understandable description of survey results)*

Chapter 6: Overall Plans, Guidelines and Action Steps

16. p. 35, Figure 5: add Americana Blvd to map *(helpful reference for reader)*
17. p. 36, column 2, 1st paragraph: It shows a concentration of mixed use, office, institutional and service commercial uses along the Connector, Capitol Boulevard, Broadway and Americana. These streets are classified as principal arterials and carry high volumes of traffic; relative to other downtown streets. Given their functional classification and design, traffic and these volumes on these streets are expected to increase as downtown intensifies over the next 20 years. (Estimates of traffic demand in 2025 on streets in the River-Myrtle planning area are discussed in Section 5: Transportation and Parking of this chapter. These estimates were derived from traffic modeling done as part of the Downtown Boise Mobility Study in 2004.) It is difficult to locate residential uses... *(Response to question raised by CCDC Board member; correct spelling.)*
18. p. 39, Figure 6: add Americana Blvd to map *(helpful reference for reader)*
19. p. 42; column 2, Guideline 3.2: Apply the Design and Development Guidelines regarding building and site design contained in the 2025 Downtown Boise Redevelopment Plan to development proposals in the River-Myrtle planning area. *(maintain consistency with references elsewhere to 2025 Boise Redevelopment Plan—include Boise in title; use caps.)*
20. p. 43, Figure 7: add Americana Blvd to map *(helpful reference for reader)*
21. p. 45, column 2, 3rd paragraph: Descriptions of the types of streetscape treatments used in the 2025 Downtown Boise Redevelopment Plan are provided below. Figure 8 shows where these treatments are expected to be used in River-Myrtle planning area. *(maintain consistency with references elsewhere to 2025 Downtown Boise Redevelopment Plan—include Boise in title; use caps.)*
22. p. 47, Figure 8: add Americana Blvd to map *(helpful reference for reader)*
23. p. 49, Figure 9: add Americana Blvd to map *(helpful reference for reader)*
24. p. 51, 2nd column, Guideline 4.3: Use the Streetscape Character map (Figure 8) to identify the type of streetscape treatment required for the streets in the River-Myrtle planning area. Apply the streetscape design

-
- standards from the 2025 Downtown Boise Redevelopment Plan when streetscapes are improved in the River-Myrtle area. *(desire to use consistent term for this plan throughout document)*
25. p. 54, 1st column, 1st paragraph. This analysis shows that the intersections of 3rd, 5th, 6th, and 9th streets with Front Street will operate at a level of service (LOS) F during the PM peak hour, and steps need to be taken to prevent downtown from choking on commute traffic in the future. *(Waiting on results from DBMS—may need to be rewritten.)*

New material added based on preliminary results from Downtown Boise Mobility Study as follows:

The DBMS proposes a policy for that a level of service (LOS) of D is acceptable during peak hours on downtown streets (see sidebar). This policy recognizes that downtown is more congested than is typical in other areas of Boise, and a certain level of congestion is an acceptable tradeoff in order to maintain downtown Boise as the region's urban center. The traffic analysis showed that a number of intersections in the River-Myrtle planning area will have a level of service (LOS) of E or F in 2025 during peak hours. These include all intersections on Front and Myrtle except 11th and Myrtle, Grove and 9th, Main and 9th, Main and 6th, Broadway and University, Lincoln and University, Capitol and University, River and 9th and Americana and Shoreline.

Most of the congestion predicted by the traffic model is associated with commuter traffic between the region and downtown since downtown is expected to continue growing as an employment center.

Creating a multimodal transportation system for downtown Boise that encourages the use of transit, bicycling and walking as alternatives...

26. p. 54, column 2, paragraph 2: For example, for neighborhoods south of the Boise River and on the Boise Bench, there are only three main crossing points over the Boise River into downtown using that use roadways: Broadway at the east end of the River-Myrtle planning area, Capital Boulevard-8th Street-9th Street Broadway at the east end and, in the center, the three bridges carrying 9th Street, 8th Street and Capitol Boulevard—and. There are also two pedestrian bridges that provide a means of crossing the Boise River from the south.¹ For people crossing at Capitol Boulevard-8th Street-9th Street the center point, 8th Street as the route to and from downtown... *(clearer wording for reader not familiar with Boise)*
27. p. 57, Figure 10: add Americana Blvd to map *(helpful reference for reader)*
28. p. 58, Guideline 5.8: Evaluate Parcel A (bounded by 15th, 13th and the Connector) and Parcel B (bounded by 13th, 11th and the Connector) in the West Connector subdistrict... *(explains location of Parcels A and B)*

Chapter 7: Subdistrict Plans, Guidelines & Action Steps

29. p. 63, Figure 11: add Americana Blvd to map *(helpful reference for reader)*

-
30. p. 66, Guideline WC-16: ... the the Warehouse/Cultural District, The Grove Plaza and CBD. *(maintain consistency in name of The Grove throughout plan)*
31. p. 67, 2nd column, 2nd bullet:
- ~~Implement the streetscaping standards in the 2025 Downtown Boise Redevelopment Plan when street improvements are required as part of a development project or whenever improvements are made to streets in the West Connector subdistrict. *(fix nucky format in plan)*~~
32. p. 75, footnote: ...Also across Capitol Boulevard is the Basque Block, where the Basque Museum and Cultural Center is located, and the Flicks Theater. *(add comma)*
33. p. 80, 2nd bullet: ...for Fall for Boise, the city's annual month-long celebration of the arts, to breathe life into this concept. *(add comma)*
34. p. 85, 1st column, 1st paragraph: The Courthouse Corridor is bounded on the north side by property owned by Ada County, Broadway Avenue on the east, Julia Davis Park on the south and 3rd and 2nd streets on the west; the University of Idaho property on the west side of Broadway between Front and Myrtle Streets, Kreizenbeck office building at 251 E. Front Street, Adams property on the northeast corner of Myrtle and Avenue A and the Winco warehouse grocery store. It is bordered by the West Connector/Parkside subdistrict, Old Boise/Eastside planning area, St. Luke's Regional Medical Center, MK Plaza, and Julia Davis Park and the Boise River. *(duplicate and confusing deleted)*
35. p. 89, Guideline CC-11: Work with property owners to improve the Broad Street alignment from 2nd Street to Broadway Avenue as a pedestrian promenade, ~~that works as~~ which provides a walking and bicycling connection through their properties. Recommend this connection be included when reviewing new developments projects traversed by this alignment when the Broad Street alignment crosses or abuts these projects. ~~Restore Broad Street as~~ Reconstruct a street on the Broad Street alignment where feasible. *(clearer wording)*

Chapter 8: Implementation

36. p. 96, 1st column, list of figures: Correct amount for parking garages from \$21,187,500 to ~~\$19,312,500~~; for property acquisition from \$2,550,000 to ~~\$1,800,000~~; cultural investments from 196,875 to ~~\$169,438~~. Correct total from \$66,661,555 to ~~\$64,009,118~~. *(correct figures based on revised economic feasibility study)*
37. p. 97, 1st column, 3rd paragraph: The City of Boise has achieved some success in implementing the Downtown Boise Plan by establishing the Central Business ~~District~~ (C-5) and the Residential Office (R-O) zoning classifications. *(use correct district name)*

* * * * *

Additional Corrections for Publication

After City Council action on November 30, 2004 to approve the River Street–Myrtle Street Master Plan, necessary corrections were made to the plan document to prepare it for publication. These corrections were very limited and did not affect the substance of the document. They include the following items:

The usage and capitalization of the terms River Street–Myrtle Street Urban Renewal District, River Street–Myrtle Street Master Plan, River–Myrtle planning area was made consistent throughout. References to the Old Boise–Eastside Master Plan, which is a companion document, and to the Old Boise–Eastside planning area have been changed so the format of the name is as shown rather than Old Boise/Eastside.

Duplicate or misspelled words, stray punctuation and errors in subject-verb agreement have been corrected. Page numbers and figure numbers that were highlighted in blue (in order to assist in proofreading the document) have been changed to black. Notes in blue indicating text that needed to be edited once the outcome of the City Council's action was known have been removed, and the text has been edited appropriately and changed to black.

References to the 2025 Downtown Boise Redevelopment Plan (which sometimes appeared under this name and sometimes under 2025 Downtown Redevelopment Plan) have been changed to be consistent throughout except for the title in the sidebar on page 16—where the title would be too long if the full name was used.

In the specific plan for the East Connector/Parkside subdistrict, the following corrections were made:

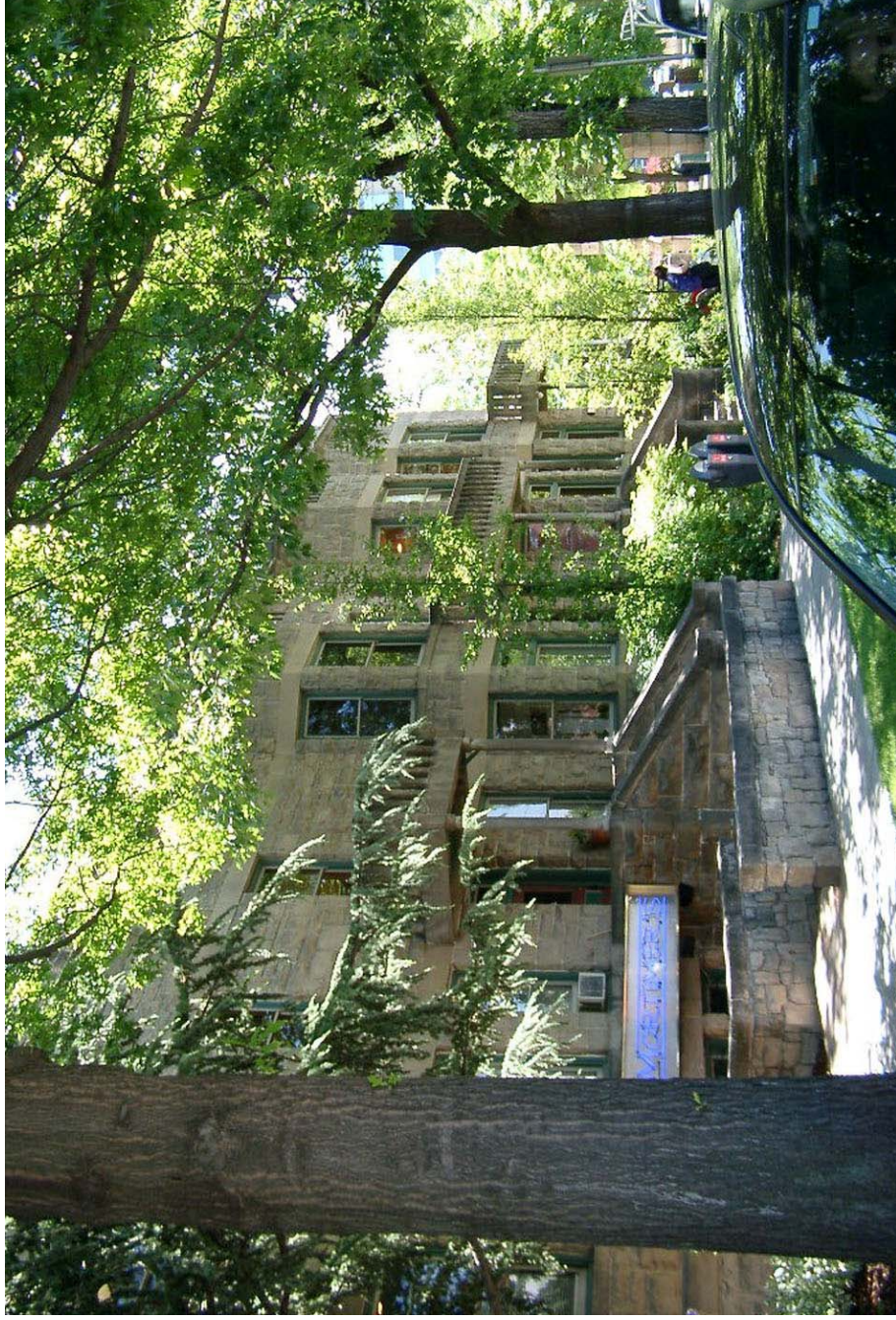
- 38. p. 82, EC-4; p. 84, 1st bullet, 2nd bullet: Change West Connector/Parkside subdistrict to East Connector/Parkside subdistrict (*error in name of subdistrict*)
- 39. p. 83, EC-9; p. 84, 7th bullet: Change reference to Warehouse/Cultural District to East Connector/Parkside subdistrict (*word processing error from copying text*)

In the specific plan for the Courthouse Corridor subdistrict, the following corrections were made:

- 40. p. 88, 2nd column, 2nd paragraph: Every effort should be made to moderate the impact of ~~transform~~ Front and Myrtle ~~into boulevard-style streets to moderate its impact on adjacent properties~~ with by creating a more appealing pedestrian environment along their edges. This plan envisions the Connector transformed into a set of ~~more~~ boulevard-style streets that continues to provide a high capacity vehicular route to, from and through downtown, but are also compatible with a downtown setting. (*correct incomplete, scrambled sentence*)
- 41. p. 89, 1st column, title: Design ~~and~~ & Development Guidelines (*correct format to match other titles*)
- 42. p. 89, CC-4, CC-5; p. 90, 3rd, 4th, 5th, 6th bullets under action steps; p. 91, 4th bullet: Change Courthouse Corridor to Courthouse Corridor subdistrict (*make reference consistent*)
- 43. p. 89, CC-9; p. 91, 2nd bullet: Change Warehouse/Cultural District to Courthouse Corridor subdistrict (*word processing error from copying text*)
- 44. p. 90, 5th bullet under action steps; last line: Change Corridor to Courthouse Corridor subdistrict (*make reference consistent*)



Old Boise–Eastside Master Plan



Prepared by Capital City Development Corporation

Boise, Idaho
Approved by the Boise City Council
November 30, 2004

Approved by the Boise City Council

(with supplemental changes to the August 23, 2004 version approved by the CCDC Board of Commissioners--see Appendix B)

Ordinance #6362

Adopted: November 30, 2004

Effective Date: December 6, 2004

David Bieter, Mayor
Jerome Mapp, Council President
Maryanne Jordan, Council Pro-Tem
Vernon Bisterfeldt
Elaine Clegg
David Eberle
Alan Shealy

Approved by the

CCDC Board of Commissioners

Date: August 23, 2004

Resolution #1002

Kevin McKee, Chairman
Dale Krick, Vice Chairman
Cheryl Larabee, Secretary/Treasurer
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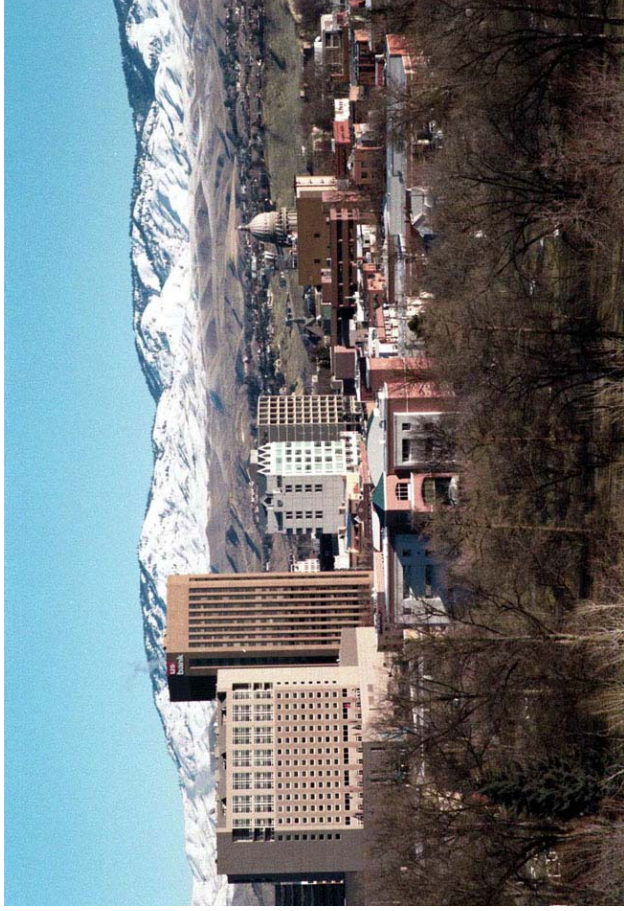
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1: Introduction



Since its beginning, Boise has been an oasis for travelers and a place for people to settle and build. Nestled in a valley, next to the Boise river and near the foothills, Boise is protected from the harsh desert climate that surrounds it.

The place now known as Boise has been a focal point for human contact and trade since the Shoshoni and Bannock Indians used the Treasure Valley as a “gathering place for bartering goods, taking healing baths in the hot springs along the river, and telling the stories of their lives.” French trappers searching for beaver gave the place its name in 1811. After a grueling trip across the desolate Snake River plain, they came upon a place that had ready access to a beautiful river lined with trees and surrounded by rolling hills. They shouted “Les bois” (the woods) in wonderment and relief and Boise became known as an oasis for the traveler and a place with promise. Boise served as a way station on the Oregon Trail—the ruts are

still visible outside of town. Fur traders and pioneers were followed by miners and then entrepreneurs, settlers, farmers and ranchers, each seeking opportunity and reward from the land. The citizens of Boise held their first town meeting in 1863, and “the next day platted an area of ten city blocks between the river and the foothills.” Three of these blocks are located within the area covered by this master plan. Boise City was chosen as the territorial capital in 1864 and then became the state capital in 1890. Boise City remained a modest-sized town for a long time, but was the only town of significant size for a very long way in every direction.¹

¹ Source: Barbara Herrick, *Boise: A Global Community in the West*, Community Communications, Inc. (1996), p. 17.

This master plan addresses the Old Boise/Eastside area of downtown Boise (see Figure 1). Signs of history are everywhere in this area. The Assay Office at 3rd Street, built from local sandstone and situated in a park-like setting on a full city block, attests to the mining era. The storefronts on Main and Idaho streets west of 5th Street were part of the city's original business district. This area continues to be a vibrant part of downtown Boise with restaurants, bars and shops. The lovely shaded streets east of 5th Street are lined with Victorian and craftsman-style homes from the 1800s and early 1900s. Many of these homes have been converted to businesses but the residential feel remains. The Belgravia Building, built in 1904 on the southeast corner of 5th and Main, began as an apartment building and now is occupied by businesses. With its bay windows, stone staircases and street trees, it lends a dignified and gracious air to Main Street. The Veltex Building built in 2004 on the northeast corner of 5th and Main complements the Belgravia Building with historic references in its design. Nearby at 5th and Grove streets, C.W. Moore Park celebrates the Boise City Canal by bringing the water in the underground canal up to the surface where it powers a water wheel. It provides a reminder of the importance of irrigation to the entire Treasure Valley. With its shade trees, benches and the sound of flowing water, the park provides a quiet and welcome respite from the surrounding city.

The Old Boise–Eastside area is recognized as a place of history and one of the most attractive areas of downtown in this present day. The district has two distinct areas: Old Boise, which is part of the central business district and is known for its restaurants and nightlife, and

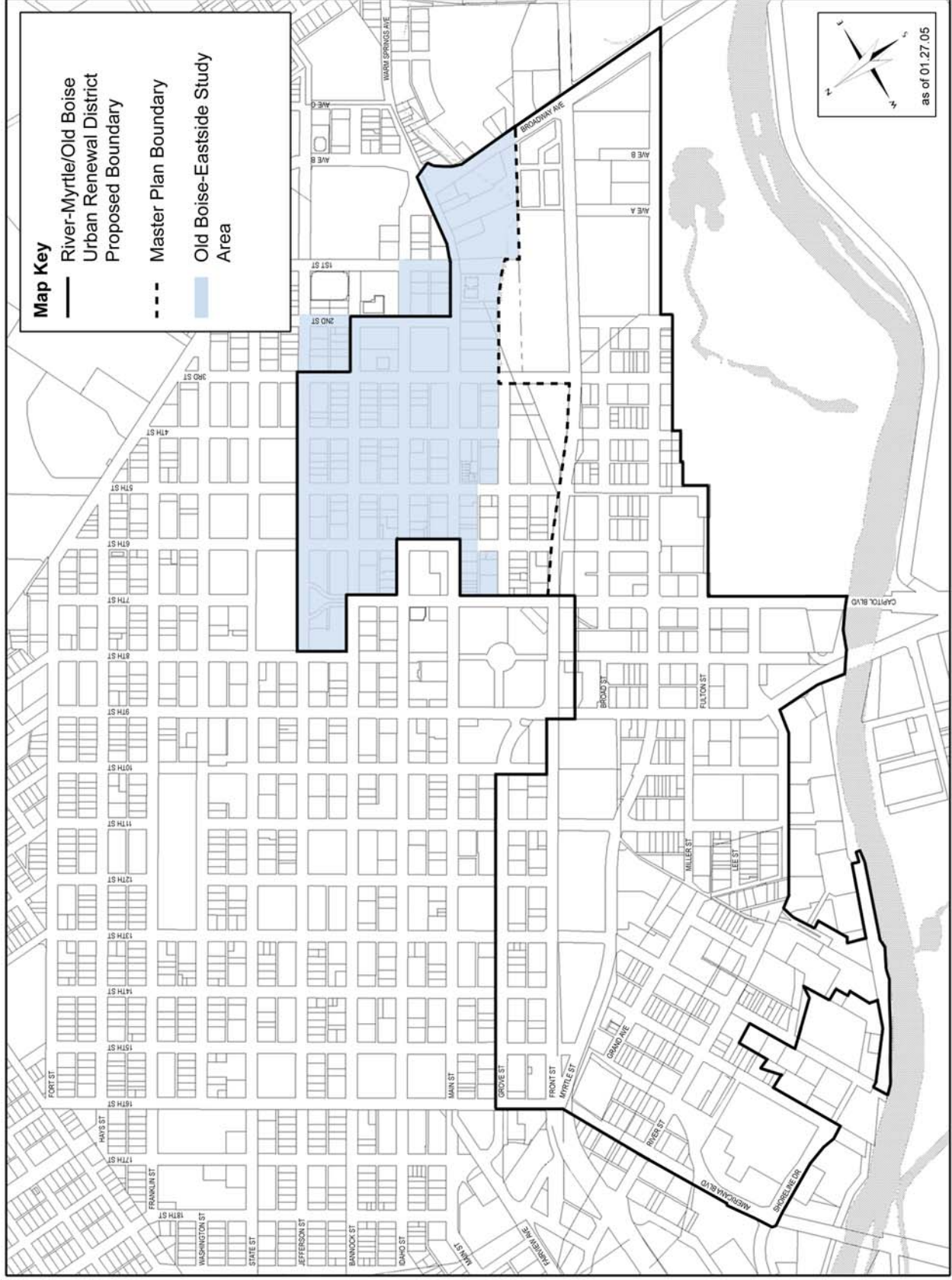
the Eastside Neighborhood, with streets lined with historic houses used as residences and offices and lush greenery. It is uncertain whether the area will remain a place that includes both a business district and a residential neighborhood or if it will become a place where government and medical offices and other businesses become the predominant use, and residential uses disappear.

As a part of the larger downtown, Old Boise–Eastside faces challenges to its long-term vitality. Capital City Development Corporation (CCDC), the urban renewal agency for Boise, has prepared this master plan to better define the desired future for Old Boise–Eastside, to identify the challenges it faces in achieving this future and to establish objectives, guidelines and action steps for use in making decisions regarding development, redevelopment and public improvements so the end result is consistent with this desired future.

Reasons for Initiating Old Boise – Eastside Master Plan

Development of a master plan for Old Boise–Eastside was initiated in summer 2000 after several property owners and other stakeholders sent letters requesting that CCDC consider creating an urban renewal district for Old Boise. They expressed concern about Old Boise lagging in prosperity behind the Central and River Street–Myrtle Street urban renewal districts. Some were interested in aesthetic improvements such as street trees, historic lighting and brick sidewalks in order to extend the attractiveness of downtown into Old Boise. Others

Figure 1: River Myrtle–Old Boise Urban Renewal District



Tax Increment Financing

Tax increment financing (or revenue allocation as it is called in the Idaho State Code) is the method established for funding revitalization projects and activities in urban renewal districts. At the time an urban renewal district is established, a city council also decides whether to create a revenue allocation area that coincides with the district. If this area is created, the county assessor establishes the current value for each property in the district. This value is referred to as the *base value*. Over time, as both public and private dollars are invested and development occurs in the district, property values tend to rise. The increase in value over the base is called the *incremental value* or increment. In Idaho, the taxes generated by this incremental value are shared by the school district and the urban renewal agency. In Boise, the city council has formed revenue allocation areas for each of its urban renewal districts. The revenues derived from the increment for urban renewal activities are allocated to CCDC as Boise's urban renewal agency.

identified the need for adequate parking as an issue to be addressed. The stakeholders viewed the formation of an urban renewal district as a means to capture future property tax revenues, which would be generated in any event, and to assure they would be re-invested in improvements to benefit Old Boise–Eastside.

Idaho Code requires the creation of an urban renewal district to be accompanied by an urban renewal plan. The typical timeframe for an urban renewal plan is 24 years. An essential element of this urban renewal plan is a master plan which states the desired future character for the district and sets forth a development concept, objectives and guidelines to assist decision makers with the physical development and redevelopment of the district. The master plan also includes action steps describing how urban renewal resources should most effectively be applied.

The actual master planning process for Old Boise–Eastside began in summer 2002 with the establishment of the study area and the hiring of a consultant team to assist in the effort. The study area is shown in Figure 1. The effort included five community meetings, meetings with individual property owners, a workshop with the Boise City Historic Preservation Commission, a market analysis, a traffic analysis and the creation of a preferred development concept. The Old Boise–Eastside Master Plan is the result. It addresses:

- Eligibility for Urban Renewal Activities
- Current Conditions
 - Existing Character of Old Boise–Eastside
 - Planning & Policy Context
 - Evolving View of Old Boise–Eastside

- Market Context
 - Initial Development Concept
 - Market Analysis & Forecast
 - Preferred Development Concept
 - Specific Plans, Guidelines & Action Steps
 - Implementation

Chapter 6: Specific Plans, Objectives & Guidelines discusses particular issues of interest in Old Boise–Eastside. These are:

- Preferred Land Uses
- Preservation, Revitalization & Renewal
- Building & Site Design
- Streetscapes, Civic Spaces & Pedestrian Ways
- Transportation & Parking

Old Boise–Eastside as part of River Street–Myrtle Street District

Idaho State Code requires that an economic feasibility analysis be done before a district is formed to determine how anticipated tax increment revenues compare to estimated costs for revitalization projects and programs. In the case of Old Boise–Eastside, a preliminary analysis was done in spring 2003. It showed that anticipated tax increment revenues from Old Boise–Eastside might not be enough to cover the expected cost of improvements (see sidebar).

At the time, some of the factors seen as reasons for this situation were:

- Constructing at least one public parking garage and upgrading the Old Boise business district to have the same streetscaping and pedestrian amenities as the downtown core are expensive.
- Old Boise–Eastside does not have large unused parcels that could generate significant new development.
- The preferred development concept for Old Boise–Eastside endeavors to complement the existing character of the area and thus shows only a modest amount of new development.
- The amount of tax increment expected from the preferred development concept for Old Boise–Eastside is correspondingly modest.

Amending the River Street–Myrtle Street District to include Old Boise–Eastside

To address this situation, the CCDC Board of Commissioners decided to evaluate what the financial situation would be if the Old Boise–Eastside area were added to the existing River Street–Myrtle Street Urban Renewal District.

Adding Old Boise–Eastside to the River Street–Myrtle Street district offered the following advantages:

- The number and size of parcels suitable for redevelopment varies between different geographic areas of downtown Boise. As a result, the redevelopment potential and the ability to generate tax increment varies between these areas. The need

for improvements in different geographic areas also varies. There are instances where the ability to generate revenue and the need for improvements are not a perfect match. Having a larger land area in an urban renewal district tends to moderate these differences and allows a balance between costs and revenues to be achieved.

- Certain types of improvements, such as parking garages, have areas of benefit or service areas. When an urban renewal district is small, service areas may extend beyond the boundary of the district. The district cannot access resources generated by development that is in the service area but outside the district. When urban renewal districts are larger, it is more likely that service areas for improvements will fall within the district boundaries.

- Typically bonds are used to finance improvements in urban renewal districts. The ability to issue bonds depends on whether investors believe that sufficient revenues will be generated to repay the bonds. In the case of bonds backed by tax increment revenue, the ability to repay bonds depends on future development generating increment. How much and what type of development activity will occur in a small geographic area is more unpredictable than for larger areas. Having a larger area in an urban renewal district increases the likelihood that development projections will be met and investors will be willing to buy bonds.

- Certain types of development, like a lifestyle retail center, have the potential for generating significant tax increment and for creating spin-off development that also generates revenues. Such developments have very specific locational requirements and there are only very limited locations that will work for them. Having a larger urban renewal district allows revenues from these unique developments to be reinvested over a wider area than is possible in a smaller district.

- A geographic area may have characteristics that are valued on a community-wide basis. It may make a unique contribution to the economic prosperity of downtown which would damage downtown's competitive position and synergy if lost. The Old Boise Historic District is an example. The contribution made by such areas would be enhanced by public improvements, but compromised if wholesale redevelopment were undertaken. Placing such areas in a larger district makes it possible to improve these areas without overdeveloping them.

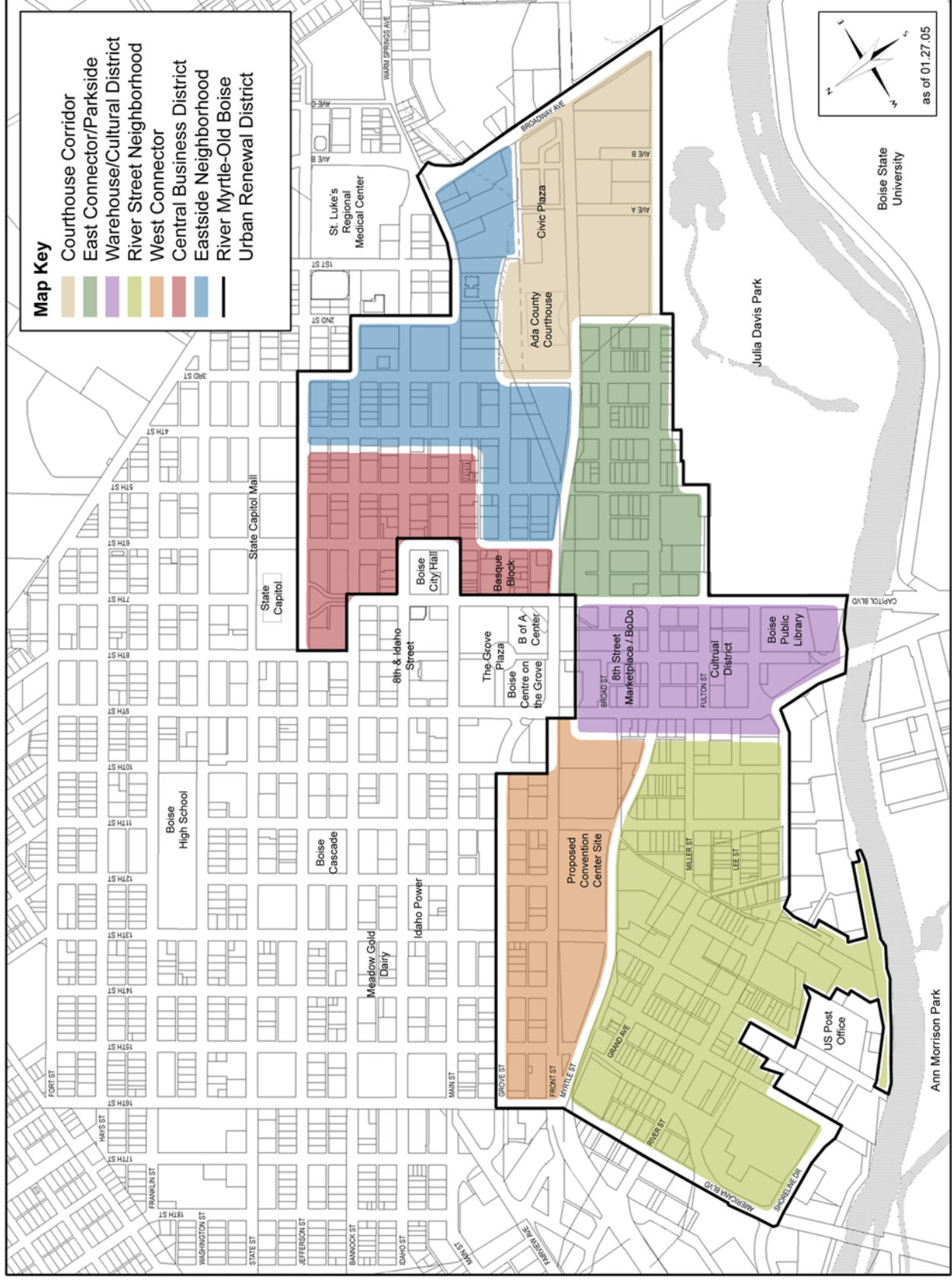
Adding Old Boise–Eastside to the existing River Street–Myrtle Street district also raised the issue of geographic distribution of public investment and redevelopment activity. During the public review of this plan, property owners in the River Street–Myrtle Street district expressed concerns that adding Old Boise–Eastside will drain resources from the River Street–Myrtle Street district and leave some areas of River Street–Myrtle Street district without resources.

Long Term Financial Plan for River–Myrtle and Old Boise–Eastside

At the end of the master planning process for both the River–Myrtle planning area and the Old Boise–Eastside planning area, Keyser Marston Associates prepared a financial analysis for a combined River Myrtle–Old Boise Urban Renewal District and for Old Boise–Eastside as a stand-alone district. In both cases, the financial analysis projects that sufficient resources from tax increment revenue and other sources will be sufficient to pay for the proposed improvements. In the intervening time between the preliminary and final analysis for Old Boise–Eastside, the assumptions regarding the value of future development and the cost and extent of improvements were refined for this area and led to a positive result. (The financial analyses for the combined River Myrtle–Old Boise District and Old Boise–Eastside are available from CCDC.)

The CCDC Board of Commissioners recommended and the Boise City Council elected to proceed with a combined River Myrtle–Old Boise Urban Renewal District because of the advantages of larger districts discussed earlier.

Figure 2: River Myrtle–Old Boise Subdistricts

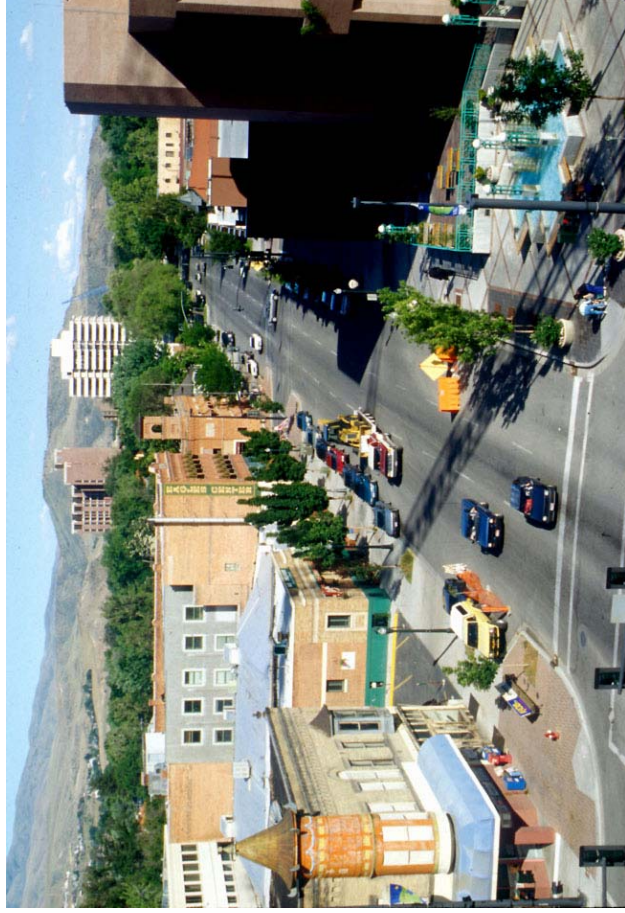


At the same time, CCDC wanted to address the concerns expressed by property owners about the geographic distribution of resources. This plan includes CCDC's commitment to prepare a capital improvement plan for the River Myrtle–Old Boise district every five years for the remaining life of the district, and that the five-year plan be re-examined each year as part of CCDC's annual budget process. The first five-year plan would cover 2006-2010. A stakeholder committee including property owners, business owners and residents in the district would assist in the preparation of the five-year plan and make recommendations to the CCDC Board of Commissioners on the type and timing of public investments in the various subdistricts of the River Myrtle–Old Boise district. These subdistricts are shown in Figure 2.

Relationship between River Street–Myrtle Street & Old Boise–Eastside Master Plans
Recognizing that property owners, business owners and residents in Old Boise–Eastside have a desire for a master plan that focuses on their area, the urban renewal plan for the combined River Myrtle–Old Boise district has been structured so it includes two master plans: one for the original River Street–Myrtle Street district and one for the Old Boise–Eastside study area.¹ The map in Figure 1 shows how these areas relate to one another. As noted earlier, the area covered by this master plan is referred to as the Old Boise–Eastside planning area.

¹ A slight adjustment has been made to the boundary between the two master planning areas so that properties which were in the original River Street–Myrtle Street Urban Renewal District, but relate better to the Old Boise–Eastside area, are now covered by the Old Boise–Eastside Master Plan. These properties are located north of Front Street between Capitol Boulevard and 3rd Street.

2: Eligibility for Urban Renewal Activities



Old Boise—Eastside is a unique area that has much to offer. Improvements to streets and sidewalks, transit improvements and additional parking facilities would further enhance the area.

Preparation of an eligibility report represents the first step in deciding whether an urban renewal district should be formed. Idaho State Code requires that before a city or county undertakes urban renewal activities, the local governing body must determine the area proposed for urban renewal meets the definition of a deteriorated or deteriorating area. The local governing body makes this determination based on whether the area meets at least one of the criteria in the definition in the Idaho State Code.

The eligibility report for the Old Boise—Eastside study area was prepared by HWM Consultants. The report concluded that the Old Boise—Eastside study area is a deteriorating and deteriorated area based on the definition in the Idaho State Code and, as such, is appropriate for an urban renewal project. The primary factors leading to this conclusion were:

- Frequency of deteriorating buildings based on age of structures.
- Faulty lot layout in relation to size, adequacy, accessibility or usefulness.

- Deterioration of public improvements such as streets, sidewalks, alleys and sewer lines, and deteriorated private improvements such as gravel parking lots. Instances where there is inadequate telecommunications infrastructure were also noted.
- Diversity of ownership.
- Economic underdevelopment of the area.

The report contains a detailed analysis of the criteria in the definition of deteriorating and deteriorated area. Each criterion has been assigned a number and the map attached to the report shows, by number, where these conditions exist in the study area. (The Old Boise—Eastside eligibility report is available from CCDC.)

The CCDC Board of Commissioners accepted the eligibility report on September 10, 2001. The Boise City Council received the report in August 2002 and adopted Resolution #17216 at its meeting on September 10, 2002, thus determining that Old Boise—Eastside met the definition of a deteriorated and deteriorating area in the Idaho State Code and was eligible for urban renewal activities. The City Council also directed CCDC to commence preparing an urban renewal plan for this area for consideration by the CCDC Board of Commissioners and Boise City Council.

3: Current Conditions



Although many of the structures in Old Boise–Eastside are historic residential buildings, office uses dominate the area. A stronger residential neighborhood needs to be established to support the retail and restaurant district in Old Boise and add to the vitality of downtown.

Existing Character of Old Boise–Eastside

Old Boise–Eastside is readily identified as distinct from other areas of downtown. It is located at the eastern edge of the downtown core and is surrounded geographically by four major employers and generators of activity: State of Idaho, St. Luke’s Regional Medical Center, and Ada County and Boise City governments. The western half of the area (Old Boise) has a thriving restaurant and nightclub district and a strong identity created by the Old Boise Historic District and the Basque Block. Most of the businesses occupy authentic brick and stone buildings preserved from the early days of Boise’s founding. The strong commitment made to renovation and reuse of these buildings by the Carley

family and others and the formation of the Old Boise Historic District by the city have played a significant role in protecting the historic resources of this area.

Much of the eastern half of the district (Eastside) has a distinctive historic and residential feel with the original Assay Office from the mining era, impressive turn-of-the-century historic homes and streets lined with large shade trees. Other strengths include its proximity to jobs, entertainment and other downtown attractions, parks and public spaces, urban and pedestrian-scaled environment and traditional street grid with continuous sidewalks. It is a very gracious and walkable place that holds the affection of the community. (A more complete description of the Old Boise–Eastside area is contained in the Existing Conditions Report which is available from CCDC.)

In spite of its basic strengths, Old Boise—Eastside faces challenges to maintaining its vitality over time and the possibility that opportunities to maintain its unique character, attract investment and expand economic activity will be missed. The purpose of this plan is to address these challenges and opportunities in a way that reinforces and complements the existing character of the Old Boise—Eastside area and assures its long-term prosperity. The more significant challenges and opportunities are described below:

Challenges to vitality and existing character:

- The proximity of Old Boise—Eastside to the downtown core has led to rising property values which create market pressure for redevelopment and work against residential development.
- The location of State of Idaho offices and St. Luke's Regional Medical Center has encouraged the conversion of houses into offices within Old Boise—Eastside.
- When houses become offices, adjacent houses are often demolished to provide required on-site parking for offices.
- Urban vitality is eroded when surface parking lots proliferate.
- The trend toward office uses replacing residential uses dampens pedestrian activity in the area.
- The survival of restaurants and retail in the Old Boise Historic District is less certain as the residential base erodes.
- Long-term prosperity of restaurants and retail is also affected by lack of convenient short-term parking and public parking in general.

- Old Boise is at a disadvantage, in comparison to the downtown core, when it comes to access to public parking, quality of streetscapes and pedestrian amenities.
- The ability to finance public parking garages and streetscaping is beyond the resources of individual property owners.

Opportunities to maintain and enhance this area:

- The Old Boise—Eastside area has a character infused with history, which makes it very attractive as a business area and as a residential neighborhood.
- Market pressure needs to be channeled into development that complements the historic character of the area.
- Encouraging infill development at an appropriate level of intensity would reinforce vitality.
- Increasing the supply of housing in Old Boise—Eastside would reinforce the vitality of restaurants and retail businesses and keep this area vibrant after hours and on weekends.
- Functionally, the business district in Old Boise is an extension of the downtown core.
- Adding streetscaping and pedestrian amenities to Old Boise would help unify it with the downtown core and overcome the core's current competitive advantage.
- Construction of public parking garages rather than surface parking lots would put more land into productive use and reduce the deadening effect of surface lots.

Planning & Policy Context

The Old Boise/Eastside Master Plan should be considered in the context of other related plans. These include the Downtown Boise Plan (1993), Boise City Comprehensive Plan (1997) and East End Neighborhood Policy Guide adopted by Boise City and the River Street–Myrtle Street Urban Design Plan (1994) prepared by CCDC and adopted by the Boise City Council at the time the River Street–Myrtle Street urban renewal district was established. These plans serve as foundation stones for development of the Old Boise/Eastside Master Plan. The 2025 Downtown Boise Redevelopment Plan addresses the overall vision for the long-term development of and public and private investment in downtown. It also includes the development objectives, guidelines and standards that apply to all of the approved urban renewal districts.

Each of these plans is described below, with special note taken on how they relate to the Old Boise–Eastside area.

Downtown Boise Plan (1993)

Boise City adopted this plan “to preserve and enhance the strengths of downtown businesses by providing guidance for future growth and development in the downtown area.” The Downtown Boise Plan promotes downtown as a center of employment and services but also envisions downtown as a desirable place for residential neighborhoods. Figure 3 shows the different subdistricts in downtown identified by this plan and describes their intended character.

‘The goals and policies in the Downtown Boise Plan regarding the central business district (CBD) and the area immediately surrounding it are of particular relevance to Old Boise/Eastside. The plan considers the portion of the Old Boise/Eastside west of 4th Street and south of Bannock as part of the central business district. The plan encourages intense, compact development within the CBD with a mix of uses. The portion of Old Boise/Eastside generally located east of 4th Street and west of St. Luke’s Regional Medical Center, is viewed as a buffer area of higher density residential uses and offices. Boise City has achieved some success in implementing the Downtown Boise Plan by establishing the Central Business District (C-5) and the Residential Office (R-O) zoning classifications.

Boise City Comprehensive Plan (1997)

Adopted in 1997 as the general plan for all of Boise and its area of impact, the Boise City Comprehensive Plan designates almost the entire Old Boise/Eastside study area as Mixed Use. East of 3rd Street is designated Institutional in recognition of St. Luke’s Regional Medical Center. The comprehensive plan includes a land use matrix that compares the land use categories used in the plan with the zoning districts used in the Boise City Zoning Ordinance. The matrix indicates the mixed-use and the institutional designations are both open to a wide range of land uses and zoning districts, however, the institutional designation does not accommodate intense commercial or industrial uses. A footnote to the matrix explains that the mixed-use and institutional designations “may be most appropriately implemented with a Specific Plan.”¹

¹ St. Luke’s Regional Medical Center has a master plan that more specifically defines the area expected to develop as part of the medical center. Boise City has created a zoning category called Health Service or H-S that has been applied to this area. The H-S classification is bounded more or less by 2nd and Fort streets, Avenue A and Warm Springs Avenue. As such, it occupies a smaller area than identified as institutional in the Downtown Boise Plan.

New Urbanism Principles

New urbanism, when practiced in a downtown environment, envisions a lively mix of uses such that housing, employment, cultural, educational, recreational uses, health care and other services are in close proximity to each other, allowing people to satisfy daily needs by walking, bicycling and transit. Development is at urban densities, thus making such alternative modes of transportation more feasible. Dependence on individual vehicles for transportation is minimized. A mix of uses may occur in a single building or there may be single-use buildings with different uses within the same block or subdistrict of downtown. Retail uses are clustered into both small and large shopping districts. Housing is clustered into neighborhoods. Large concentrations of office buildings, which tend to go dark in the evenings and on weekends, are discouraged. Buildings are built close to or at the sidewalk line, using designs that create interest at the street level for passersby. Sidewalks are improved with street trees, furniture, planters and distinctive lighting and may include special paving. There is a system of public spaces with seating, plaza space, landscaping and art work where people can gather for relaxation, people-watching, socializing, dining and cultural events. Specific attention is given to creating a lively, welcoming public realm for people which structures and brings coherence to the urban fabric. The street system retains the traditional grid system common to downtowns at the turn of the century. Surface parking is minimized since it tends to sap urban vitality. Every effort is made to screen surface parking lots from street view. Where parking lots are on the street, they have an edge that replicates the building line along the sidewalk and are landscaped to soften their impact. Most parking is supplied by parking structures. Whenever possible, these structures are designed so the first floor is lined with shops, restaurants, housing units and other activating uses wherever this first floor faces a street, thus making the parking garage invisible to pedestrians.

'The Boise City Comprehensive Plan acknowledges downtown Boise as the civic, economic and cultural center of the Boise City Planning Area. Downtown Boise is expected to continue to develop with a concentrated, higher density central business district and integrated subdistricts each maturing under new urbanism principles as applied by specific design overlays (see sidebar).

East End Neighborhood Policy Guide (1988)

'The East End Neighborhood Policy Guide is a neighborhood plan originally adopted by the Boise City Council in 1988 and incorporated into the Boise City Comprehensive Plan in 1999. The neighborhood plan directs the city to consider the concerns of area residents as the Near East End (most of the Old Boise—Eastside area east of 6th) makes the “transition from residential to offices and support services.” The plan also supports “preservation of the historic character and scale and the adaptive reuse of historic structures.”

River Street—Myrtle Street Urban Design Plan (1994)

'The River Street—Myrtle Street Urban Design Plan was prepared at the time the district was formed in 1994, and served as the master plan for this district until adoption of the River—Myrtle Master Plan in 2004. The original River Street—Myrtle Street District did not include Old Boise—Eastside but the River—Myrtle Urban Design Plan identifies the Old Boise Expansion Area as a subdistrict and includes these guidelines:

- Reinforce the pattern of small-scale commercial and retail uses typical of the Old Boise area.
- Provide low-rise buildings (four stories or less) that are street-oriented with parking below or to the rear.

- Encourage the development of a consolidated parking facility for the area to relieve short-term (day and night) parking demand. This could be shared with government offices.

- Reinforce Grove Street as the primary pedestrian connection between the Old Boise area, the new development areas to the east and central downtown Boise to the west.

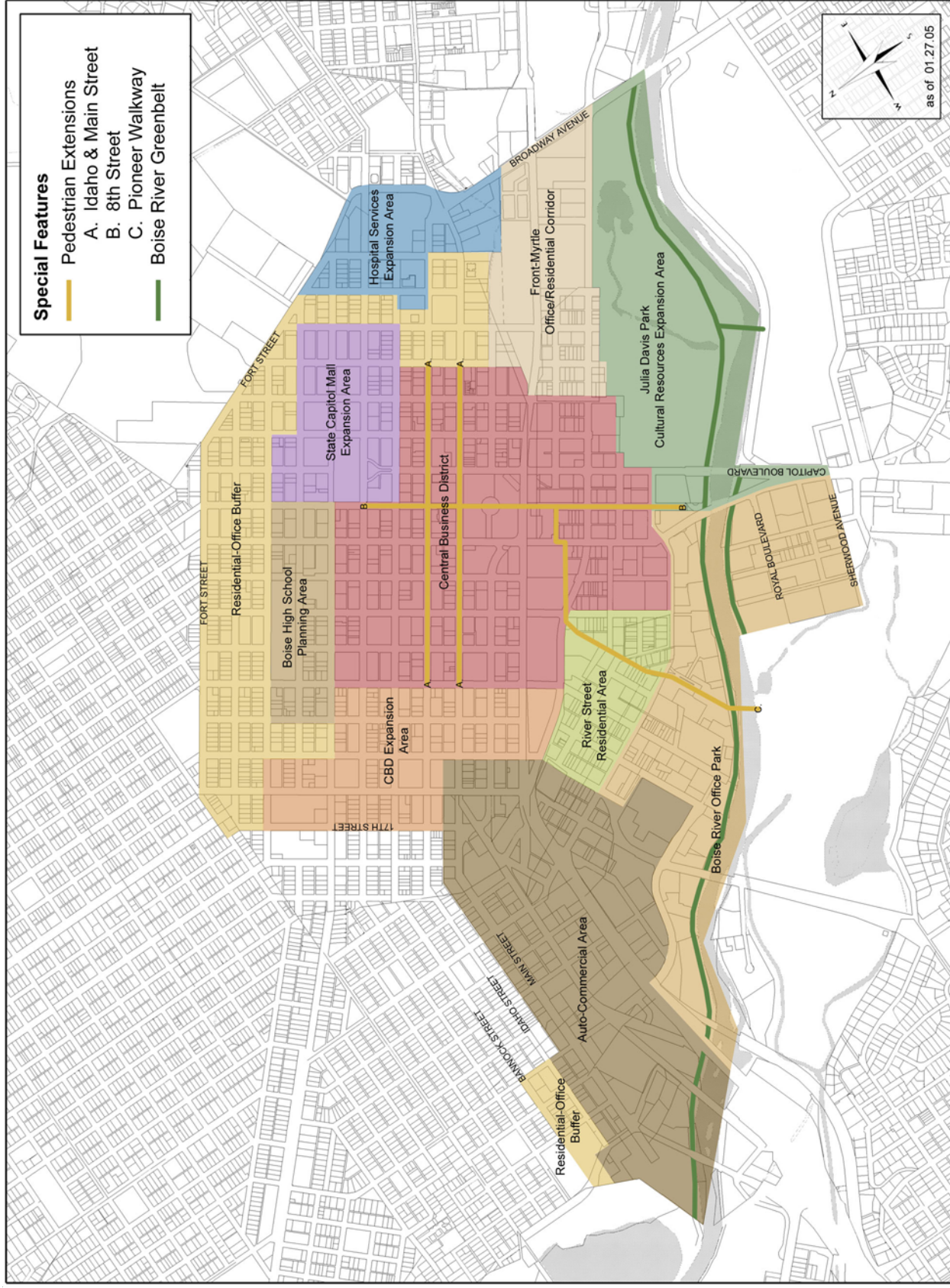
- Maintain the pedestrian scale of streets in the area.

'These guidelines have been taken into account in the preferred development concept for the Old Boise—Eastside area presented in this master plan.

2025 Downtown Boise Redevelopment Plan

Boise now has three urban renewal districts that encompass the heart of downtown as one coherent piece of geography: Central District, River Myrtle—Old Boise and Westside Downtown. During the preparation of the Old Boise—Eastside Master Plan, it became obvious that the vision and many of the design and development guidelines for downtown redevelopment are common to all of the urban renewal districts. In addition, it is helpful to look at certain elements such as land uses, streetscaping, civic spaces, bicycle and pedestrian routes and transportation and parking on a downtown-wide basis to see the overall picture. From a functional standpoint, the boundaries for the urban renewal districts are actually artificial lines that are needed to account for where tax increment revenue is generated and spent. One of the goals of downtown redevelopment is to arrive at a point where these districts function as interconnected places and where land uses and activity centers in one district complement and support the other districts, and downtown functions as an organic whole.

Figure 3: Downtown Boise Plan (1993) – Land Use



Vision of Downtown Boise - 2025

The 2025 Downtown Boise Redevelopment Plan envisions downtown remaining the foremost urban center in the region for business, government, culture, education and urban living. It is and will continue to be a collection of districts, each with its own unique character and function. Districts, activity centers and in-town neighborhoods will be connected by a well-designed and functional multimodal transportation system. This transportation system will provide connections within downtown and to the surrounding region.

Downtown will become a mosaic of delightful places that evoke a sense of community and invite people to explore and enjoy. It will have a rich collection of both historic and contemporary buildings—valuing its ties to the past as well as the possibilities of the future with architecture that conveys a sense of quality and permanence. These buildings will provide the setting for a lively mix of uses, including housing, offices, retail, hotels and convention facilities, and cultural, entertainment and learning opportunities. Downtown will become a model for sustainable land use patterns, building practices and transportation and how to make wise use of resources. Downtown will offer an invigorating environment with bustling streets, music, festivals, public art, parks and plazas. There will be a rich intellectual environment that attracts talented people and sparks creativity and innovation, supported by state-of-the-art telecommunications and technology. People from all walks of life will feel welcome and become engaged in the life of downtown. The history and culture of its peoples will be celebrated. There will be room for quirkiness and spontaneity. Downtown Boise will be a vibrant place where people thrive and businesses prosper—a place that contributes to the social, spiritual, environmental and economic well-being of residents, workers, visitors and the region.

The 2025 Downtown Boise Redevelopment Plan was created to illustrate the seamless nature of downtown and to describe both the overall vision and goals for its revitalization and the design and development guidelines shared by all of the districts. It depicts the common elements on maps that show all of the districts and the periphery around them so these elements are put into context. Plans, objectives, design and development guidelines and action steps that apply to a particular district are stated in the master plan for that district. The 2025 Downtown Boise Redevelopment Plan serves as a preface to these master plans.

The 2025 Downtown Boise Redevelopment Plan has a time frame of 20 years because the existing districts are scheduled to sunset on or before 2025. Boise City and CCDC have established a vision (see sidebar) and overall goals for downtown revitalization as part of this plan. Downtown revitalization goals include:

- **Heart of region:** Assure that downtown Boise remains the foremost urban center in the region for business, government, medical care, culture, education and urban living.
- **People place:** Develop downtown as a place for people, offering a delightful and enriching environment for living, working and playing. Assure that the mix of land uses and the design of the urban fabric support urban vitality. Emphasize the needs of people over vehicles.

- **Economic development:** Promote a prosperous economy in downtown and the region by creating a place that is attractive to cutting-edge companies and the workforce they employ.

- **Urban form:** Create a compact, urban form with a lively mix of uses. Emphasize quality, sustainability and a feeling of permanence in the how downtown is built. Develop subdistricts, in-town neighborhoods and activity centers so each has its own character and sense of identity.

- **Public realm:** Develop a system of parks, plazas and public spaces that become focal points in the downtown, act as catalysts for private development, provide refreshment and relief from urban surroundings and add to the livability of downtown. Improve streets and sidewalks so they create a welcoming pedestrian environment.

- **Transportation system:** Create an effective multimodal transportation system in downtown Boise that promotes walking, bicycling, carpooling and transit, and reduces dependence on automobiles. Ease traffic flow in downtown by improvements to street design and signalization and by traffic demand management. Develop a multimodal transportation for the region that addresses potential traffic impacts caused by future growth in downtown, and makes travel to, from and within downtown by alternative modes practical and attractive.

- **Parking:** Provide an adequate supply of parking while minimizing the use of land for surface parking lots. Achieve maximum utilization of parking by developing land uses that have offsetting parking demands, making private parking available to the public when not needed otherwise and making public parking facilities easily accessible from the various subdistricts and activity centers in downtown.

Evolving View of Old Boise–Eastside

In looking at these plans together, it is apparent that there has been an evolution of thought about what Old Boise–Eastside should become. The East End Neighborhood Plan, prepared in 1988, anticipates that Old Boise–Eastside will transition from a residential neighborhood to office and institutional uses. The Downtown Boise Plan (1993) and Boise City Comprehensive Plan (1997) define the central business district as including the western part of Old Boise–Eastside and encourage the concentration of retail and other business activity in the CBD. Unlike the earlier East End plan, these plans envision a buffer area of office and residential uses around the CBD that would include the eastern part of Old Boise–Eastside. Greater experience and an improved understanding of the market indicate:

- It is important to have a critical mass of residents in neighborhoods close to the downtown core to support retail and restaurant activity.
- Housing is essential to creating a vibrant and safe downtown with 24-hour activity.

- Housing is more successful if it is clustered into neighborhoods rather than placed on scattered sites.
- Office uses are very adaptable and can locate almost anywhere in downtown.
- As land values rise in downtown, office is often viewed as the most profitable use for downtown properties, and housing tends to be either converted to or replaced by office.
- The presence of major employers adjacent to Old Boise–Eastside has resulted in housing being converted to offices for businesses seeking to be close to these employers, which has accelerated the trend toward office uses.
- Housing in Old Boise–Eastside is close to disappearing.
- Creating emphasis areas for residential development where attention is focused on promoting housing will help housing regain its place in transition areas and re-establish in-town neighborhoods.
- The demand for retail is limited and is most successful if concentrated in shopping districts rather than attempting to stretch it along numerous street frontages in downtown.
- The current extent of the Old Boise business district is probably sufficient to meet the need for retail activity in this area of downtown.

Like previous plans for downtown Boise, the Old Boise–Eastside Plan follows the guiding principles of mixed land uses, compact, urban-style development and pedestrian-oriented urban design. The Old Boise–Eastside Plan is essentially consistent with the more

recent of these plans and moves these plans forward by focusing in particular on how to:

- Assure that the Old Boise business district will be seen as a seamless part of the downtown core.
- Create an appropriate balance between office and residential uses, such that housing does not disappear but increases.
- Accommodate new development while retaining the distinctive character of Old Boise—Eastside.
- Maintain the vitality and character of Old Boise—Eastside for the long term.

This plan also proposes specific objectives, development guidelines and action steps for this unique area of downtown Boise.

4: Market Context



Old Boise—Eastside is at the focal point for a number of intersecting economic and social forces. Its location close to the downtown core has led to rising property values, which usually stimulate more intensive development. Residential structures are being converted to or replaced by office uses. Expanding the housing supply would increase support for retail in downtown. There is a strong desire to preserve historic structures characteristic of this area. Plans for future development in Old Boise—Eastside need to find a balance point between these forces.

Preparation of the Old Boise—Eastside Plan included an examination of the market for office, retail and housing development to determine if the amount and form of development envisioned by the plan is achievable. Market analyses came from two sources: Old Boise—Eastside Master Plan: Market and Development Findings prepared by Leland Consulting Group and Downtown Boise Mobility Study: Market Analysis and Growth Projections written by Strategic Economics.

Initial Development Concept

As a first step, CCDC contracted with Zimmer Gunsul Frasca Partnership (ZGF) to identify opportunity sites in the Old Boise—Eastside area and draft the initial development concept. ZGF's work was based on preliminary ideas about the eventual character desired for the Old Boise—Eastside area. This initial concept emphasized the following principles that have been a consistent part of the planning for downtown Boise since 1986:

- Maintain a wide mix of uses.
- Emphasize a compact, urban style of development.

- Create a people-oriented rather than auto-oriented environment by creating public spaces, streetscaping and reducing surface parking.
- Provide long-term parking in structures.
- Redevelop historic structures and districts in a manner that both preserves the community's heritage and stimulates downtown development.

Using these principles, ZGF estimated how much development could be accommodated in Old Boise—Eastside based on the current use of land, parcel sizes, ability to aggregate parcels, existence of historic buildings and expected land-use patterns. The initial development concept suggested that the following uses would occur in Old Boise—Eastside:

- 350 to 450 units of housing
- 175,000 to 300,000 square feet of commercial space (office and retail)

Market Analysis & Forecast

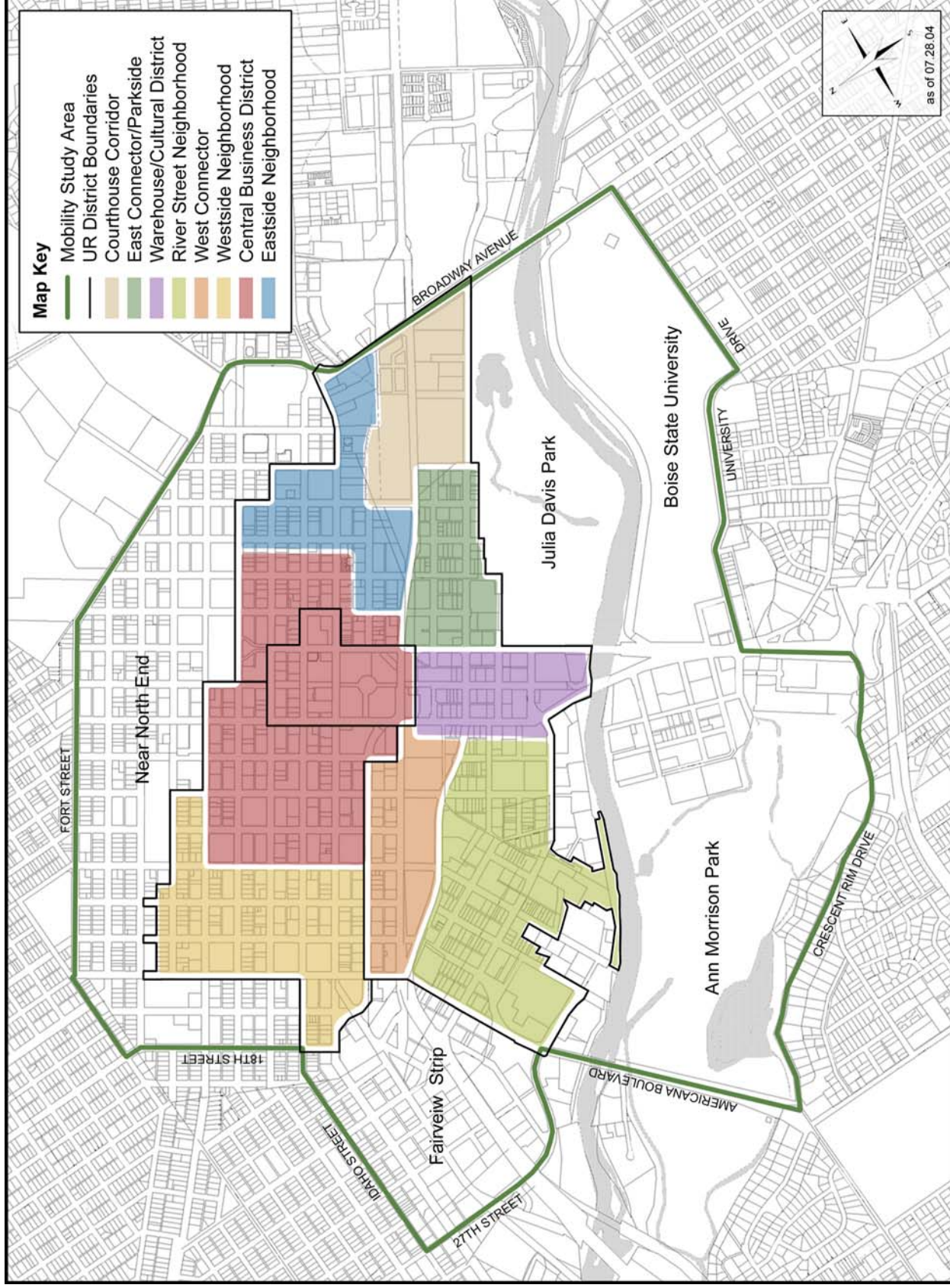
This initial concept was tested by Leland Consulting Group, which looked at the marketplace from two approaches: top down and bottom up. The top-down approach looks at the amount of market demand existing in the region and the amount that downtown as a whole, and Old Boise—Eastside in particular, would capture over the 20-year time span for the plan. The bottom-up approach looks at how much physical capacity is available to absorb development in Old

Boise—Eastside given availability of unused and underutilized land, parcel sizes, existing zoning, likely development patterns and the preferred development concept. The market analysis, prepared by Leland, concludes that the 350 to 450 units of housing anticipated in the development concept is achievable given population and housing trends for the region. The report also concludes it is reasonable to assume Old Boise—Eastside can absorb the 175,000 to 300,000 square feet of commercial space given the area's proximity to large employers, likely expansion of existing operations, and the need for supporting office uses.

The findings of the Leland market analysis have been compared to the market analysis and growth projections prepared by Strategic Economics in 2004 for the Downtown Boise Mobility Study (DBMS). The planning area for the DBMS is shown in Figure 4. Strategic Economics examined aggregate demand for housing, general and government-related office, and retail for the entire study area and then apportioned it to subareas including Old Boise—Eastside. This distribution of market demand was based on the existence of opportunity sites—sites that are either vacant or underdeveloped and likely to redevelop. Strategic Economics forecasts the following market demand for Old Boise—Eastside:

- 354 housing units
- 46,028 square feet of retail
- 289,238 square feet of office
- 381,150 square feet of government office

Figure 4: Downtown Boise Mobility Study Area & Urban Renewal (UR) Districts



Old Boise was apportioned a large share of government office because of its proximity to the State Capitol and inclusion of other state lands. (Four blocks of the Idaho State Capitol Mall are located in Old Boise/Eastside study area.)

Both Leland Consulting and Strategic Economics anticipate sufficient market will be available over the next 10-20 years to support the development concept proposed by the Old Boise–Eastside Master Plan.¹

¹The Strategic Economics projection relies on an aggregate forecast which was then apportioned to Old Boise–Eastside. It did not look in detail at existing conditions or take into consideration specific development objectives for the Old Boise–Eastside area, as did the Leland Consulting report. Leland's conclusions about office and retail are probably more realistic since they are more closely linked to the opportunity sites identified by ZGF, plan objectives and the preferred development concept. The economic feasibility analysis for the River Myrtle/Old Boise Urban Renewal District assumed 350 housing units and 175,000 square feet of office and retail in Old Boise–Eastside.

5: Preferred Development Concept



Creating a vigorous in-town neighborhood with a variety of housing choices is a key component of the preferred development concept for Old Boise–Eastside.

The preferred development concept for Old Boise–Eastside depicts the desired pattern of development for this area over the 20-year life of the plan. It forms much of the basis for the plan's policy emphasis and is used to predict the tax increment revenue that will be available to pay for the capital investments needed to implement the plan. It is intended to take the long view—to depict what Old Boise–Eastside will be like in the year 2025.

Desired Character for Old Boise–Eastside – 2025

An ideal result of implementing the Old Boise–Eastside Master Plan would be a vital and self-sustaining downtown district with the following characteristics:

- The Old Boise commercial area would increase in prosperity, retain its unique historic character and, at the same, time become a seamless part of Boise's central business district in terms of quality of streetscaping and public amenities.
- The amount and range of housing available in Eastside would increase and this area would be retained as a close-in, urban neighborhood.

- Historic buildings would be restored and fully occupied where appropriate.
- Weaker buildings would be replaced by infill development that complements the existing historic fabric.
- Blocks currently dominated by surface parking would be redeveloped with a mix of uses in structures designed to maximize the capacity of each block, giving consideration to the scale and character of adjacent buildings.
- On-street parking would be supplemented by structured parking, much of which would be available for public use, especially in the evening and on weekends.
- Street design and traffic controls would meet the needs of Old Boise–Eastside in its renewed state, with walking and bicycling encouraged by safe and convenient routes.
- Additional street trees and public open spaces would create a system of greenery that is well maintained and enhances the aesthetics and public enjoyment of the area.

Arriving at this vision for Old Boise–Eastside began with an inventory of existing conditions, developing a market forecast and identifying historically significant sites¹ and opportunity sites for development and redevelopment (see Figures 5 and 8). Old Boise–Eastside is in a very desirable location, given its proximity to the central business district and major employers, and has an attractive historic character and neighborhood ambiance. As a result, land values are rising in this area,

which tend to create pressure for higher intensity development. One of the questions that had to be answered as part of this master planning process was how much development might be attracted to this area and, more importantly, the type and amount of development considered desirable for the area.

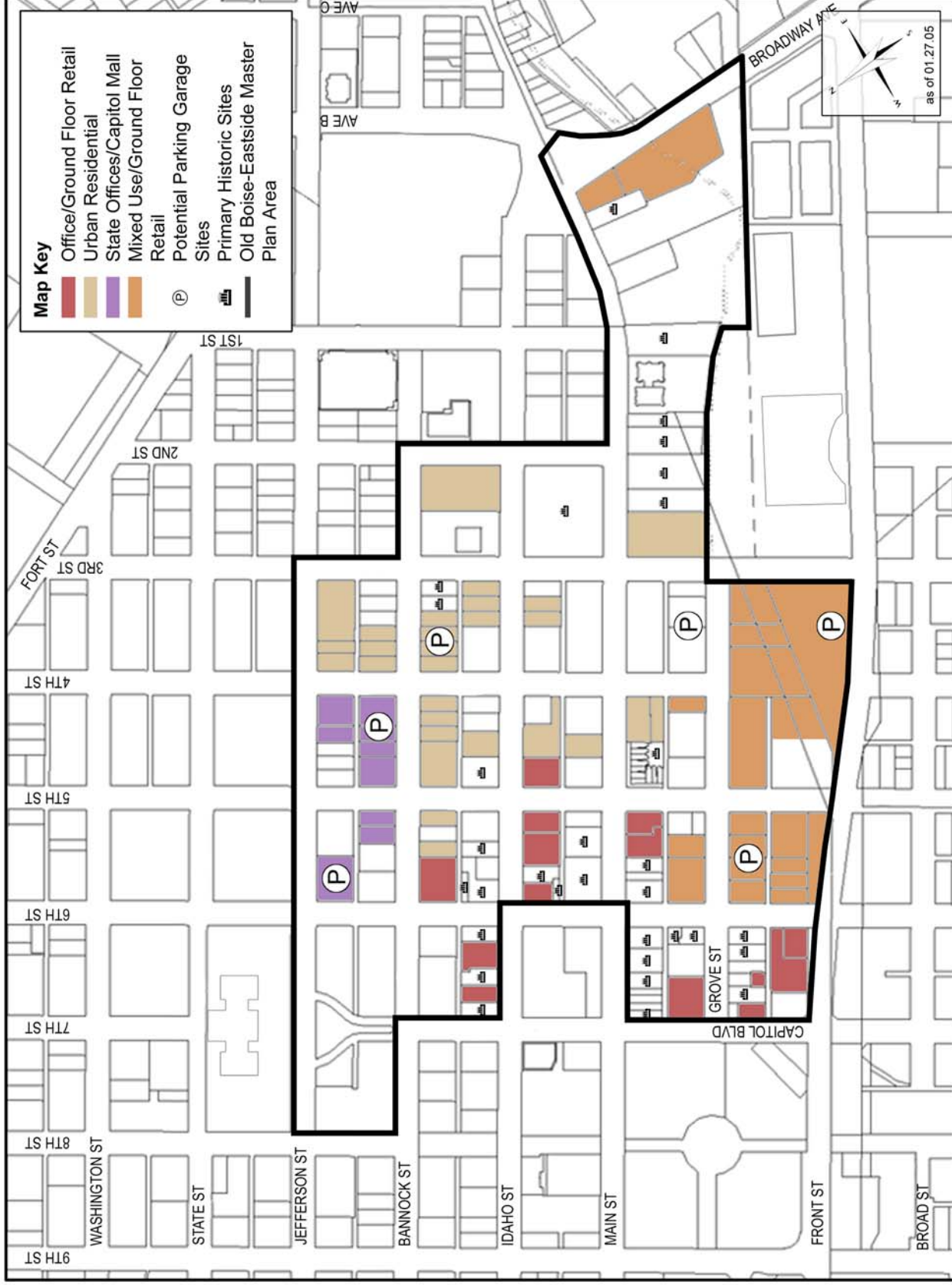
A series of stakeholder and community meetings were held to develop a vision for the future development of the Old Boise–Eastside area. A workshop was also held with the Boise City Historic Preservation Commission. This chapter focuses on the preferred development concept for Old Boise–Eastside, which emerged from these conversations, and how to achieve this concept.

Preferred Type & Amount of Development

The preferred development concept for Old Boise–Eastside does not envision a dramatic change in character of the physical environment, other than a modest increase in intensity and a better balance between land uses. At present, housing is diminishing in the Old Boise–Eastside study area through conversion to or replacement by office uses. Adding housing is seen as vitally important. Because housing will increase the number of people living in Old Boise–Eastside, it will promote retail vitality, allow people to work and live in downtown, reduce traffic congestion and increase safety. These results can be achieved through medium-scale infill development and redevelopment.

¹ For the purposes of this plan, historically significant sites are those occupied by either contributing or secondary historic structures as identified by two surveys of buildings in Old Boise–Eastside prepared for the Boise City Historic Preservation Commission. The first was conducted in 1979 and resulted in the establishment of the Old Boise Historic District. The second was conducted in 1985. See Figure 8 for full depiction of historic assets in Old Boise–Eastside.

Figure 5: Opportunity Sites



The development concept anticipates that 350 to 450 housing units and 175,000 to 300,000 square feet of office and retail space will occur in Old Boise–Eastside. Approximately 40% of the housing units are expected to be condominiums or townhouses, while 60% will likely be apartments. About 115,000 square feet of the office space is expected to be for government use due to the fact that some of the opportunity sites are located in the Idaho State Capitol Mall area where state facilities are planned.

It is important to note that the development concept expects only a small amount of new retail space. Much of the demand for retail will be absorbed in buildings already built for that purpose. Some retail might occur on the ground floor of new residential and office buildings, but this form of retail will mostly likely serve neighborhood needs and is not expected to be significant.

The preferred development concept is depicted in a broad-brush fashion in Figure 6. A mixed use area and four emphasis areas are identified. Emphasis areas have a predominant use but they are not expected to be limited to that use only. In fact, a mix of uses in each of the emphasis areas is encouraged so subdistricts are created where people can meet their needs locally by walking, bicycling and transit. The desired mix of land uses in the emphasis areas and the mixed use area are described below.

- **Retail/Office Emphasis:** Areas where office uses and street-level retail, restaurant and entertainment uses are concentrated, which could also have higher density residential and lodging uses.

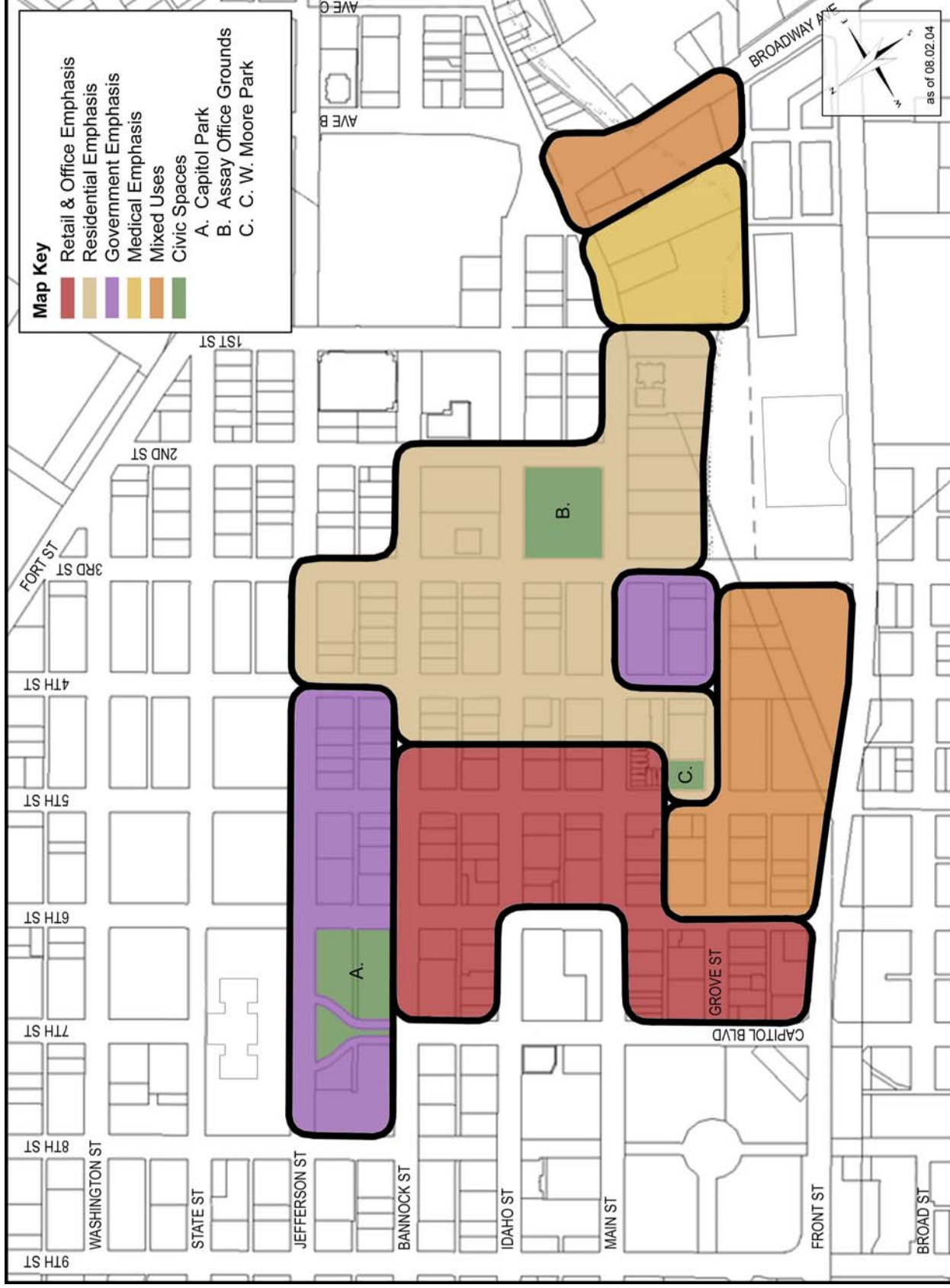
- **Residential Emphasis:** Areas with a mix of housing in terms of type and cost, usually clustered into neighborhoods, that could also have retail, restaurants, office, service businesses and other support uses that meet people's daily needs.

- **Government Emphasis:** Areas where government offices are concentrated. These areas could also include general offices, parking facilities, retail, restaurants and residential uses that complement the primary use. In the case of Old Boise–Eastside, this area recognizes the Idaho State Capitol Mall.

- **Institutional/Medical Emphasis:** Areas where medical offices and health-related institutional uses are concentrated. These areas could also include general offices, parking facilities, retail, restaurants, lodging and residential that complement the primary use. In the case of Old Boise–Eastside, this area recognizes the St. Luke's Regional Medical Center.

- **Mixed Uses:** Higher intensity areas that typically have a mix of office, residential, retail, restaurants and entertainment uses, and may also have lodging and convention or conference facilities. These areas typically have a high concentration of pedestrian activity.

Figure 6: Preferred Development Concept



The preferred land-use plan, discussed in Chapter 6, provides a more specific look at desired development patterns (see pages 29-32). The sections on preservation, revitalization and renewal and on building and site design describe additional dimensions of this concept (see pages 32-34). Old Boise–Eastside is a unique area that contains a fine-grained pattern of development, many historic resources at a modest scale and few large blocks of undeveloped land. At the same time, it is also the focal point of economic forces that would most likely lead to much higher intensity development—most likely office uses—if the market were given free rein. These forces are intensified by the presence of large institutions immediately adjacent to Old Boise–Eastside and by its proximity to the downtown core.

The approach to development and redevelopment proposed by this plan is restrained given these economic forces. The plan regards an increase in intensity and better land utilization as possible and desirable, because it will assure continued vitality of the Old Boise commercial district and a renewed in-town neighborhood. The plan also emphasizes that the amount of intensity on specific sites needs to be tempered by attention to the surrounding context, especially the existing historic district, as depicted in the Built Form map (see Figure 9).

Achieving the Concept

Achieving the preferred development concept will most likely require market intervention and incentives, especially to achieve the amount of housing envisioned. At the same time, Old Boise–Eastside has the advantage of being viewed as one of the most desirable locations for housing by people who are interested in living in downtown.¹ Development will be leveraged by investments in public facilities, such as improvements to the streetscape and public parking facilities while public-private partnerships are needed to develop housing.

Currently, only a fraction of Old Boise–Eastside’s economic potential has been realized. The challenge for the future is to achieve the right balance between encouraging increased development activity and more investment in improvements on one hand and maintaining the gracious character and ambience of this area on the other. This plan endeavors to set a course that will increase the vitality of Old Boise–Eastside by maintaining a thriving business district and re-establishing a sustainable in-town neighborhood while preserving the most beloved characteristics of this area, thus benefiting the area as well as the larger community.

¹ In spring 2004, CCDC conducted a consumer survey of people living in Boise and in Ada County to measure the level of interest in downtown living. Two of the questions focused on what areas of downtown were considered most desirable as a place to live. When respondents were asked whether they were interested in living in each of six different areas of downtown, they chose Old Boise most frequently as an area in which they were interested, i.e. Old Boise ranked first of the six areas when responses were tallied. When asked to pick their top choice among the six areas, respondents chose Old Boise most often after the Heart of Downtown. It ranked second ahead of the Cultural District and River Street, Parkside and Westside districts.

6: Specific Plans, Guidelines & Action Steps



The Assay Office, built in 1872, speaks to the mining era in Boise's history. Millions of dollars of gold and silver passed through the Assay Office between 1872 and 1933.¹ The Assay Office grounds provide a welcome green space in Old Boise—Eastside. Making better use of neighborhood assets like the Assay Office is part of the action plan for Old Boise—Eastside.

Certain issues are of particular importance in the Old Boise—Eastside area. They are:

- Preferred Land Uses
- Preservation, Revitalization & Renewal
- Building & Site Design
- Streetscapes, Civic Spaces & Pedestrian Ways
- Transportation & Parking

Specific plans, objectives, design and development guidelines and action steps have been developed for each of these issue areas. They help make understanding the overall plan's intent easier. They identify the actions that need to be taken to make the plan come alive. They also provide the basis for the capital investments proposed to implement the plan.

¹ Source: Arthur H. Hart, *Historic Boise*, Historic Idaho, Inc. (1993), p. 26.

1.0 Preferred Land Uses

The preferred land-use plan as shown in Figure 7, depicts a pattern of land uses that support the vision for Old Boise—Eastside and will work to create a vibrant subdistrict and neighborhood within the larger downtown. The land-use plan is consistent with the principle of mixed use shared by all of the downtown plans.

This principle does not necessarily mean a mix of uses within every individual building. It allows for buildings with a mix of uses such as street-level retail and office or housing in the floors above or a mix of single-use buildings with different uses in different buildings. This plan strives for mixed use to enliven downtown streets and to allow people to meet their needs by walking,

2025 Downtown Redevelopment Plan

The 2025 Downtown Boise Redevelopment Plan is a companion document to the master plans for the downtown urban renewal districts. Each of the master plans addresses key elements such as land use, built form, street character and civic spaces for a specific part of downtown. The 2025 Downtown Boise Redevelopment Plan consolidates this information on maps that show these elements on a downtown-wide basis—thus providing a more holistic view of what the eventual character, form and structure of downtown will be. The 2025 Downtown Boise Redevelopment Plan also includes maps showing pedestrian and bicycle routes, transit routes and parking facilities on an area-wide basis.

General design guidelines and standards for building and site design, civic spaces, streetscaping and urban design that apply to all of the approved urban renewal districts in downtown Boise are set forth in the 2025 Downtown Boise Redevelopment Plan. This approach saves duplication in each master plan. These general guidelines and standards supplement the guidelines and standards contained in Chapters 6 of this master plan. CCDC considers both the 2025 Downtown Boise Redevelopment Plan and the master plans for the individual urban renewal districts when making decisions regarding public investment and evaluating development proposals.

Design & Development Guidelines:

- 1.1 Using the Preferred Land Use Map as a guide, reinforce the distinction among the three subdistricts of Old Boise—Eastside: Historic Old Boise as a part of the Central Business District, a residential and office district east of 4th Street and the Capitol Mall district north of Bannock and west of 4th Street.
- 1.2 Promote development of new retail uses in clusters by encouraging this use to occur west of 4th Street and south of Bannock in the existing Old Boise business district.
- 1.3 Encourage buildings with ground floor office and retail uses to include housing units above the first floor.
- 1.4 Using the Preferred Land Use Map as a guide, encourage the development of housing in mixed-use buildings in the Old Boise historic district and the creation of an in-town residential neighborhood in the area east of 4th Street.
- 1.5 Initiate a concentrated public agency effort to promote partnerships with landowners and developers, and use incentives, amenities and catalyst projects to create a critical mass of residential development in Old Boise—Eastside.

bicycling and transit and lessen their dependence on driving. Mixed use helps create more sustainable places and reduces traffic congestion, air pollution and the demand on the transportation system, especially roads.

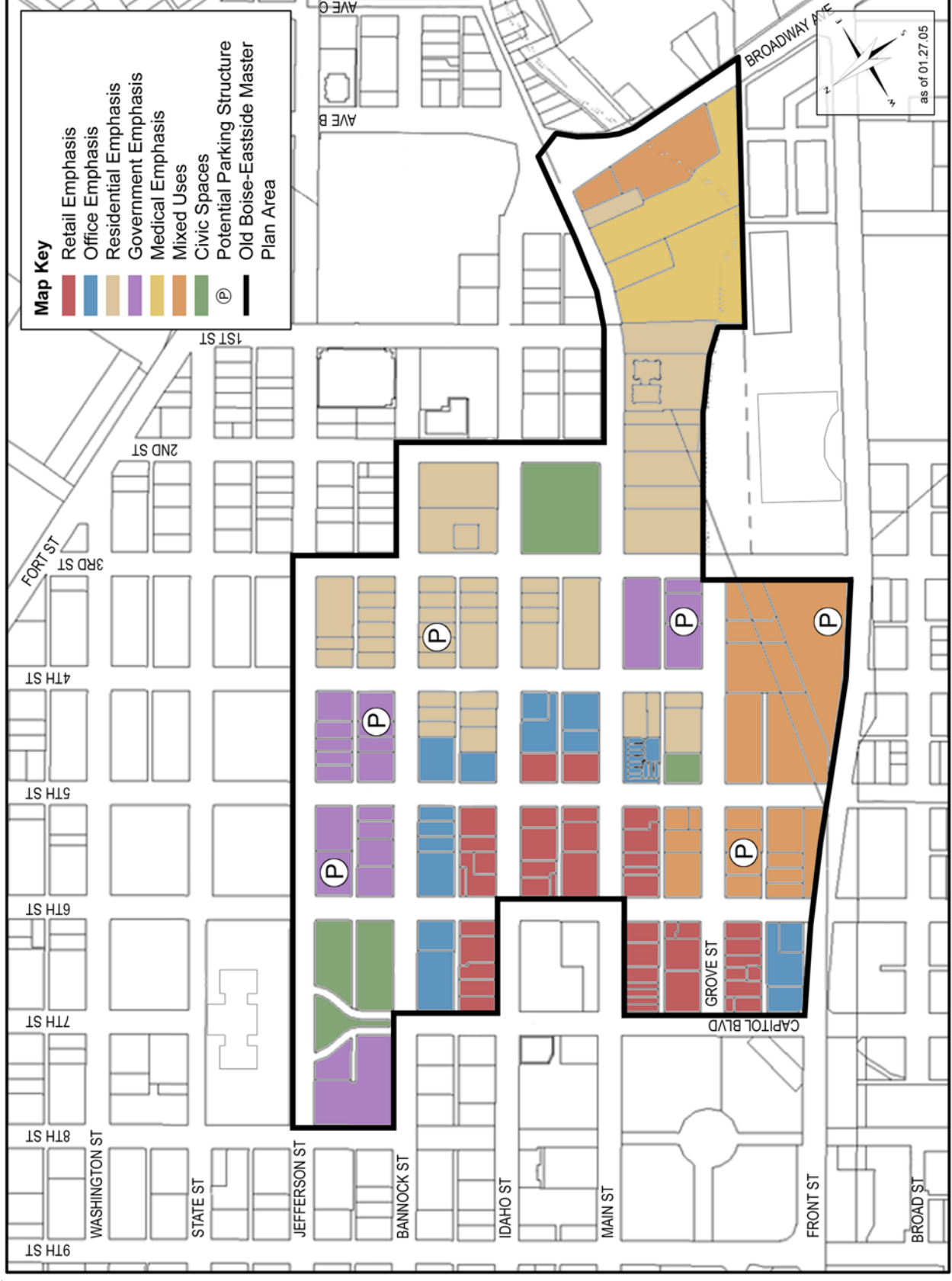
The plan also reflects the desire to focus retail and office activity at the west end of Old Boise—Eastside, housing at the east end and government and institutional uses within the existing State of Idaho's Capitol Mall and St. Luke's Regional Medical Center campus.

The land-use plan reflects and builds upon Old Boise—Eastside's existing strengths: historic character and existing identity; proximity to jobs, services and entertainment; and visual appeal and ambience. The land-use plan recognizes that certain uses, particularly retail and residential uses, will tend to benefit from the synergy that comes from clustering. The plan's objective to increase the amount of housing in Old Boise—Eastside will likely require the most effort. Market forces are eroding the residential base and replacing it with office uses. It is important to stem this tide, on the premise that increasing the supply of housing is the most effective means of maintaining and improving the vitality of Old Boise—Eastside and the downtown core.

Objective:

Achieve a mix of uses in a compact, urban pattern in Old Boise—Eastside that complements its existing character and enhances the long-term prosperity of this area, as well as the larger downtown.

Figure 7: Preferred Land Uses



1.6 Recognize the positive contribution that adjacent institutional uses, such as Idaho state government offices, St. Luke's Regional Medical Center, Boise City Hall and the Ada County Courthouse make to the vitality of Old Boise—Eastside.

1.7 Consider the Idaho State Capitol Mall Plan and St. Luke's Regional Medical Center Master Plan as the guiding policy for the nature and location of new buildings and expansions for these institutions.

Action Steps:

- Use capital investments and public-private partnerships as leverage to establish a neighborhood of moderate- to high-density urban housing east of 4th Street.
- Propose revisions to zoning regulations for the purpose of reaching plan objectives. In particular:
 - Evaluate whether the existing Residential-Office (R-O) zoning designation needs to be revised to better support plan goals for housing in the Old Boise—Eastside area.
 - Propose a revision to the Central Business (C-5) designation that would classify residential as an activating use for purposes of meeting the requirement that 70% of each street frontage must have activating uses on the ground floor.
 - Identify incentives that encourage housing development, which could be incorporated into zoning regulations.

2.0 Preservation, Revitalization & Renewal

Old Boise—Eastside has many positive attributes, making it a desirable place to live, work and play. Throughout this planning process, Old Boise—Eastside has been recognized as an area suitable for complementary infill development and not for wholesale redevelopment. The vision supports retaining Old Boise—Eastside's historic character, its inventory of mature trees and the three public park sites: C.W. Moore Park, Capitol Park and the Assay Office grounds (as identified in Figure 6). Figure 8 identifies historic resources in the area and the location of the Old Boise Historic District. The purpose of this section is to address the relationship between the area's historic character and the strong possibility that new development and redevelopment will occur in Old Boise—Eastside, given its location and desirability. This section also addresses protection of the area's mature tree canopy. Maintenance of Old Boise—Eastside's existing parks is addressed in Section 4.0.

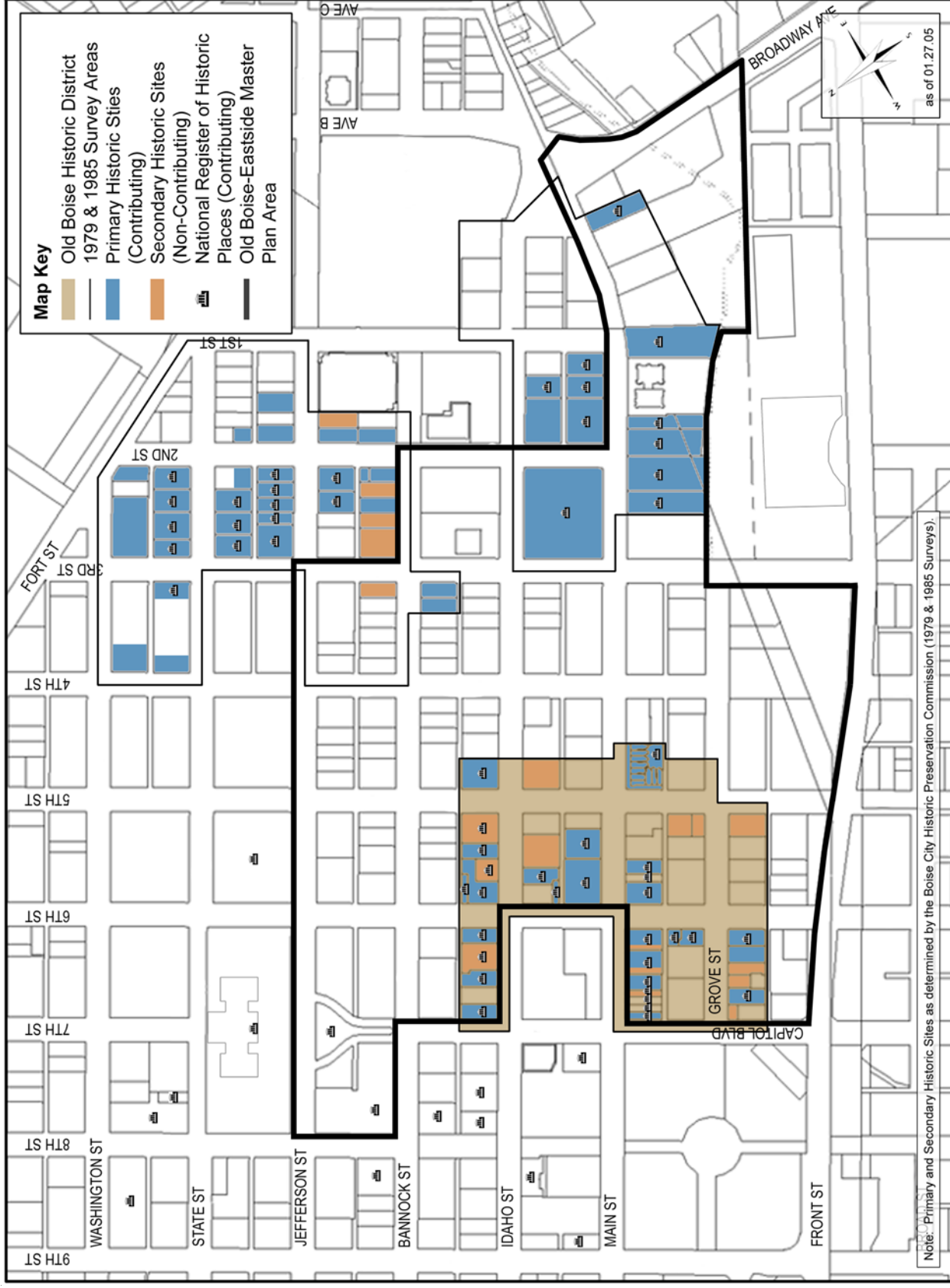
Objective:

Preserve and enhance the historic character and existing desirable attributes within Old Boise—Eastside while allowing for complementary infill development that will add to the area's vitality and long-term prosperity.

Design & Development Guidelines:

- 2.1 Preserve the historic character of Old Boise—Eastside while encouraging development of underutilized property.

Figure 8: Designation of Historic Resources



2.2 Encourage a range of architectural styles in new buildings in a way that does not detract from the historic character of Old Boise–Eastside.

2.3 When considering redevelopment of properties with historic structures, require alternative approaches to be explored that would include reuse of both the structure and the façade as part of redevelopment. Weigh the contribution made by the structure to the historic fabric of Old Boise–Eastside and to the community’s record of its history, against the benefits derived from the development proposal.

2.4 When making decisions regarding preservation, rehabilitation or demolition, take into account the benefits of increasing the supply of housing for Old Boise–Eastside. On properties occupied by historic structures constructed for residential use, give particular attention to restoring them to residential use or replacing them with housing

2.5 Preserve the mature tree canopy by protecting existing trees that are in good condition, interplanting new trees in areas with aging trees, and replacing diseased or dying trees that must be removed.

2.6 When a development proposal shows mature trees to be removed, require an evaluation and a finding that no reasonable alternative exists before allowing removal of trees. Require planting of replacement trees when mature trees are removed.

Action Steps:

- Work with the Boise City Historic Preservation Commission (HPC) to create a method of evaluating redevelopment proposals affecting historic structures. Weigh the assets being gained, preserved and lost, and the degree to which redevelopment will achieve the desired character and preferred development concept stated in this plan.
- Develop and implement a program to preserve the mature tree canopy, including the replacement of trees removed due to poor health or new development.

3.0 Building & Site Design

The 2025 Downtown Boise Redevelopment Plan includes guidelines that apply to building and site design in the downtown urban renewal districts. The purpose of these guidelines is to establish an overall approach to downtown development that is consistent with the urban design principles established in 1984 with the revision of the Central District plan. These principles of compact, mixed-use urban development and pedestrian-oriented design have been reaffirmed in the Downtown Boise Plan (1993), Boise City Comprehensive Plan (1997), the River Street–Myrtle Street Urban Design Plan (1994) and the Westside Downtown Framework Master Plan (2001).

This section provides urban design guidelines regarding building and site design, particular to Old Boise–Eastside, that enhance livability and economic vitality. Figure 9 shows the pattern of building heights recommended, by this plan, for Old Boise–Eastside.

The guidelines focus on three areas:

- Recognizing the existing differences in character and intensity between the business district on the west end and the residential area on the east end of the Old Boise–Eastside planning area when designing new development and redevelopment.
- Striking a balance between the preserving the area’s historic character, encouraging a prosperous business district and developing a desirable residential neighborhood within Old Boise–Eastside.
- Maintaining Old Boise–Eastside as an attractive place for people through building design that enlivens the street, pedestrian-oriented design and parks and plazas that provide gathering places for people’s enjoyment.

Objective:

Maintain and strengthen the urban character of Old Boise–Eastside with the density of development projects and by placement and design of buildings. Respect the existing historic and architecturally detailed nature of the area by careful development of structures and sites, while allowing for innovation in architecture and building form.

Design & Development Guidelines:

- 3.1 Use the Built Form map (Figure 9) as a general guide to appropriate building heights. Give attention to the scale and massing of adjacent buildings in designing for a particular site.
- 3.2 Refer to the 2025 Downtown Boise Redevelopment Plan for general principles that apply to building and site design for the urban renewal districts in downtown Boise, which in turn apply to Old Boise–Eastside.
- 3.3 Differentiate between the Retail and Office Emphasis area and the Residential Emphasis area in the preferred development concept (Figure 6), through the building placement and site design allowed in each area. (See 3.5 and 3.6 below.)
- 3.4 Create pedestrian orientation through design and location of buildings, streetscape, parking and public spaces throughout Old Boise–Eastside to maintain the refined, people-oriented character of the area.
- 3.5 Require new buildings in the Retail and Office Emphasis area to be placed at or near the sidewalk to create a strong street wall consistent with historic Old Boise.

3.6 Require the design of new development in the Residential Emphasis area to have an urban density and character that contributes to creating an attractive in-town residential neighborhood. Allow new buildings to use innovative architecture as long as it is compatible with the refined nature of Old Boise—Eastside and respects adjacent historic buildings. Give particular care to the design of new buildings and additions to buildings located within the Old Boise Historic District.

3.7 Encourage green space in development projects in the Eastside area to assure it will be a desirable neighborhood.

Action Step:

- Evaluate the Residential-Office (R-O) zoning district to determine if revisions are needed to create consistency between the desired densities and building heights stated in this plan and the city's zoning regulations.

4.0 Streetscapes, Civic Spaces & Pedestrian Ways

Many of the streets in Old Boise—Eastside are already graced by large, mature trees and wide tree lawns that create its distinctive character and attractiveness as a place for living and working. In addition to its trees, Old Boise—Eastside also contains three public parks: C.W. Moore Park, the Assay Office grounds and Capitol Park. These public spaces provide places for people to gather,

socialize and enjoy a respite from the surrounding urban activity and, therefore, are key to the vitality of Old Boise—Eastside. The parks are especially important to encouraging the development of an in-town neighborhood in the eastern half of the area. In a large part, the mix of amenities between the roadway and the building—referred to as the streetscape—establishes the character of downtown and its districts.

One purpose of the urban renewal district is to provide local government the capability of generating revenues that can be used toward the improvement and maintenance of public parks and streetscapes. The guidelines and actions in this section are intended to guide how these public infrastructures are improved and maintained in Old Boise—Eastside. Figure 10 identifies the streetscape character anticipated in Old Boise—Eastside. Figure 11 shows the civic spaces and pedestrian ways in the area.

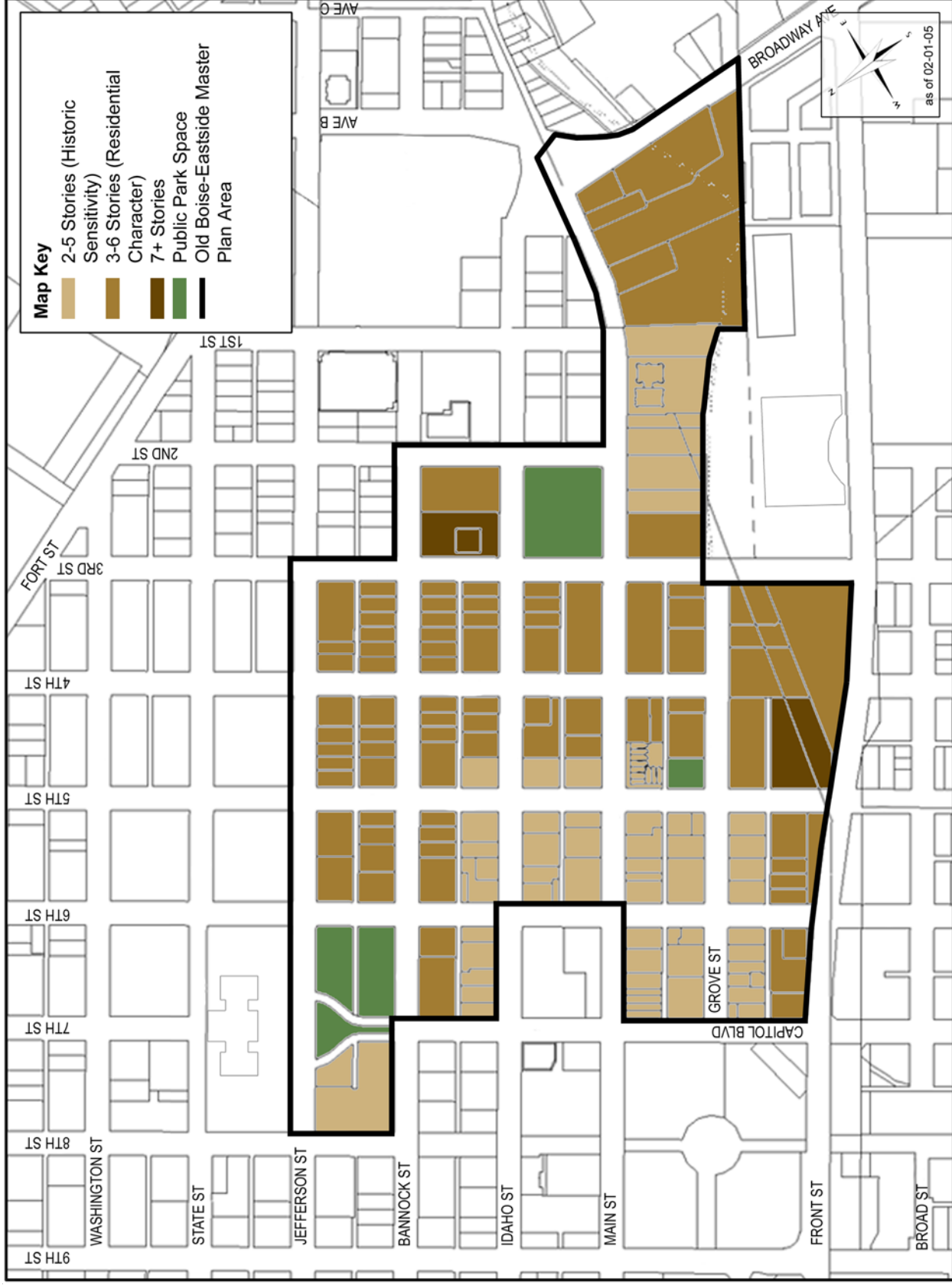
Objective:

Create a memorable and dignified civic framework of streetscapes, civic spaces and pedestrian ways that complements the activity and design of adjacent uses as well as links Old Boise—Eastside to its neighboring subdistricts.

Design & Development Guidelines:

- 4.1 Create a hierarchy of streets that serve the needs of the Old Boise—Eastside area, not only by traffic capacity, but also by design treatment of sidewalks, crosswalks, streetscapes, lighting and furnishings.

Figure 9: Built Form



4.2 Re-establish the traditional street grid between Grove and Front streets by reconstructing 4th Street. If a street connection is not possible, establish a pedestrian connection as an alternative.

4.3 Differentiate between the Retail and Office Emphasis area and the Residential Emphasis area (see Figure 6) through the type of streetscapes used. In general utilize hardscape from building to curb in the business district and use tree lawns in the residential area. Refer to the Streetscape Character map (Figure 10) and the streetscape standards in Appendix B of the 2025 Downtown Boise Redevelopment Plan.

4.4 Use streetscapes to create pedestrian-friendly streets with continuous tree coverage, especially in the east end of the district.

4.5 Create a network of pedestrian routes that connect activity centers such as the CBD, Capitol Park, State Capitol Mall, City Hall, Assay Office, St. Luke's Regional Medical Center, C.W. Moore Park, Ada County Courthouse, Basque Block and Julia Davis Park, and mark this network by installing special streetscape improvements on the sidewalks along these routes.

4.6 Create and improve public spaces through public improvement projects and as part of private developments.

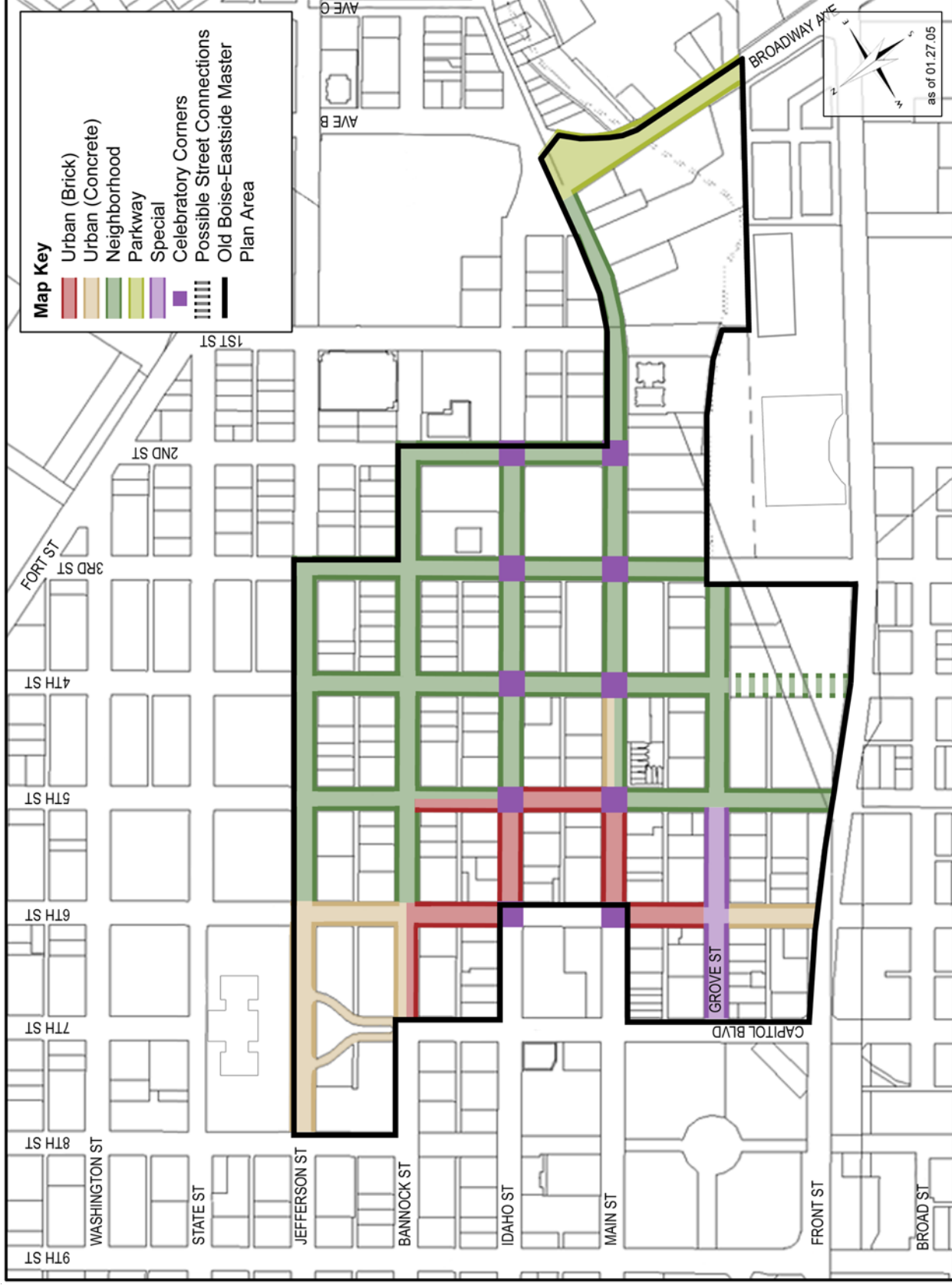
4.7 Encourage the inclusion of public spaces in the business district, which serve as outdoor dining areas as well as gathering and socializing spaces, to increase the vitality of the area.

4.8 Preserve existing parks and other public spaces in Old Boise–Eastside, including C.W. Moore Park, the Assay Office grounds and Capitol Park.

Action Steps:

- Utilize the Streetscape Character map (Figure 10) to determine what streetscaping standards to use in improving Old Boise–Eastside streets. (The specific standards for each type of streetscape are contained in Appendix B of the 2025 Downtown Boise Redevelopment Plan.) Implement these standards at the time properties are developed or redeveloped and as part of the capital improvement program for the River Myrtle–Old Boise Urban Renewal District.
- Initiate a program to install streetscapes in the Old Boise business district, so it becomes a seamless part of the CBD, as funding becomes available.
- Provide distinctive sidewalk treatment and a wayfinding system that demarcates the pedestrian network to encourage walking, exploring and discovering the special places in this area.
- Extend special design treatment applied in the Central and Westside districts, on Main and Idaho streets, east to 4th Street in Old Boise–Eastside. Apply or maintain a neighborhood street treatment largely consistent with the current character generally east of 5th Street.
- Improve pedestrian access within Old Boise–Eastside that links St. Luke's Regional Medical

Figure 10: Streetscape Character



Levels of Service (LOS)

Level of Service or LOS is a term used by traffic engineers to qualitatively describe operating conditions for automobile travel on existing or planned roadways. Typical traffic characteristics described by LOS include: speed, travel time, maneuverability, traffic interruptions, driver comfort and driver convenience. Similar to a report card, LOS grades roadway conditions from "A" to "F" with "A" representing the best driving conditions and "F" the worst. LOS E represents the capacity threshold of a given roadway. The LOS grades for roadway facilities are generally defined as follows:

- LOS A represents free-flow travel with an excellent level of comfort and convenience and the freedom to maneuver.
- LOS B has stable operating conditions, but the presence of other road users causes a noticeable, though slight, reduction in comfort, convenience and maneuvering freedom.
- LOS C has stable operating conditions, but the operation of individual users is substantially affected by the interaction with others in the traffic stream.
- LOS D represents high-density, but stable flow. Users experience severe restriction in speed and freedom to maneuver, with poor levels of comfort and convenience.
- LOS E represents operating conditions at or near capacity. Speeds are reduced to a low but relatively uniform value. Freedom to maneuver is difficult with users experiencing frustration and poor comfort and convenience. Unstable operation is frequent, and minor disturbances in traffic flow can cause breakdown conditions.
- LOS F is used to describe forced or breakdown conditions. This condition exists whenever the volume of traffic exceeds the capacity of the roadway. Long queues can form behind these bottleneck points with queued traffic travelling in a stop-and-go fashion. Signalized intersections do not clear in one signal cycle.

Center, Ada County Courthouse Corridor and Julia Davis Park.

- Work with property owners to re-establish 4th Street between Grove and Front Streets, or create a safe and attractive pedestrian connection along this alignment, so Old Boise–Eastside will better connect to the River Street–Myrtle Street portion of the urban renewal district.

- Improve the pedestrian environment on 3rd Street between Grove and Idaho streets in order to provide a more pedestrian-friendly connection from the downtown core through Old Boise–Eastside to the Ada County Courthouse Corridor.

- Establish a safe and attractive pedestrian connection at Avenue A between St. Luke's Regional Medical Center and the Ada County Courthouse Corridor.

- Coordinate with state and local governments to evaluate existing parks and make improvements that provide appropriate amenities to downtown residents and workers as well as to the visitors of downtown Boise.

- Coordinate with the Idaho state government to improve the Assay Office property as a cultural attraction and as an urban park for local use.

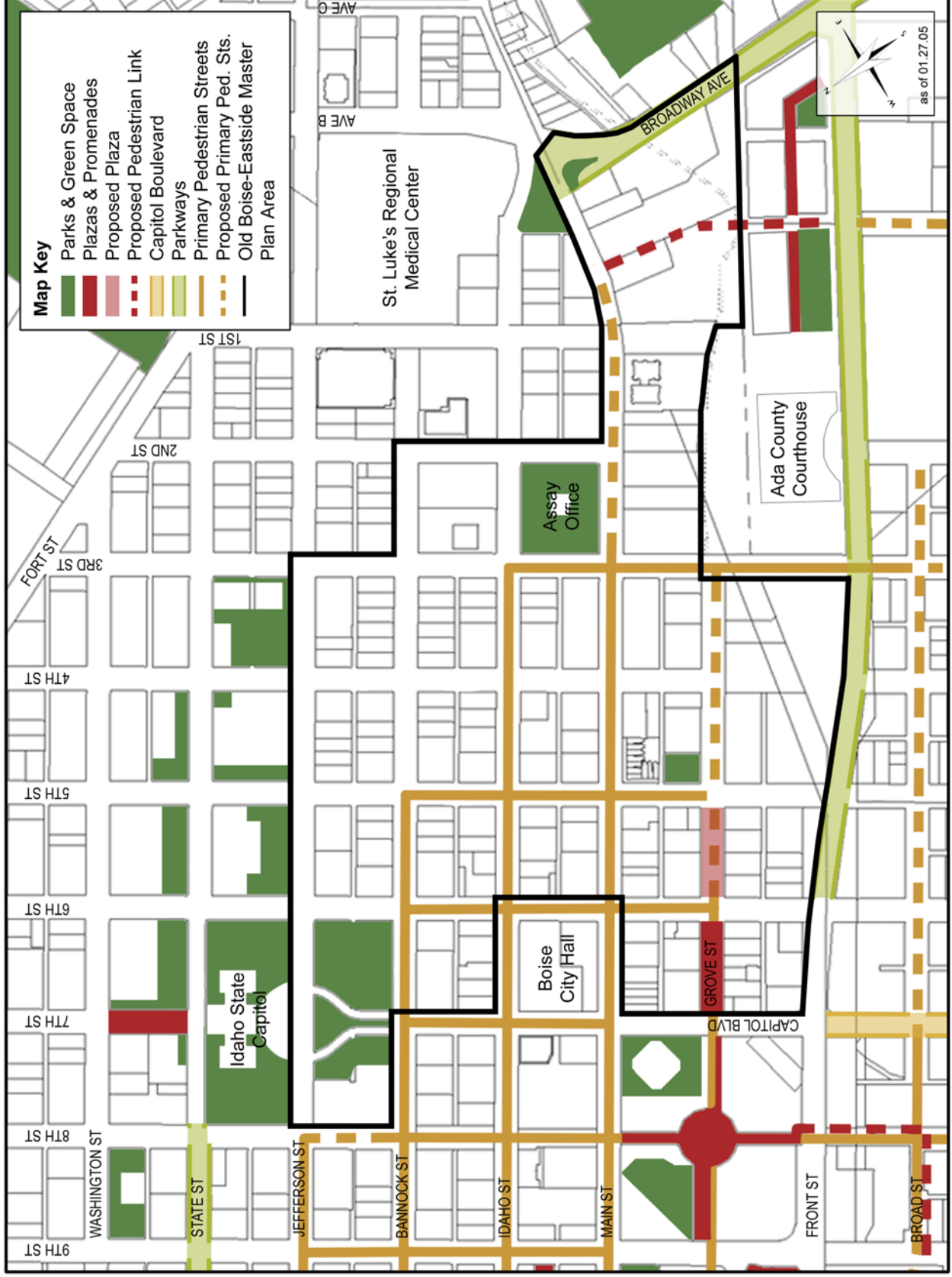
- In partnership with Boise City, make improvements to Capitol Park that are compatible to its location and function within the Idaho State Capitol Mall.

- In partnership with Boise City, support enhancement of C.W. Moore Park to improve its function as a neighborhood park. Add signs explaining the historic significance of the features in the park.

5.0 Transportation & Parking

Old Boise–Eastside is blessed with a grid street system that handles vehicle traffic easily with little congestion, and its attractive tree-lined streets encourage people to travel at slower speeds. However, new development anticipated between now and 2025 will add traffic in the area. The Downtown Boise Mobility Study (DBMS) modeled future traffic using the 2025 growth scenario. It proposes a policy for that a level of service (LOS) of D is acceptable during non-peak hours and an LOS of E is acceptable during peak hours on downtown streets (see sidebar). This policy recognizes that downtown is more congested than is typical in other areas of Boise, and a certain level of congestion is an acceptable tradeoff in order to maintain downtown Boise as the region's urban center. The traffic analysis showed that certain intersections in the Old Boise–Eastside area will have a level of service (LOS) of F in 2025 during peak hours. These include Capitol, 6th and 3rd streets at Front Street, Capitol and Jefferson, 6th and Main streets, and the intersection of Broadway Avenue with Main and Idaho streets. The other intersections in Old Boise–Eastside, which were included in the modeling, are operating at a LOS of C or better during peak hours except for Main and Capitol and Idaho and 6th streets, which are operating at a LOS of D.

Figure 11: Civic Spaces



Most of the congestion predicted by the traffic model is associated with commuter traffic between the region and downtown since downtown is expected to continue growing as an employment center.

One of the goals of the DBMS is to develop a multimodal transportation system that encourages use of transit, bicycling and walking as alternatives to the use of personal vehicles. The DBMS advocates the philosophy that if people bring their cars to downtown, transportation improvements are needed so individuals can park once and travel in downtown using other modes. The DBMS proposes a downtown circulator that will connect the various subdistricts and activity centers in downtown so people can live, work, shop, socialize and play without having to use their cars. A portion of the proposed downtown circulator route goes through Old Boise–Eastside. The DBMS also advocates improving the regional transportation system so commuters can travel to downtown without bringing their cars at all.

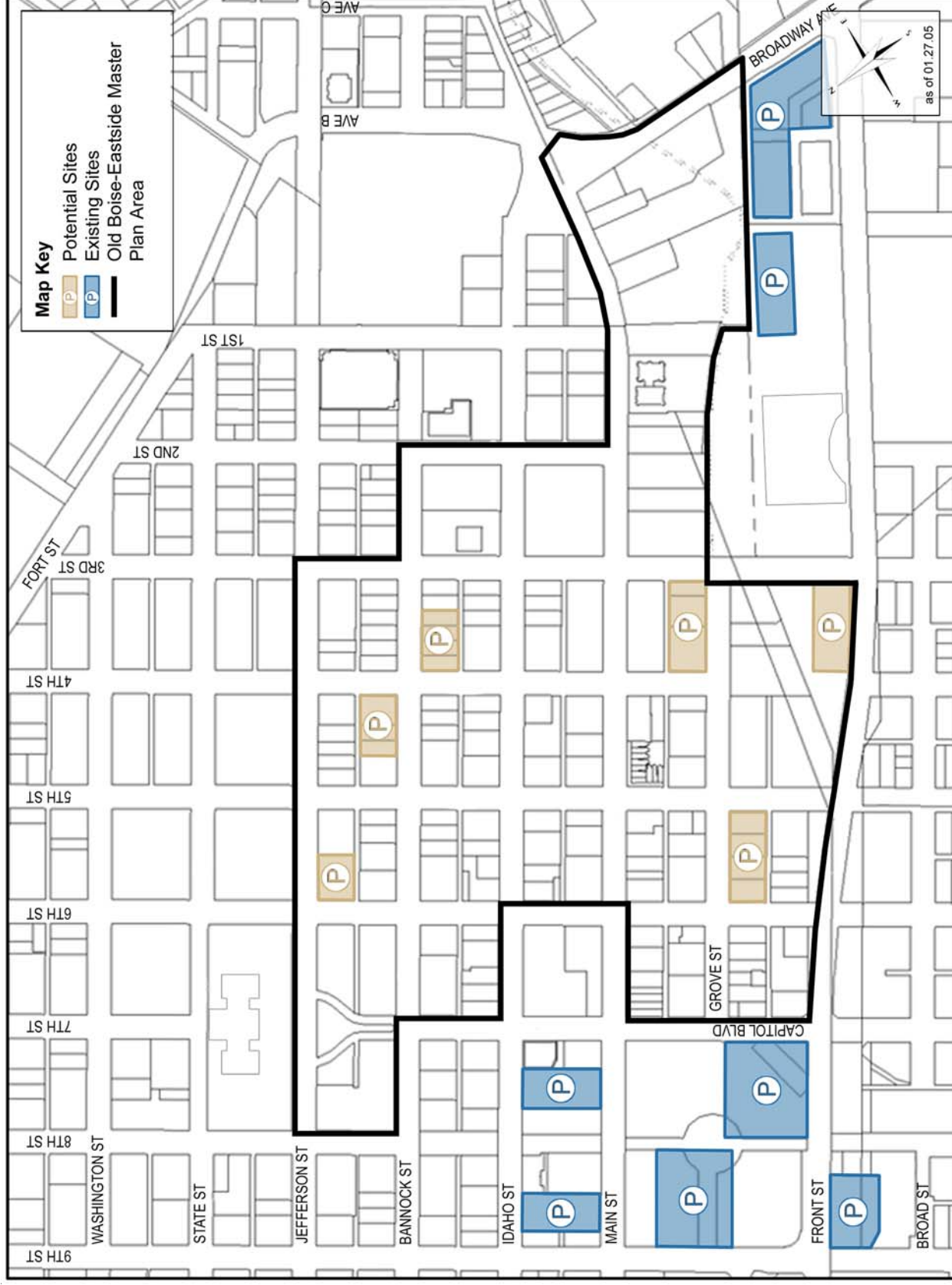
The DBMS recognizes that for the foreseeable future, most people will use individual vehicles for their transportation needs. This situation creates a demand for parking in downtown. Stakeholders in Old Boise–Eastside have emphasized that there is a parking problem, especially in the west end of the area, where there is a concentration of retail shops, restaurants and bars. Most of the parking in Old Boise–Eastside is supplied by on-street parking or in privately owned surface parking lots. This situation puts Old Boise–Eastside at a disadvantage relative to the downtown core where there is ample public parking in garages. The lack of parking also restricts the future business growth.

Creating a better parking situation in the Old Boise business district is one of the key objectives of this plan.

In general, when parking is provided in surface lots, it tends to sap vitality from the area. Numerous large-scale surface parking lots in an area tend to discourage pedestrian traffic because they create a much less stimulating environment for walking than buildings. A key objective of this plan is to locate parking in parking garages that are designed so they do not interrupt active uses at the street level. Another objective is to encourage existing parking lots to be made available to the public rather than reserved for specific businesses. This will extend the currently limited parking resources. Figure 12 identifies potential locations for public parking garages. It should be noted that these locations would require further study, and other locations may prove to be suitable as well. The fact that multiple parking garage sites are shown on the map does not mean that all of these sites will be used.

Objective:

Establish a multimodal system of transportation to serve the Old Boise–Eastside area that encourages the use of transit, bicycling and walking as alternatives to automobiles. Recognize that automobiles will continue to be the primary method of travel and create a system of public parking that supports desired land uses while minimizing the use of surface parking lots. Stretch parking resources by encouraging mixed use, by working with owners of private parking lots to allow public use of these lots during off-hours, and joint use of parking lots by users with offsetting parking demands.



Design & Development Guidelines:

- | | |
|---|--|
| 5.1 Increase the supply of public parking available in the Old Boise–Eastside area, especially in close proximity to the Old Boise business district. | ■ Construct at least one public parking garage within or in close proximity to the Old Boise business district to help alleviate parking needs. |
| 5.2 Provide public parking facilities that are convenient for the public and located strategically to support desired land uses. | ■ Explore with the city whether the parking garage at City Hall could be used in the evening and on weekends to provide parking for the Old Boise business district. |
| 5.3 Discourage the development of surface parking lots and encourage fuller utilization of land currently occupied by surface parking. | ■ Encourage owners of existing private parking lots to make these lots available in the evening and on weekends for public use. |
| 5.4 Design parking structures so they do not interrupt active uses at the street level. | ■ Pursue funding for and implement the downtown circulator. Design the route so it provides service to Old Boise–Eastside when funding becomes available. |
| 5.5 Establish strong pedestrian and bicycle connections between activity centers in Old Boise–Eastside and with the rest of downtown in order to encourage walking and bicycling over automobile use. | ■ Improve frequency of transit service to the Old Boise–Eastside area. |
| 5.6 Establish a downtown circulator that provides a transit alternative for people travelling in downtown, and provide service to the Old Boise–Eastside area. | ■ See also the Action Steps regarding pedestrian and bicycle connections under Section 4.0. |

Action Steps:

- Expand the P-1 parking district to include the Old Boise business district.

7: Implementation



C.W. Moore Park is one of two parks in the Old Boise–Eastside Planning Area. It provides a delightful place for a summer stroll or eating lunch on a busy work day. Improvements to this park are one of the projects identified in the long-term investment plan for Old Boise–Eastside.

However good a plan may be, its benefits will not be realized until it has been implemented. Implementation occurs through many avenues including private and public investments, grant programs, the use of regulatory tools such as zoning and design review, construction of catalyst projects and other actions. Boise has experienced successful redevelopment in the Central District, where investments of public funds have precipitated a five-fold investment of private funds. Construction of The Grove plaza, reconstruction of downtown streets and installation of brick sidewalks, street trees and planters, turning 8th Street into a pedestrian-oriented street where restaurants and shops

thrive, creating the transit mall and building public parking garages have brought about a transformation of Boise's downtown core. Entering into partnerships with private developers and using land write-downs has resulted in renovation and reuse of historic buildings and new development.

This success has continued in the River Street–Myrtle Street District with development of the River Plaza Apartments and office building, and the construction of the Ada County Courthouse, Civic Plaza apartments and parking garages in the Courthouse Corridor.

Resources for Public Investment

The Old Boise–Eastside Master Plan sets forth objectives, development guidelines and action steps that describe how this area should be developed over the next 20 years. The focus of the plan is to enhance the vitality of the existing commercial district in the west end of the study area, while reinforcing a mixed-use urban neighborhood on the east end. As noted earlier, the development concept for Old Boise–Eastside does not envision a dramatic change in character of the physical environment, other than a modest increase in development intensity and a better balance among land uses. At present, housing is being lost in the Old Boise–Eastside study area through the conversion or replacement of residential structures by office uses. Adding housing is seen as vitally important because it will increase the number of people living in Old Boise–Eastside, which in turn promotes retail vitality, allows people to both work and live in downtown, reduces traffic congestion and increases safety. These results can be achieved through medium-scale infill development and redevelopment.

The plan will help guide public and private actions as development proceeds so that they complement each other and yield greater results than what otherwise would occur if no plan were in place. These results will not be achieved, however, without commitment, dedicated effort and investment from both the public and private sectors. This section of the plan describes what resources are available and what steps must be taken in order to implement the Old Boise–Eastside Master Plan.

Tax increment financing is the primary means for funding public improvements in an urban renewal area. Idaho State Code allows for the establishment of urban renewal districts and the creation of revenue allocation (TIF) areas to provide a source of funding for urban renewal activities. When a revenue allocation area is created, a portion of the property taxes generated from increases in assessed valuation are allocated to the urban renewal agency. These funds must be used in the urban renewal district for activities that are intended to revitalize and redevelop the area.

Other sources of funding for redevelopment activities include:

- Revenues from public parking and transit systems operated by the urban renewal agency
- Revenue bonds
- Improvement districts
- Federal and state funding programs for transportation and transit projects, housing, community development, business development and other funding programs
- Federal tax credit programs
- Grants and donations

Old Boise–Eastside Financial Plan

The initial financial analysis for the Old Boise–Eastside study area showed that it might not generate sufficient tax increment revenue to pay for anticipated improvements. As a result, the CCDC Board of Commissioners evaluated combining the Old Boise–Eastside area with the existing River Street–Myrtle Street Urban Renewal District as part of this master planning process. The advantages of having a larger land area in an urban renewal district are discussed in Chapter 1.

At the conclusion of the master planning process for both the River Street–Myrtle Street planning area and the Old Boise–Eastside planning area, Keyser Marston Associates prepared a financial analysis for a combined River Myrtle–Old Boise Urban Renewal District and for Old Boise–Eastside as a stand-alone district. In both cases, the financial analysis projects that sufficient resources from tax increment revenue and other sources will be available to pay for the proposed improvements. In the intervening time between the preliminary and final analysis for Old Boise–Eastside, the assumptions regarding the value of future development and the cost and extent of improvements were refined for this area and led to a positive result. (The financial analyses for the combined River Myrtle–Old Boise Urban Renewal District and Old Boise–Eastside are available from CCDC.)

As noted in Chapter 1, during the public review of this plan, property owners in the existing River Street–Myrtle Street district expressed concerns that adding Old Boise–Eastside would drain resources from the River Street–Myrtle Street district and leave some areas of this district

without resources. To address this concern, this plan includes CCDC’s commitment to work with a stakeholder committee to prepare a capital investment plans for each successive five years remaining in the life of the district. The stakeholder committee would include property owners, business owners and residents in the district. This committee would make recommendations to the CCDC Board of Commissioners on the type and timing of public investments in the various subdistricts of the River Myrtle–Old Boise Urban Renewal District. These subdistricts are shown in Figure 2.

The CCDC Board of Commissioners and the Boise City Council elected to proceed with a combined River Myrtle–Old Boise Urban Renewal District because of the advantages of larger district described earlier.

The financial analysis for the combined River Myrtle–Old Boise Urban Renewal District includes the following:

- Assumptions about the amount, type and timing of development that would occur during the planning period. Development assumptions are used to estimate the amount of property tax revenue that would flow to CCDC from the revenue allocation area.
- Assumptions about the number and size of parking garages that would be built and estimates of revenue that would be generated.
- Estimates of other likely sources of funding.
- A list of potential projects CCDC would undertake to revitalize both Old Boise–Eastside and River Street–Myrtle Street planning areas during the next 20 years. These projects are intended to be catalysts that will trigger private investment in the district.

The table below lists the revitalization projects and estimated costs expected in Old Boise—Eastside during the 20-year planning horizon. It provides a description of the anticipated public investments in the Old Boise—Eastside area. The complete financial plan for the combined River Myrtle—Old Boise District is available from CCDC.

Revitalization Projects & Costs

Parking Garages (1)	\$ 3,968,750
Transit Facilities/Downtown Circulator	\$ 5,330,000
\$1,066,000 local	
\$4,254,000 federal	
Bus Shelters	\$ 110,000
Streetscaping/Sidewalks/Curb & Gutter	\$ 3,132,950
Sewer/Storm Drainage	\$ 50,000
Water Quality Treatment	\$ 10,000
Utility Undergrounding	\$ 400,000
Telecommunications Facilities	\$ 50,000
Geothermal Capacity Increase	\$ 67,000
Traffic Signals	\$ 300,000
Road Re-Surfacing	\$ 100,000
Special Purpose Park	\$ 50,000
Other Park/Open Space	\$ 10,000
Housing	\$ 50,000
Façade Easements	\$ 18,000
Façade Improvements	\$ 20,000
Property Acquisition	\$ 1,755,000
Improve Vehicular/Pedestrian Access	\$ 50,000
Special Projects	\$ 100,000
Cultural Investments	\$ 35,000
Total	\$15,606,700

It should be noted that the financial plan does not guarantee that all of these improvements will be made. Over time, the amount of tax increment produced will depend on the development activity in the River Myrtle—Old Boise Urban Renewal District. The flow of revenues may be higher or lower than projected in the financial plan, which would lead to adjustments in the improvements undertaken. The amount of funding allocated to each type of improvement may be adjusted depending on opportunities that arise or needs that develop. The financial plan is an approximate picture of the types and amount of investment that will occur given what is now known, based on assumptions about the amount of development activity and its value, tax rates, financing, construction and operating costs and other factors.

Old Boise—Eastside Master Plan as part of Boise City Comprehensive Plan

City decision makers use the Boise City Comprehensive Plan to evaluate development applications. The Boise City Zoning Ordinance states that approval for zoning revisions, land subdivisions, conditional uses and design review requires a finding that the proposal under consideration be consistent with the comprehensive plan. Existing urban renewal plans for the Central, River Street—Myrtle Street and Westside Downtown districts have been adopted, by reference, as part of the comprehensive plan, and thus are used in determining consistency when a development proposal is located in one of those districts. Initiating an amendment to the compre-

hensive plan to include the Old Boise–Eastside Master Plan is one of the important next steps in implementing this plan, so it will also be used in making consistency findings.

Zoning Revisions

Zoning is one of the key tools for implementing any long-range plan for revitalization and development. The Old Boise–Eastside Master Plan establishes a vision and desired direction for this area, expressed through framework plans, objectives, design and development guidelines and action steps. The Boise City Zoning Ordinance actually regulates how property owners can use their property. Plan implementation is more effective when there is consistency between the urban renewal plans and the zoning regulations.

Current zoning designations for Old Boise–Eastside are included on the Zoning Map in Appendix A. Most of the properties in Old Boise–Eastside are in either the C-5D, C-5HD or R-OD zoning districts. The C-5 and R-O districts and the DD and HD overlay districts are discussed below, along with recommendations regarding possible revisions to create greater consistency between the Old Boise–Eastside Master Plan and the zoning in this area.

C-5 and R-O Zoning Districts

The Central Business District (C-5) district encourages urban density, pedestrian-oriented development while the Residential-Office (R-O) district is oriented toward a mix of lower intensity residential and office development. (The D in the district designations indicates that the city’s design review committee reviews development projects proposed in these areas. The HD indicates that an area coincides with an adopted historic district and the city requires design review by the Boise City Historic Preservation Commission when development projects are proposed.)

For the most part, 4th Street serves as the dividing line between the C-5 district and the R-O district in Old Boise/Eastside. C-5 coincides with the Old Boise commercial district and R-O with the area where there are lower-scale office and apartment buildings, the Imperial Plaza condominiums and numerous houses—many of which have been converted to offices. The R-O portion of Old Boise–Eastside feels more like a neighborhood with its wide tree lawns, mature trees and shaded streets. One of the issues identified during the planning process was the effect of the R-O district on the housing situation in Old Boise–Eastside. This zoning district treats residential and office uses as equal, but in reality market pressures in Old Boise–Eastside encourage converting houses to offices and replacing residential structures with office buildings. The Old Boise–

Eastside Master Plan has an objective to retain and increase the supply of housing in this area. Evaluating and possibly revising the zoning designations in Old Boise–Eastside is an important step in implementing this objective. The plan includes the following development guideline and action step related to this issue (see 1.0 Preferred Land Uses, page 32):

Design & Development Guideline:

- 1.5 Initiate a concentrated public-agency effort to promote partnerships with landowners and developers, and use incentives, amenities and catalyst projects to create a critical mass of residential development in Old Boise–Eastside.

Action Step:

Propose revisions to zoning regulations for the purpose of reaching plan objectives. In particular:

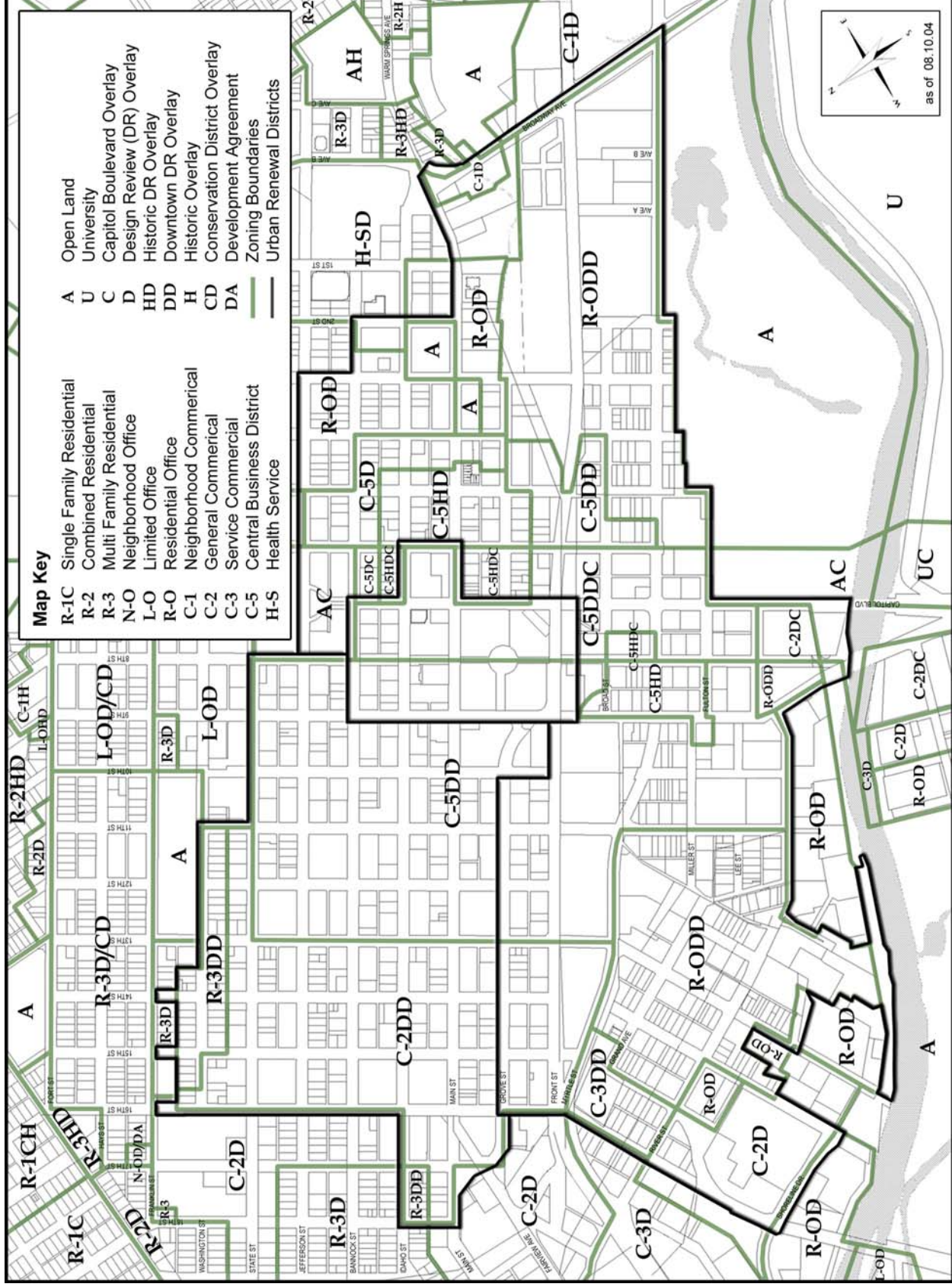
- Evaluate whether the existing Residential-Office zoning district needs to be revised to better support plan goals for housing in the Old Boise–Eastside area.

DD and HD Overlay Districts

The Boise City Zoning Ordinance includes a “down-town design review” or DD designation that applies to properties located in an urban renewal district. This designation requires that development applications be referred to CCDC for comment prior to the city taking action. CCDC provides comments on the degree to which the development proposal implements the plan for that district. The Planning & Zoning Commission, Design Review Committee and City Council take such comments into consideration when making decisions on development proposals.

The zoning ordinance also includes a “historic design review” or HD designation that applies to properties in city-adopted historic districts. In some cases, properties are included in both a historic district and an urban renewal district. Where such overlaps occur, the properties are shown on the zoning map with an HD designation rather than both an HD and DD designation. The regulations governing the HD designation do not include a requirement that development applications for properties that are in both a historic district and an urban renewal district be referred to CCDC for comment. In actuality, Boise City staff refers such cases to CCDC for comment in order to maintain a consistent process for all properties in urban renewal districts. Revising the zoning ordinance to include referral of development applications to CCDC when properties with an HD designation are also in an urban renewal district would recognize this practice.

Appendix A: Zoning Map



Appendix B: Supplemental Changes

OLD BOISE–EASTSIDE MASTER PLAN SUPPLEMENTAL CHANGES MADE TO THE AUGUST 23, 2004 DRAFT ADOPTED BY THE CCDC BOARD OF COMMISSIONERS AND TRANSMITTED TO THE BOISE CITY COUNCIL IN THE OCTOBER 12, 2004 FINAL DRAFT.

This list contains changes that were made to the August 23, 2004 Draft of the Old Boise–Eastside Master Plan adopted by the CCDC Board of Commissioners, which were accepted by the Boise City Council as part of its action to approve the Old Boise–Eastside Master Plan on November 30, 2004, except as otherwise noted. Text that was added is underlined; text that was removed is shown by strikeouts. The reason for each change is noted in parentheses and italics at the end of each item. The Final Draft for City Council–October 12, 2004 incorporated these changes. A list identifying these changes was entered into the record at the October 12, 2004 public hearing on the River Myrtle–Old Boise Urban Renewal Plan.

In the August 23, 2004 Draft, there were comments highlighted in blue that explained where text might need to be modified, added or deleted based on Boise City Council action or when information was pending. Where these comments were resolved prior to October 12, 2004, they were removed without a note being included in the list below.

A limited number of very minor changes to the August 23, 2004 Draft were not included in the list provided to the Boise City Council. Examples include adding a horizontal line at the top of a page consistent with the page format used throughout the plan, bolding subheadings, removing background street names on maps to reduce clutter, and capitalizing words that are consistently capitalized elsewhere but got missed in a particular instance. (The Boise City Council was made aware of this caveat at the time the list was submitted.)

Please Note: After City Council action on November 30, 2004 to approve the Old Boise–Eastside Master Plan, minor changes were made to the plan document to prepare it for publication. These changes do not affect the substance of the document. They are listed at the end of this appendix.

Credits

1. Corrected Zimmer-Gunsul-Frasca to Zimmer Gunsul Frasca Partnership (*remove hyphens; use correct name*)

List of Figures (p. ii)

2. 7: Preferred Land Uses (*match title of figure to title of section in which it appears; see page 29 in text of plan*)

Chapter 1: Introduction

3. p. 3, Figure 1: add Americana Blvd to map (*helpful reference for reader*)
4. p. 7, Figure 2: add Americana Blvd to map (*helpful reference for reader*)

Chapter 2: Eligibility for Urban Renewal Activities

5. p. 9, 2nd column and p. 10, 1st column; bulleted list: End each bullet item with a period; remove commas and semicolons. *(make format of bulleted lists consistent)*

Chapter 3: Current Conditions

6. p. 13, 2nd column, 2nd paragraph: ...designates almost the entire Old Boise/Eastside study area as ~~Mixed Use~~ Mixed Use. East of 3rd Street is designated ~~Institutional~~ Institutional *(remove italics)*
7. p. 14, 1st column, 1st paragraph: ...integrated subdistricts each maturing under ~~"New Urbanism"~~ "new urbanism" principles as applied by specific design overlays. *(remove quotes and capitals)*
8. p. 14, 1st column, 3rd paragraph: The original River Street-Myrtle Street District did not include Old Boise/Eastside but the River-Myrtle Urban Design Plan identifies the ~~Old Boise Expansion Area~~ Old Boise Expansion Area as a subdistrict... *(remove italics)*
9. p. 14, 1st and 2nd column, bulleted list: End each bullet item with period; remove colons and semicolons. *(make consistent with other bulleted lists)*
10. p. 14, 2nd column, last paragraph: During the preparation of the Old Boise/Eastside Master Plan, it became obvious that describing a vision and writing goals and objectives, and doing mapping for each individual district often causes duplication. It also tends to obscure the message that there is one unified vision for the long-term revitalization of downtown Boise. . the vision and many of the design and development guidelines for downtown redevelopment are common to all of the urban renewal districts. *(better wording; no change in meaning)*
11. p. 16, 1st column, 2nd paragraph: The 2025 Downtown Boise Redevelopment Plan was created to illustrate the seamless nature of downtown and to describe both the overall vision and goals for its revitalization and the design and development ~~objectives and~~ guidelines shared by all the districts. It depicts the common elements on maps that show all of the districts and the periphery around them so these elements are put into context. Plans, ~~development objectives, and~~ design and development guidelines and action steps that apply to a particular district are stated in the master plan for that district. *(better match text in 2025 Plan; no change in meaning)*
12. p. 16, 1st column to p. 17, 1st column; list of overall goals for downtown revitalization:

Urban form: ... Develop subdistricts, activity centers and in-town neighborhoods so that each have their own unique identity has its own character and sense of identity. *(better sentence construction; subject-verb agreement)*

Transportation system: Transform the existing Create an effective multimodal transportation system into a well- designed, integrated and effective multimodal system in downtown Boise that promotes walking, bicycling, carpooling and transit, as alternatives to the use of automobiles and reduces dependence on automobiles. Ease traffic flow in downtown by improvements to street design and signalization and by traffic demand management. Develop a multimodal transportation system for the region that addresses potential traffic impacts caused by future growth in

downtown, and makes travel to, from and within downtown by alternative modes practical and attractive. *(2025 plan includes revised version of this goal; revisions make transportation goal more comprehensive; recognize automobiles as a factor in transportation; address traffic flow, traffic demand management and regional transportation system)*

Parking system: Provide an adequate supply of parking while minimizing the use of land for surface parking lots. Achieve maximum utilization of parking by developing land uses that have offsetting parking demands, making private parking available to the public when not needed otherwise and connecting parking facilities to activity centers by alternative means of transportation *(clearer wording; correct typo in facilities)* making public parking facilities easily accessible from the various subdistricts and activity centers in downtown. *(clearer wording; correct typo in facilities)*

Chapter 4: Market Context

13. pp. 19-20, list of bullets that spans pages: remove semi-colon at the end of each statement and replace with period. *(make format of bulleted lists consistent; less confusing for reader— see #15 below)*
14. p. 20, 1st column, 1st bullet: Creating Create a people-oriented rather than auto-oriented environment...*(parallel construction; all other bullets in this list start with verb)*
15. p. 20, 1st column, 2nd bullet: Provide long-term parking in structures; and *(remove semi-colon and word “and”; end statement with period; less confusing for reader; in original form this item gives reader impression that something is missing)*
16. p. 22, 1st column, last sentence: (Four blocks of the Idaho State Capitol Mall are located in Old Boise / Eastside study area.) *(use proper name)*

Chapter 5: Preferred Development Concept

17. p. 24, 1st column, paragraph after bulleted list: ...identifying historically significant sites and opportunity sites for development and redevelopment (see Figures 5 and 8). *(direct reader to map showing historic resources)*
18. p. 24, footnote: ...as identified by two surveys of buildings in Old Boise / Eastside ~~commissioned by~~ prepared for the Boise City Historic Preservation Commission. *(eliminate duplication between commissioned and Commission)*
19. p. 26, 1st column, 1st paragraph: ...fact that some of the opportunity sites are located in the Idaho State Capitol Mall area...*(use proper name)*
20. p. 26, 1st column, 1st paragraph: Much of the demand for retail will be absorbed in buildings already built for that ~~purpose~~ purpose. *(correct typo)*
21. p. 26, 2nd column, 2nd bullet, last sentence: In the case of Old Boise / Eastside, this area recognizes the Idaho State Capitol Mall. *(use proper name)*
22. p. 28, footnote: In spring 2004, CCDC conducted a consumer survey of people living in Boise and in Ada County to measure the level of interest in downtown living. Two of the questions focused on what areas of downtown were considered the most desirable as a place to live. ~~The Old Boise District was seen as the first choice when respondents were asked to rank six areas of downtown.~~ When respondents were asked whether they were interested in living in six different areas of downtown, Old Boise was the area chosen most often

i.e. it ranked first of the six areas when responses were tallied. When asked to pick their top choice, respondents were asked what their first choice would be among the six areas, Old Boise ranked 2nd after the Heart of Downtown, but ahead of the Cultural District, River Street District, Parkside District and Westside District respectively. *(use more accurate and more understandable description of survey results)*

Chapter 6: Specific Plans, Guidelines & Action Steps

23. p. 31; Figure 7: Preferred Land Uses *(fix title of map)*
24. p. 32; Guideline 1.7: Consider the Idaho State Capitol Mall Plan and... *(use proper name)*
25. p. 32; 2nd action step, 1st bullet: Evaluate whether the existing Residential-Office (R-O) zoning designation... *(add abbreviation)*
26. p. 32; 2nd action step, 2nd bullet: Propose a revision to the Central Business (C-5) zoning designation that would classify residential as an activating use... *(add name of designation)*
27. p. 34; 2nd column; last paragraph: ...that apply to building and site design; throughout in the downtown urban renewal districts; ~~in downtown Boise~~. *(take out commas; simplify sentence)*
28. p. 38, Guideline 4.3: Differentiate between the Retail and Office Emphasis area and the ~~Residential Residential~~ Emphasis area (see Figure 6) through the type of streetscapes used. ...Refer to the Streetscape Character map (Figure 10) and the streetscape standards in Appendix B of the 2025 Downtown Boise Redevelopment Plan for specific requirements. *(correct typos; remove comma; add reference to Appendix B)*
29. p. 38, Guideline 4.7: Encourage the inclusion of public spaces in the business district, ~~that may which~~ serve as outdoor dining areas as well as gathering and socializing spaces, to increase the vitality of the area. *(take out may; public spaces need these characteristics to add vitality)*
30. p. 38, 2nd column, 1st action step: (The specific standards for each type of streetscape are contained in Appendix B of the 2025 Downtown Boise Redevelopment Plan.) *(add reference to Appendix B)*
31. p. 38, 2nd column, 1st action step: ...as part of the capital improvement program for the River ~~Myrtle~~ Myrtle/Old Boise Urban Renewal District. *(correct typo)*
32. p. 39, Figure 10: Celebratory corners removed from 6th and 5th streets. *(These corners are intended to be used on Main and Idaho where they are neighborhood streets to demarcate these streets as primary pedestrians routes between downtown and the Eastside neighborhood. They are not intended for use where brick sidewalks are used in the commercial district. This was an inadvertent error.)*
33. p. 40, 1st column, 7th bullet: In partnership with Boise City, make improvements to Capitol Park that are compatible to its location and function within the Idaho State Capitol Mall. *(use proper name)*
34. p. 40, column 2 and p. 42, column 1: new material added based on preliminary results from Downtown Boise Mobility Study as follows:

5.0 Transportation & Parking

Old Boise/Eastside is blessed with a grid street system that handles vehicle traffic easily with little congestion, and its attractive tree-lined streets encourage people to travel at slower speeds. However, new development anticipated between now and 2025 will add traffic in the area. The Downtown Boise Mobility Study (DBMS) modeled future traffic using the 2025 growth scenario. It proposes a policy for that a level of service (LOS) of D is acceptable during non-peak hours and an LOS of E is acceptable during peak hours on downtown streets (see sidebar). This policy recognizes that downtown is more congested than is typical in other areas of Boise, and a certain level of congestion is an acceptable tradeoff in order to maintain downtown Boise as the region's urban center. ~~This~~ The traffic analysis showed that the intersections of 3rd, 5th, and 6th streets at Front Street are operating at a level of service (LOS) of F and require improvements to meet the policy on LOS for downtown. certain intersections in the Old Boise/Eastside area will have a level of service (LOS) of F in 2025 during peak hours. These include Capitol, 6th and 3rd streets at Front Street, Capitol and Jefferson, 6th and Main streets, and the intersection of Broadway Avenue with Main and Idaho streets. The other intersections in Old Boise/Eastside, which were included in the modeling, are operating at a LOS of C or better during peak hours except for Main and Capitol and Idaho and 6th streets, which are operating at a LOS of D.

Most of the congestion predicted by the traffic model is associated with commuter traffic between the region and downtown since downtown is expected to continue growing as an employment center.

One of the goals of the DBMS is to encourage a multimodal transportation system...

35. p. 42, 2nd column 2, 1st full paragraph: This will extend the currently limited parking resources. Figure ~~44~~12 identifies potential locations of public parking garages. *(correct figure #)*
36. Figure ~~44~~12: Public Parking Garages *(correct figure #)*

Chapter 7: Implementation

37. p. 45, caption: ~~Summer stroll in C.W. Moore Park~~ C.W. Moore Park is one of two parks in the Old Boise/Eastside Planning Area. It provides a delightful place for a summer stroll or eating lunch on a busy work day. *(drop phrase at beginning and text to main body of caption so format of captions are consistent)*
38. p. 45, 1st column, 1st paragraph: Construction of ~~The Grove Plaza~~, reconstruction of downtown streets... *(correct name of The Grove)*
39. p. 48, column 1, list of figures: Correct total from 15,606, ~~400~~ to 15,606,700
40. p. 47, 2nd column, 2nd bullet: Assumptions about the number and size of parking garages that would be built and estimates of revenue that would be generated. *(add space)*
41. p. 49, 1st column, 2nd paragraph: The Old Boise/Eastside Master Plan establishes a vision and desired direction for this area, expressed through frame plans, objectives, design and development guidelines and

-
- action steps. *(correct reference so accurate)*
42. p. 49, 1st column, 3rd paragraph: Current zoning designations for Old Boise/Eastside are included on the Zoning Map in Appendix A or in the Existing Conditions Report available from CGDC. *(both references not necessary)*
43. p. 49, 1st column, 3rd paragraph: A majority of the property Most of the properties in Old Boise/Eastside is are in either the C-5D, C-5HD or R-OD zoning districts. These C-5 and R-O zoning districts and the DD and HD overlay districts are discussed below, as well as along with recommendations regarding possible revisions... *(make text clearer and more accurate given text that follows)*
44. p. 50, column 1: Design and Development Guideline *(match the title used for guidelines in Chapter 6)*

Additional Corrections for Publication

After City Council action on November 30, 2004 to approve the Old Boise–Eastside Master Plan, necessary corrections were made to the plan to prepare it for publication. These corrections were very limited and did not affect the substance of the document. They include the following items:

The format for the name of the planning area was changed from Old Boise/Eastside to Old Boise–Eastside. The format for the name of the amended River Street–Myrtle Street Urban Renewal District (which now includes the Old Boise–Eastside Study Area and other minor parcels) was changed from River–Myrtle/ Old Boise Urban Renewal District to River Myrtle–Old Boise Urban Renewal District. These changes have been made throughout the master plan.

Duplicate or misspelled words and stray punctuation have been corrected and the name of the master plan and the name of the planning area were made consistent throughout. Page numbers and figure numbers that were highlighted in blue (in order to assist in proofreading the document) have been changed to black. Notes in blue indicating text that needed to be edited once the outcome of the City Council's action was known have been removed, and the text has been edited appropriately and changed to black.

References to the 2025 Downtown Boise Redevelopment Plan (which sometimes appeared under this name and sometimes under 2025 Downtown Redevelopment Plan) have been changed to the 2025 Downtown Boise Redevelopment Plan throughout except for the title in the sidebar on page 16 where the title would be too long if the full name was used.

On page 38, in guideline 4.5 the word “using” has been removed and the word “installing” has been left to eliminate a redundancy in the following phrase: “...mark this network by installing special streetscape improvements on the sidewalks along these routes.”

RIVER MYRTLE - OLD BOISE URBAN RENEWAL PLAN¹

ATTACHMENT 4: PROPERTIES TO BE ACQUIRED

The properties listed below may or may not be acquired. The goals of the Plan can be achieved through development partnerships, close coordination with developers on building and site design, easements and methods other than outright acquisition. The typical practice of the Boise urban renewal agency since 1980 has been to acquire properties by mutual agreement with the property owner.

APN	Property Owner	Address	Acres	Purpose (see key below)
S1010223320	JRS Properties III L P	S 13th St	3.473	a, c
R1431000671	Boise Cold Storage (Boise VGT LLP)	W Grand Ave	0.350	b
R1431000646	Fuson United Northwest	1412 W Grand	0.050	b
R1431000631	JRS Properties III L P	W Grand Ave	0.050	b
R1431000618	Fuson United Northwest I	W Grand Ave	0.000	b
R1431000616	Reynolds Dean	1402 W Grand Ave	0.120	b
R1431000620	Fuson United Northwest I	W Grand Ave	0.000	b
R1431000601	Viehweg Tim	421 S 14 th St	0.200	b
R1431000777	S-Sixteen Limited Partner	1360 W Grand Ave	0.200	b
R1431000811	S-Sixteen Limited Partner	422 S 14 th St	0.140	b
R1431000826	JRS Properties III L P	1315 Borah St	0.230	b
R1749100426	Boise City	705 S 8 th St	1.320	a, d
R1013002036	Wilcomb LLC	329 W Grove St	0.950	a, b

Key: a) public parking garage and related uses
b) housing development
c) multi-modal transit facility with supporting retail and services
d) enhancements to community facilities

Other parcels may be acquired for the purpose of facilitating housing development, constructing or facilitating catalyst or demonstration projects, constructing public parking garages, implementing the Pioneer Corridor concept plan, restoring sections of the original downtown street grid such as 10th Street from Myrtle to Front streets, enhancing parks or other public spaces, or to implement other goals and objectives identified in the urban renewal plan.

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REV 3-21-2015.doc

¹ Name of urban renewal district as it appears on this page has been changed from the original "River-Myrtle/Old Boise Urban Renewal District" to the simplified version now in common usage "River Myrtle-Old Boise Urban Renewal District". Revision made in March 2015. Path to filename in 2004 originally appeared at bottom of page. This path has been replaced by current path.

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RIVER MYRTLE - OLD BOISE URBAN RENEWAL PLAN¹

ATTACHMENT 6: ECONOMIC FEASIBILITY STUDY

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FEASIBILITY STUDY REV 3-21-2015.doc

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**ECONOMIC FEASIBILITY OF THE
FIRST AMENDED AND RESTATED
RIVER-MYRTLE / OLD BOISE URBAN RENEWAL DISTRICT (2004)**

**(Annexing the Old Boise/Eastside Study Area and Additional Parcels to the
River Street/Myrtle Street Urban Renewal District formed in 1994)**

Prepared For

**THE CAPITAL CITY DEVELOPMENT CORPORATION
OF THE CITY OF BOISE
805 West Idaho Street, Suite 403
Boise, Idaho 83702**

Prepared By

**KEYSER MARSTON ASSOCIATES, INC.
500 South Grand Avenue, Suite 1480
Los Angeles, California 90071**

September 24, 2004 Update

**ECONOMIC FEASIBILITY OF THE FIRST AMENDED AND RESTATED
RIVER-MYRTLE/ OLD BOISE URBAN RENEWAL DISTRICT (2004)
(Annexing the Old Boise/ Eastside Study Area and Additional Parcels to the
River Street-Myrtle Street Urban Renewal District formed in 1994)**

September 24, 2004 Update

Idaho State Code 50-2905 provides that the urban renewal agency of the municipality shall prepare and adopt a plan for each revenue allocation area and submit the plan and recommendation for approval thereof to the local governing body. The plan shall include an economic feasibility study. The following represents such a study for the annexation of the Old Boise/ Eastside Study Area and additional parcels to the River Street/Myrtle Street Urban Renewal District formed in 1994 (the Combined Project) proposed by the Capital City Development Corporation (CCDC), including an update of financial information since the original feasibility study in 1994.

Economic feasibility, for purposes of this analysis, will be defined as a comparative analysis of one potential scenario of anticipated costs for implementation of the Combined Project to the resulting revenues expected to be generated by the Combined Project. Idaho State Code 50-2904 provides that the Urban Renewal Area Plans shall have a duration not-to-exceed 24 years from the adoption date. The duration of the revenue allocation financing¹ provision may be extended beyond this 24-year limit in order to repay outstanding bond debt, provided such bond maturity is not greater than 30 years. State law allows those plans adopted prior to 2000 which have a term beyond 24 years to continue through that duration. The original plan for the River/Myrtle URD adopted in 1994 established a term of 30 years. The proposed time parameters of the Combined Project are assumed to be dictated by the 30-year limit currently in place for the River/Myrtle URD.

This section contains a general discussion of overriding general assumptions, the costs associated with the anticipated urban renewal program and an evaluation of the general financing methods that may be available to the CCDC. Economic feasibility is determined through a summarized feasibility cash flow analysis for the Combined Project as summarized on Table 1.

A. GENERAL ASSUMPTIONS

1. Economic Feasibility Cash Flow

The economic feasibility cash flow has been created to identify the amount of discretionary resources available to fund the urban renewal projects, programs and activities identified by CCDC staff. A series of cash flow iterations was then prepared to ensure that annual expenditures did not exceed annual resources. To determine the annual resources available, the following assumptions were incorporated in the Table1 cash flow projection:

¹ Under Idaho Law the financing mechanism is referred to as "revenue allocation financing". In other states the term is referred to as "tax increment financing". These terms are used interchangeably in this Report.

- a. The revenue sources identified for the Combined Project include tax increment revenue, net bond proceeds from revenue allocation bonds, operating revenue from parking garage income, grant funds and interest earnings. No other funding sources have been identified or assumed for purposes of this feasibility study.
- b. Expenditures presented on Table 1 are restricted to the funding sources shown and annual expenditures in any given year may not exceed the amount of resources available. Expenditures include debt service, operating expenditures and capital expenditures.
- c. To the extent necessary, resources are carried forward into the subsequent year in order to ensure that adequate funding is available for future expenditures.
- d. The revenue and expenditure line items shown on Table 2 were provided by CCDC staff and present one scenario of economic feasibility of the Combined Project. When applicable, an inflationary increase has been applied.
- e. Revenue allocation bond financings are generally assumed in anticipation of major capital project construction scheduled in the cash flow scenario. The capital projects requiring such financing include parking garage construction and a proposed downtown circulator system project.
- f. Operating revenues include parking income projected from current and future garages in the Combined Project. Assuming that each new garage would provide up to a maximum of 500 parking spaces, a revenue per stall and expenditure per stall assumption was created based upon the average of actual revenue and expense figures provided by CCDC staff.

2. Tax Increment Projection

The economic feasibility study presents a long range cash flow analysis incorporating a projection of tax increment revenue for the Combined Project. The following assumptions were incorporated in the tax increment revenue projections shown on Tables 3-A and 3-B:

- a. Future values for the River/Myrtle URD were based upon revised current year assessed values reported by the Ada County Assessor and aggregated by the County Auditor. Future values for the Old Boise/Eastside URD were based upon the parcel-specific aggregation of current values as determined by CCDC staff.

Future real property values annually increase as a result of the new developments. River/Myrtle URD projects shown on Table 4-A were provided as of May 2004, by George K. Baum & Company. Old Boise/Eastside URD projects shown on Table 4-B were created from assumptions determined by CCDC staff as of July 2004 utilizing the March 2004 revised Downtown Boise Mobility Study.

- b. Additional trended value growth assumptions have been incorporated in the Table 3 projections to reflect future inflationary increases, transfers of ownership or other construction activities that may occur in the Combined Project. The annual percentage increase is assumed to be 2% per year for purposes of this analysis.
- c. The reported effective 2003-04 tax rate is 0.017936. Commencing in 2004-05, future tax rates are assumed based upon a 1% annual reduction over the term of the respective projections.
- d. An annual statutory allocation of tax increment revenues to local schools is incorporated in the Table 3 projections based upon provisions set forth in Idaho State Codes 50-2908 and 33-1002.

B. ESTIMATED TOTAL PROJECT COSTS

A determination of economic feasibility requires an identification of the potential costs associated with urban renewal of the Combined Project. Urban renewal could require significant participation from the CCDC in activities to promote and achieve the goals and objectives of the URD Plans and to address deteriorating conditions. The proposed activities and programs of the Combined Project have been prepared by CCDC staff and include administrative expenses and proposed capital improvement projects, programs and activities.

The economic feasibility cash flow presented on Table 1 assumes that all annual Combined Project expenditures will be limited to the amount of tax increment revenue, net bond proceeds, grants or other Combined Project resources available in each respective fiscal year. In other words, the

CCDC's ability to implement the anticipated urban renewal activities outlined on Table 2 will be limited by the amount of available resources generated in the Combined Project (annual tax increment, net bond proceeds and other income). To the extent other funding resources become available in future fiscal years once the Combined Project is implemented, funding of additional project expenditures not currently assumed in the Table 1 cash flow, will be feasible.

The urban renewal program described in this Report outlines a set of activities to be implemented by the CCDC for the purpose of facilitating private reinvestment in the Combined Project and eliminating physical and economic deteriorating influences. The estimated costs of the proposed urban renewal programs over the life of the Plan are as follows:

Operating Expenditures	\$52,095,000
Debt Service – Existing	58,124,000
Debt Service -- Future	77,357,000
Capital Expenditures	79,616,000
Additional Discretionary Expenditures	<u>3,668,000</u>
Totals Projected Costs	\$270,860,000

1. Operating Expenditures

Administration includes anticipated expenditures for CCDC administrative expenses, professional services, project operations and management, public parking system operations and the Combined Project's anticipated share of costs for a downtown circulator system (*referenced herein, but not detailed by CCDC*). Total projected funding for operating expenditures in this scenario is projected to total \$52,095,000 over the remaining term of the cash flow.

2. Debt Service

The CCDC will consider funding alternatives to finance the anticipated public projects assumed herein, including the issuance of revenue allocation bonds and line of credit financing. The CCDC may utilize tax increment revenues generated in the Combined Project to secure the debt service of revenue allocation bonds to assist in the financing of anticipated project costs. The issuance of tax-exempt bonds and the use of said proceeds are subject to certain federal tax restrictions.

The River/Myrtle URD currently makes debt service payments towards Series B and Series C Bonds, subordinate Series A Bonds, and the Courthouse Master Ground Lease. In addition, the cash flow incorporates the debt service contemplated for the Series 2004A and Series 2004B Bonds and a line of credit payment to Keybank. These are detailed on Table 2 and amount to \$58,124,000 over the term of the projection.

Under the economic feasibility scenario shown on Table 1, the CCDC could issue new additional revenue allocation bonds commencing in the first year after the annexation of the Old Boise/Eastside Study Area to the River/Myrtle URD. Commencing in various years thereafter, net tax increment revenues are assumed to be used for the issuance of additional new or escrowed bonds to finance all or a significant portion of public costs contemplated for the Combined Project. The combined future debt service payments assumed under the given scenario shown on Table 1 totals \$77,357,000 (based upon an assumed bond interest rate of 5%, a 1.15 times coverage requirement, net proceeds factor of 10%, and capitalized over the remaining term of the Plan).

3. Capital Expenditures

A significant step toward revitalization in the Combined Project would be the construction of various public parking garages and potential funding of various transportation-related projects of benefit to the Combined Project, including a proposed downtown circulator system project. Other projects, programs and activities proposed for the Combined Project include capital improvement expenditures for streetscape improvements, sidewalks, curbs and gutters, utility undergrounding, storm drain improvements, water quality treatment projects, bus shelters, telecommunications projects, traffic signal improvements, road resurfacing, public parks and open space, housing assistance, façade easements and improvements, property acquisition and funding of other potential special projects implemented by CCDC. Assumed costs summarized on Table 2 are based upon the amounts provided by CCDC staff.

Implementation of any capital improvements desired by the CCDC will be subject to the annual availability of funds. The Table 1 economic feasibility analysis portrays one scenario of funding based upon projected available resources each year, and a commitment to use bond financing and the securing of federal transportation grants is assumed. Total projected funding for the identified capital expenditure program that could be implemented under this scenario is projected to total \$79,616,000 over the term of the cash flow projection.

4. Additional Discretionary Expenses

To the extent future Combined Project resources continue to be allocated to CCDC and exceed assumed debt service, operating expenditures and capital expenditures, the cash flow projection assumes that CCDC will exercise its discretion in funding other future projects, programs or activities of benefit to the Combined Project beyond any that have been already identified by staff on Table 2. Although such discretionary revenue sources are not expected to be materially available until the last 4 years of the projection, the amount of available resources is projected to be \$3,668,000.

C. FINANCING METHODS AVAILABLE TO THE CCDC

The Plan is prepared with the intent of providing the CCDC with the necessary legal authority and flexibility to implement the revitalization of the Combined Project. The Plan authorizes the CCDC to finance the Combined Project with financial assistance from any or all sources allowed under Idaho State law. A discussion of potential other funding sources is presented in this section.

The CCDC is granted authority to create indebtedness, issue bonds, borrow funds or obtain advances in implementing and carrying out the specific purposes of an urban renewal plan. The CCDC is authorized to fund the principal and interest on the indebtedness, bond issues, borrowed funds or advances from tax increment revenue and any other funds available to the CCDC. To the extent that it is able to do so, the City or other public entities may also supply additional assistance through loans or grants for various public facilities or other project costs.

Potential revenue sources to fund project costs, as assumed in this economic feasibility cash flow, include, but are not limited to, tax increment revenues, bond proceeds, parking system revenues, grants and interest earnings. The estimated resources available to finance the anticipated urban renewal programs are summarized as follows:

Combined Net Tax Increment Revenue	\$168,918,000
Net Bond Proceeds – Future Issuances	49,540,000
Operating Revenue	25,118,000
Other Revenue	24,015,000
Interest Earnings	<u>3,269,000</u>
Total Projected Resources	\$270,860,000

1. Tax Increment Revenues

A summary of the projection of the incremental taxable values and resulting tax increment revenues for the Combined Project is shown on Table 3-A for the River/Myrtle URD and on Table 3-B for the Old Boise/Eastside URD. As shown on Table 3-A, the total projected gross tax increment revenue for the River/Myrtle URD amounts to nearly \$202.1 million, of which \$51.4 million would be allocated to the Boise Independent School District, as set forth in Idaho State Code 50-2908 and 33-1002. The net tax increment revenue generated by the River/Myrtle URD is \$150.7 million. The total projected gross tax increment revenue for the Old Boise/Eastside URD amounts to nearly \$24.4 million, of which \$6.2 million would be allocated to the Boise Independent School District. The net tax increment revenue generated by the Old Boise/Eastside URD is \$18.2 million. Combined net tax increment revenue amounts to \$168.9 million over the term of the projection.

Tax increment revenues are based upon increases in the annual incremental assessed valuation of the respective URDs that result from future new construction activities identified on Table 4-A and Table 4-B. An annual 2% real property value increase is also incorporated in the revenue projections.

2. Net Bond Proceeds

The CCDC may pledge tax increment revenues to secure the principal and interest payments of revenue allocation bonds issued to finance anticipated program costs. The issuance of tax-exempt bonds and the use of said proceeds are subject to federal tax restrictions. As discussed previously, the Table 1 economic feasibility projection assumes that the CCDC will issue tax-exempt revenue allocation bonds from time to time over the remaining term of the projection. Net bond proceeds shown on Table 1 are based upon a 5% interest rate, 1.15 times coverage requirement, maximum payment term through the duration of the Plan and 10% cost of issuance and bond reserve factor. Total net bond proceeds projected in the economic feasibility scenario assumed on Table 1 amount to \$49,540,000.

3. Operating Revenue

It is assumed that the Combined Project will benefit from income generated by parking garages constructed by CCDC. Operating revenues include parking income projected from current and future garages in the Combined Project. Assuming that each new garage would provide up to 500 parking spaces, a revenue per stall and expenditure per stall assumption was created based upon the average of actual revenue and expense figures provided by CCDC staff. Total gross operating revenue from the parking income projected in the economic feasibility scenario assumed on Table 1 amounts to \$25,118,000.

4. Other Revenue

Other Revenues can include federal grant funding, special district funding or access to any other state or local government funding sources that may become available to assist in the financing of future capital projects. For purposes of this projection, federal transit funds are assumed to be secured to offset 80% of the projected capital costs related to the proposed downtown circulator system project. In addition, a portion of the cost is assumed to be offset by local funding sources such as a Business Improvement District. The revenues generated from the BID special assessments may be used to secure bonded indebtedness to the targeted capital improvements. The amount of other revenue funding sources estimated in the cash flow amounts to \$24,015,000.

5. Interest Earnings

The CCDC may receive interest earnings generated from funds on deposit in the bond reserve funds, project operating funds, housing set aside funds and other special funds established for the Combined Project. Bond reserve funds are assumed to maintain a fund balance equivalent to outstanding debt service. Interest earnings are based upon an assumed 2% rate and are applied to the balances available in the respective funds. Total interest earnings in the Table 1 forecast amount to \$3,269,000.

6. Potential Other Funding Sources

Although not assumed or incorporated in the Table 1 feasibility study, the CCDC may consider other potential funding sources allowable under the law to finance the anticipated urban renewal programs discussed above. The following funding sources may be available in subsequent fiscal years to the Combined Project:

- a. Local Improvement Districts (LID) – Once created, the revenues generated from the LID special assessment may be used to secure bonded indebtedness to fund capital improvements.
- b. Historic Tax Credits – To the extent applicable, investment tax credits of up to 20% of qualifying development costs may be taken by private developers towards the renovation of designated historic properties.
- c. SBA 504 Program – The program may be used as a means of reducing interest rates and limiting equity participation for land, building costs, equipment and lease hold improvements by the sale of reduced interest debentures.
- d. Certificates of Participation (COP) – Financing of public facilities can occur when a third party constructs the public project and then leases the facility to a public entity. The lease income secures repayment of the COP.
- e. HOME Program – HOME funds are used to fund various housing programs, rehabilitation, new construction, rent subsidy and other special housing needs programs.
- f. City Housing Rehabilitation Fund – The City administers a Housing Rehabilitation Fund for qualifying applicants.

- g. Community Development Block Grant (CDBG) – CDBG funds are administered through the Department of Housing and Urban Development and are allocated to assist eligible activities of the CCDC.
- h. Surface Transportation Program – Authorized under the Intermodal Surface Transportation Efficiency Act, this program is intended to fund eligible projects, which will enhance air quality and ease traffic congestion.
- i. Developer Contributions – Developers may be required to make a one time contribution to fund related capital costs. Typically, such contributions are determined by a formula established by the City and the contributions can be applied towards any legal purposes of the CCDC.
- j. Developer Advance – Developer advances may allow a project to commence in instances when initial Project revenues are insufficient or unavailable. The amount of such advances, and repayment provisions for the same, would be subject to negotiation.
- k. Long Term Ground Lease – Lease income generated by long term ground leases of CCDC owned properties are a potential means to allow such properties to be developed. The lease amounts are based on an agreed upon formula that typically includes a base rate with increases based upon development performance.

D. ECONOMIC FEASIBILITY

The anticipated costs to implement a program of revitalization in the Combined Project will require significant participation from the CCDC as it implements activities that promote and achieve the goals and objectives. Economic feasibility of the Combined Project has been determined based upon the Table 1 cash flow projection of the anticipated costs for implementation of the proposed urban renewal program to the resulting projected resources expected to be generated over the remaining life of the Combined Project.

The economic feasibility summarized on Table 1 was created to represent only one scenario of economic feasibility. At the discretion of the CCDC, other funding sources discussed above may present viable funding alternatives for economic feasibility of the merger. Although the CCDC may consider other funding sources permitted in the URD plans, not all of the funding sources may be available or be feasible for the CCDC to use in financing the anticipated costs.

E. LIMITING CONDITIONS TO PROJECTIONS AND CONCLUSIONS

Keyser Marston Associates, Inc. (KMA) has made extensive efforts to confirm the accuracy and timeliness of the information contained in this report. Such information was compiled from a variety of sources deemed to be reliable including Ada County and CCDC staff. Although KMA believes all information in this document is correct, it does not guarantee the accuracy of such and assumes no responsibility for inaccuracies in the information provided by third parties. Further, no guarantee is made as to the possible effect on development of current or future federal, state, or local legislation including environmental or ecological matters.

1. The analysis contained in this document is based, in part, on data from secondary sources such as local government staff and other third parties. While KMA believes that these sources are reliable, we cannot guarantee the accuracy of such data and assumes no responsibility for any inaccuracies in the information provided by third parties.
2. The analysis assumes that neither the local nor national economy will experience a recession. If an unforeseen change occurs in the economy, the conclusions contained herein may no longer be valid.
3. The new development values projected to be added to the property tax rolls will not vary significantly from that estimated on Tables 4-A and 4-B.
4. Any estimates of development costs, income and expense projections are based on the best available project-specific data as well as the experiences of similar projects. They are not intended to reflect actual commitments guaranteed by CCDC for future implementation. No warranty or representation is made that any of the estimates or projections will actually materialize.

The accompanying projections and analyses are based on estimates and assumptions that were developed using currently available economic data, project specific data and other relevant information. It is the nature of forecasting, however, that some assumptions may not materialize and unanticipated events and circumstances may occur. Such changes are likely to be material to the projections and conclusions herein and, if they occur, will differ from the projections shown.

Table 1

**Economic Feasibility of the Amended & Restated
River-Myrtle/ Old Boise Combined District
Capital City Development Corporation
of the City of Boise
(000's Omitted)**

	1	2	3	4	5	6	7	8	9	10	11	12
FY Ending September 30	2005	2006	2007	2008	2009	2010	2011	2012	2013	2014	2015	2016
I. Resources:												
Resources Carried Forward	0	3,569	5,053	2,865	2,773	2,384	134	411	1,753	891	908	3,046
Combined Net Increment (Tables 3-A and 3-B)	2,437	2,977	3,845	4,498	4,985	5,836	6,768	7,432	7,974	8,451	9,048	9,523
Net Bond Proceeds - Future Borrowing	10,805	2,680	0	2,656	3,235	5,858	6,017	3,933	2,885	2,237	2,759	1,835
Operating Revenue (Table 2)	643	643	643	643	643	1,118	1,118	1,118	1,308	1,308	1,308	1,308
Other Revenue (Table 2)	0	0	0	0	0	0	0	0	0	0	8,005	8,005
Interest Earnings at 2%	22	117	167	129	134	135	105	127	165	157	166	219
Total Projected Resources	13,907	9,986	9,708	10,790	11,770	15,331	14,143	13,021	14,085	13,044	22,194	23,937
II. Expenditures:												
Existing Debt Service (Table 2)	1,126	1,341	2,074	2,391	2,468	2,550	2,649	2,733	2,818	2,912	3,000	3,097
Future Debt Service - Future Borrowing	0	993	1,248	1,248	1,520	1,866	2,524	3,236	3,729	4,115	4,436	4,868
Operating Expenditures (Table 2)	1,735	1,818	1,935	2,032	2,113	2,351	2,386	2,423	2,582	2,622	2,613	2,656
Capital Expenditures (Table 2)	7,477	781	1,586	2,346	3,285	8,429	6,173	2,876	4,064	2,488	9,100	10,520
Total Projected Expenditures	10,338	4,933	6,843	8,017	9,386	15,196	13,732	11,268	13,193	12,136	19,148	21,141
III. Annual Net Gain/ (Shortfall)												
Future Discretionary Exp (65% of Net Gain)	3,569	5,053	2,865	2,773	2,384	134	411	1,753	891	908	3,046	2,795
Amount Remaining	0	0	0	0	0	0	0	0	0	0	0	0
	3,569	5,053	2,865	2,773	2,384	134	411	1,753	891	908	3,046	2,795

Table 1

**Economic Feasibility of the Amended & Rest
River-Myrtle/ Old Boise Combined District
Capital City Development Corporation
of the City of Boise
(000's Omitted)**

	FY Ending September 30										TI Ends 2024	Memo Totals
	13	14	15	16	17	18	19	20	21	22	23	
I. Resources:												
Resources Carried Forward	2,795	1,273	93	2	350	61	81	547				
Combined Net Increment (Tables 3-A and 3-B)	10,003	10,579	11,069	11,605	12,147	12,693	13,245	13,802				168,918
Net Bond Proceeds - Future Borrowing	1,649	1,787	1,202	0	0	0	0	0				49,540
Operating Revenue (Table 2)	1,665	1,665	1,665	1,665	1,665	1,665	1,665	1,665				25,118
Other Revenue (Table 2)	8,005	0	0	0	0	0	0	0				24,015
Interest Earnings at 2%	222	201	187	194	204	200	203	215				3,269
Total Projected Resources	24,339	15,505	14,216	13,466	14,366	14,619	15,194	16,228				270,860
II. Expenditures:												
Existing Debt Service (Table 2)	3,198	3,307	3,425	3,539	3,668	3,799	3,941	4,088				58,124
Future Debt Service - Future Borrowing	5,183	5,500	5,891	6,200	6,200	6,200	6,200	6,200				77,357
Operating Expenditures (Table 2)	2,928	2,974	3,022	3,072	3,124	3,179	3,235	3,294				52,095
Capital Expenditures (Table 2)	11,758	3,630	1,877	304	1,199	1,210	257	257				79,616
Total Projected Expenditures	23,067	15,412	14,214	13,116	14,192	14,387	13,632	13,838				267,192
III. Annual Net Gain/ (Shortfall)												
Future Discretionary Exp (65% of Net Gain)	1,273	93	2	350	174	232	1,562	2,389				3,668
Amount Remaining	0	0	0	0	113	151	1,015	2,389				
	1,273	93	2	350	61	81	547	0				270,860

Table 2

Cash Flow Detail
Economic Feasibility of the Amended & Restated
River-Myrtle/ Old Boise Combined District
Boise Capital City Development Corporation

		1	2	3	4	5	6	7	8	9	10	11
	FY Ending September 30	2005	2006	2007	2008	2009	2010	2011	2012	2013	2014	2015
I. RESOURCES												
A. Operating Revenue												
1	Parking System Revenue - New Spaces at \$951/space	361,336	361,336	361,336	361,336	361,336	836,778	836,778	836,778	1,026,955	1,026,955	1,026,955
2	Parking System Revenue - Existing 816 spaces RM at \$345/space	281,259	281,259	281,259	281,259	281,259	281,259	281,259	281,259	281,259	281,259	281,259
	Total Operating Revenue	642,595	642,595	642,595	642,595	642,595	1,118,037	1,118,037	1,118,037	1,308,214	1,308,214	1,308,214
B. Other Revenue												
1	Federal Grant - Circulator System 80% (RM/OB)	0	0	0	0	0	0	0	0	0	0	6,861,333
2	BID/ Other Local Sources - Circulator System	0	0	0	0	0	0	0	0	0	0	1,143,556
	Total Other Revenue	0	0	0	0	0	0	0	0	0	0	8,004,889
II. EXPENDITURES												
A. Existing Debt Service												
1	Series B & C Bonds less Capitalized Interest	0	192,582	546,813	591,579	630,057	682,148	722,787	776,850	829,235	879,911	933,784
2	Series 2004 A Bonds less Capitalized Interest (Proposed)	0	0	0	228,012	245,061	245,061	710,653	717,673	729,064	744,693	754,627
3	Series 2004 B Bonds (Proposed)	43,669	43,669	401,294	420,726	424,642	433,048	0	0	0	0	0
4	Subordinate Series A Bonds (AHA Payment)	277,000	277,000	277,000	277,000	277,000	277,000	277,000	277,000	277,000	277,000	277,000
5	Keybank Line of Credit (RM)	100,000	102,000	104,040	106,121	108,243	110,408	112,616	114,869	117,166	119,509	121,899
6	Courthouse Master Ground Lease	705,200	725,500	744,500	767,150	783,150	802,797	825,812	846,907	865,895	890,495	912,395
	Total Bond Debt Service	1,125,869	1,340,751	2,073,647	2,390,588	2,468,153	2,550,462	2,648,868	2,733,299	2,818,360	2,911,608	2,999,705
B. Operating Expenditures												
1	Operating Expenditures	420,877	437,712	455,221	473,429	492,367	512,061	532,544	553,845	575,999	599,039	623,001
2	Professional Services	300,000	300,000	300,000	300,000	300,000	200,000	200,000	200,000	200,000	200,000	150,000
3	Parking System Operations (New Garages RM/OB)	230,066	230,066	230,066	230,066	230,066	532,785	532,785	532,785	653,872	653,872	653,872
4	Parking System Operations (Existing 816 spaces RM)	238,094	238,094	238,094	238,094	238,094	238,094	238,094	238,094	238,094	238,094	238,094
5	Downtown Circulator System	0	0	0	0	0	0	0	0	0	0	0
6	Operating -Transfers Out (10% RM/OB TI or \$500K)	243,723	297,734	384,536	449,781	498,525	500,000	500,000	500,000	500,000	500,000	500,000
7	Operating -Transfers Out (Fund 101/205)	302,722	314,831	327,424	340,521	354,142	368,308	383,040	398,362	414,296	430,868	448,103
	Total Operating Expenditures	1,735,483	1,818,437	1,935,341	2,031,892	2,113,194	2,351,248	2,386,463	2,423,086	2,582,262	2,621,874	2,613,070
C. Capital Expenditures - Old Boise/Eastside												
1	Parking Structure - Parcel 9-5th & Grove - Land	0	0	0	0	468,750	0	0	0	0	0	0
2	Parking Structure - Parcel 9-5th & Grove - Imps	0	0	0	0	0	2,450,000	1,050,000	0	0	0	0
3	Streetcar/Downtown Circulator System	0	0	0	0	0	0	0	0	0	0	1,776,667
4	Bus Shelters	0	0	0	0	0	0	0	0	0	0	55,000
5	Streetscaping/Sidewalks/Curb & Gutter	0	100,000	300,000	300,000	300,000	300,000	250,000	250,000	250,000	200,000	120,000
6	Sewer/ Storm Drainage	0	0	0	0	0	0	0	0	25,000	0	0
7	Water Quality Treatment	0	0	0	0	0	0	0	0	0	0	0
8	Utility Undergrounding	0	0	0	100,000	100,000	100,000	100,000	0	0	0	0
9	Telecommunications	0	0	0	0	0	0	25,000	0	0	0	25,000

Table 2

Cash Flow Detail
Economic Feasibility of the Amended & Restated
River-Myrtle/ Old Boise Combined District
Boise Capital City Development Corporation

	FY Ending September 30	1 2005	2 2006	3 2007	4 2008	5 2009	6 2010	7 2011	8 2012	9 2013	10 2014	11 2015
10 Geothermal Capacity Increase (extending lines)		0	0	0	0	0	0	0	0	0	0	0
11 Traffic Signals		0	0	0	0	0	0	0	150,000	0	0	0
12 Road Re-Surfacing		0	0	0	0	0	0	0	50,000	0	0	0
13 Special Purpose Park (or similar facility)		0	0	0	0	0	0	0	0	25,000	0	0
14 Other Park and Open Space		0	0	0	0	0	0	0	0	5,000	0	0
15 Housing		0	0	0	0	0	0	0	0	0	0	0
16 Façade/Building Rehab. Easements		0	0	0	0	3,000	3,000	3,000	3,000	3,000	3,000	0
17 Façade/Building Rehab. Improvements		0	0	0	0	3,000	3,000	3,000	3,000	3,000	3,000	2,000
18 Property Acquisition		0	0	0	0	0	0	487,500	487,500	0	0	0
19 Improve Vehicular/ Pedestrian Access		0	0	0	0	0	0	0	20,000	0	0	0
20 Special Projects		0	0	0	0	0	0	0	10,000	10,000	10,000	10,000
21 Cultural Investments (1% of garage cost)		0	0	0	0	0	24,500	10,500	0	0	0	0
Capital Expenditures - River/Myrtle												
22 Expansion of Julia Davis Park		0	0	0	0	480,000	0	0	0	0	0	0
23 Streetcar/Downtown Circulator System		0	0	0	0	0	0	0	0	0	0	6,800,000
24 Pioneer Corr - Myrtle to River - land acq	120,200	120,200	120,200	120,200	120,200	240,400	240,400	240,400	0	0	0	0
25 Pioneer Corr - Boise River to River - land acq		0	0	0	0	0	456,000	456,000	456,000	456,000	456,000	0
26 Pioneer Corr - Land acq adjacent		0	0	0	450,000	450,000	0	0	0	0	0	0
27 Pioneer Corr - walkway, alignment, other construction		0	114,000	114,000	114,000	114,000	114,000	114,000	114,000	114,000	114,000	114,000
28 Broad St Ext - 11th to 10th.		0	0	0	0	0	489,600	0	0	0	0	0
29 Broad St Ext - 10th to 9th.		0	0	0	0	0	0	489,600	0	0	0	0
30 Broad St Ext - improvements 11th to 10th		0	0	0	0	0	0	47,600	0	0	0	0
31 Broad St Ext - improvements 10th to 9th		0	0	0	0	0	0	0	47,600	0	0	0
32 Broad St Ext - Streetscape 11th to 10th - north side		0	0	0	0	0	216,920	0	0	0	0	0
33 Broad St Ext - Streetscape 11th to 10th - south side		0	0	0	0	0	216,920	0	0	0	0	0
34 Broad St Ext - Streetscape 10th to 9th - north side		0	0	0	0	0	0	216,920	0	0	0	0
35 Broad St Ext - Streetscape 10th to 9th - south side		0	0	0	0	0	0	216,920	0	0	0	0
36 Reestablish 10th St-Myrtle to Front - Easement acq		0	0	0	0	0	288,000	288,000	0	0	0	0
37 Reestablish 10th St-Myrtle to Front - Improvements		0	0	0	0	0	0	62,000	0	0	0	0
38 Reestablish 10th St-Myrtle to Front - Streetscape west		0	0	0	0	0	0	0	118,800	0	0	0
39 Reestablish 10th St-Myrtle to Front - Streetscape east		0	0	0	0	0	0	0	118,800	0	0	0
40 Parking Structure - Parcel 9-5th & Grove - Land		0	0	0	0	468,750	0	0	0	0	0	0
41 Parking Structure - Parcel 9-5th & Grove - Imps	250	0	0	0	0	0	2,450,000	1,050,000	0	0	0	0
42 Parking Structure - W End - Land		0	0	0	0	0	0	0	0	0	0	0
43 Parking Structure - W End - Imps	375	0	0	0	0	0	0	0	0	0	0	0
44 Parking Structure - BoDo - Imps	380	6,000,000	0	0	0	0	0	0	0	0	0	0
45 Parking Structure - Battery Street - Imps	200	0	0	0	0	0	0	0	0	0	0	0
46 Streetscape - Capitol Boulevard		0	250,000	250,000	250,000	250,000	250,000	250,000	250,000	2,100,000	900,000	0
47 Streetscape - BoDo		1,100,000	0	0	0	0	0	0	0	0	0	0
48 Streetscape - Grove St east of Basque Block, 6th to 5th.		0	0	0	210,000	210,000	0	0	0	0	0	0

Table 2
Cash Flow Detail
Economic Feasibility of the Amended & Restated
River-Myrtle/ Old Boise Combined District
Boise Capital City Development Corporation

	FY Ending September 30										
	1	2	3	4	5	6	7	8	9	10	11
	2005	2006	2007	2008	2009	2010	2011	2012	2013	2014	2015
49 Other Streetscape Projects	196,875	196,875	196,875	196,875	196,875	196,875	196,875	196,875	196,875	196,875	196,875
50 Property Acquisition	0	0	0	0	0	0	0	600,000	0	0	0
51 Sewer/ Storm Drainage	0	0	75,000	75,000	0	75,000	75,000	0	75,000	75,000	0
52 Water Quality Treatment	0	0	30,000	30,000	0	30,000	30,000	0	30,000	30,000	0
53 Utility Undergrounding	0	0	100,000	100,000	0	100,000	100,000	0	100,000	100,000	0
54 Telecommunications	0	0	80,000	80,000	0	80,000	80,000	0	80,000	80,000	0
55 Geothermal Capacity Increase (extending lines)	0	0	60,000	60,000	0	60,000	60,000	0	60,000	60,000	0
56 Traffic Signals	0	0	80,000	80,000	0	80,000	80,000	0	80,000	80,000	0
57 Road Re-Surfacing	0	0	20,000	20,000	0	20,000	20,000	0	20,000	20,000	0
58 Other Park and Open Space	0	0	20,000	20,000	0	20,000	20,000	0	20,000	20,000	0
59 Housing	0	0	60,000	60,000	0	60,000	60,000	0	60,000	60,000	0
60 Façade/Building Rehab. Easements	0	0	10,000	10,000	0	10,000	10,000	0	10,000	10,000	0
61 Improve Vehicular/ Pedestrian Access	0	0	30,000	30,000	0	30,000	30,000	0	30,000	30,000	0
62 Special Projects	0	0	40,000	40,000	0	40,000	40,000	0	40,000	40,000	0
63 Cultural Investments (1% of garage cost)	60,000	0	0	0	0	24,500	10,500	0	21,000	0	0
Total Capital Expenditures	7,477,075	781,075	1,586,075	2,346,075	3,284,775	8,428,715	6,172,815	2,875,575	4,063,875	2,487,875	9,099,542

Table 2
Cash Flow Detail
Economic Feasibility of the Amended & Restated
River-Myrtle/ Old Boise Combined District
Boise Capital City Development Corporation

	FY Ending September 30	12 2016	13 2017	14 2018	15 2019	16 2020	17 2021	18 2022	19 2023	TIF Ends 20 2024	Memo Totals
10 Geothermal Capacity Increase (extending lines)		34,000		0	0	0	0	0	33,000	0	67,000
11 Traffic Signals		0	150,000	0	0	0	0	0	0	0	300,000
12 Road Re-Surfacing		0	50,000	0	0	0	0	0	0	0	100,000
13 Special Purpose Park (or similar facility)		0	25,000	0	0	0	0	0	0	0	50,000
14 Other Park and Open Space		0	0	0	0	5,000	0	0	0	0	10,000
15 Housing		50,000	0	0	0	0	0	0	0	0	50,000
16 Façade/Building Rehab. Easements		0	0	0	0	0	0	0	0	0	18,000
17 Façade/Building Rehab. Improvements		0	0	0	0	0	0	0	0	0	20,000
18 Property Acquisition		390,000	390,000	0	0	0	0	0	0	0	1,755,000
19 Improve Vehicular/ Pedestrian Access		0	30,000	0	0	0	0	0	0	0	50,000
20 Special Projects		10,000	10,000	10,000	0	10,000	10,000	10,000	0	0	100,000
21 Cultural Investments (1% of garage cost)		0	0	0	0	0	0	0	0	0	35,000
Capital Expenditures - River/Myrtle											
22 Expansion of Julia Davis Park		0	0	0	0	0	0	0	0	0	480,000
23 Streetcar/Downtown Circulator System		6,800,000	6,800,000	0	0	0	0	0	0	0	20,400,000
24 Pioneer Corr - Myrtle to River - land acq		0	0	0	0	0	0	0	0	0	1,202,000
25 Pioneer Corr - Boise River to River - land acq		0	0	0	0	0	0	0	0	0	2,280,000
26 Pioneer Corr - Land acq adjacent		0	0	0	0	0	0	0	0	0	900,000
27 Pioneer Corr - walkway, alignment, other construction		0	0	0	0	0	0	0	0	0	1,140,000
28 Broad St Ext - 11th to 10th.		0	0	0	0	0	0	0	0	0	489,600
29 Broad St Ext - 10th to 9th.		0	0	0	0	0	0	0	0	0	489,600
30 Broad St Ext - improvements 11th to 10th		0	0	0	0	0	0	0	0	0	47,600
31 Broad St Ext - improvements 10th to 9th		0	0	0	0	0	0	0	0	0	47,600
32 Broad St Ext - Streetscape 11th to 10th - north side		0	0	0	0	0	0	0	0	0	216,920
33 Broad St Ext - Streetscape 11th to 10th - south side		0	0	0	0	0	0	0	0	0	216,920
34 Broad St Ext - Streetscape 10th to 9th - north side		0	0	0	0	0	0	0	0	0	216,920
35 Broad St Ext - Streetscape 10th to 9th - south side		0	0	0	0	0	0	0	0	0	216,920
36 Reestablish 10th St-Myrtle to Front - Easement acq		0	0	0	0	0	0	0	0	0	576,000
37 Reestablish 10th St-Myrtle to Front - Improvements		0	0	0	0	0	0	0	0	0	62,000
38 Reestablish 10th St-Myrtle to Front - Streetscape west		0	0	0	0	0	0	0	0	0	118,800
39 Reestablish 10th St-Myrtle to Front - Streetscape east		0	0	0	0	0	0	0	0	0	118,800
40 Parking Structure - Parcel 9-5th & Grove - Land		0	0	0	0	0	0	0	0	0	468,750
41 Parking Structure - Parcel 9-5th & Grove - Imps	250	0	0	0	0	0	0	0	0	0	3,500,000
42 Parking Structure - W End - Land		500,000	500,000	0	0	0	0	0	0	0	1,000,000
43 Parking Structure - W End - Imps	375	0	1,068,750	2,671,875	1,603,125	0	0	0	0	0	5,343,750
44 Parking Structure - BoDo - Imps	380	0	0	0	0	0	0	0	0	0	6,000,000
45 Parking Structure - Battery Street - Imps	200	0	0	0	0	0	0	0	0	0	3,000,000
46 Streetscape - Capitol Boulevard		0	0	0	0	0	0	0	0	0	2,000,000
47 Streetscape - BoDo		0	0	0	0	0	0	0	0	0	1,100,000
48 Streetscape - Grove St east of Basque Block, 6th to 5th.		0	0	0	0	0	0	0	0	0	420,000

Table 2
Cash Flow Detail
Economic Feasibility of the Amended & Restated
River-Myrtle/ Old Boise Combined District
Boise Capital City Development Corporation

	FY Ending September 30	12	13	14	15	16	17	18	19	20	TIF Ends	Memo Totals
		2016	2017	2018	2019	2020	2021	2022	2023	2024		
49 Other Streetscape Projects		196,875	196,875	196,875	196,875	196,875	196,875	196,875	196,875	196,875		3,937,500
50 Property Acquisition		600,000	0	0	0	0	300,000	300,000	0	0		1,800,000
51 Sewer/ Storm Drainage		0	75,000	75,000	0	0	75,000	75,000	0	0		750,000
52 Water Quality Treatment		0	30,000	30,000	0	0	30,000	30,000	0	0		300,000
53 Utility Undergrounding		0	100,000	100,000	0	0	100,000	100,000	0	0		1,000,000
54 Telecommunications		0	80,000	80,000	0	0	80,000	80,000	0	0		800,000
55 Geothermal Capacity Increase (extending lines)		0	60,000	60,000	0	0	60,000	60,000	0	0		600,000
56 Traffic Signals		0	80,000	80,000	0	0	80,000	80,000	0	0		800,000
57 Road Re-Surfacing		0	20,000	20,000	0	0	20,000	20,000	0	0		200,000
58 Other Park and Open Space		0	20,000	20,000	0	0	20,000	20,000	0	0		200,000
59 Housing		0	60,000	60,000	0	0	60,000	60,000	0	0		600,000
60 Façade/Building Rehab. Easements		0	10,000	10,000	0	0	10,000	10,000	0	0		100,000
61 Improve Vehicular/ Pedestrian Access		0	30,000	30,000	0	0	30,000	30,000	0	0		300,000
62 Special Projects		0	40,000	40,000	0	0	40,000	40,000	0	0		400,000
63 Cultural Investments (1% of garage cost)		0	10,688	26,719	16,031	0	0	0	0	0		169,438
Total Capital Expenditures		10,519,842	11,757,979	3,630,469	1,876,591	304,375	1,199,375	1,209,875	256,875	256,875		79,615,727

Table 3 - A
Tax Increment Projection
River Street / Myrtle Street District
Capital City Development Corporation
of the City of Boise
(000's Omitted)

	1	2	3	4	5	6	7	8	9	10	11	12
	Projected											
	FY Ending September 30											
	2005	2006	2007	2008	2009	2010	2011	2012	2013	2014	2015	2016
I. Real Property - Existing Increment												
Inflationary Growth of Existing URD (1)	101,599	153,690	192,320	246,112	287,722	313,991	370,395	428,930	471,711	509,879	549,386	598,368
New Development Value (Table 4-A)	2,032	3,074	3,846	4,922	5,754	6,280	7,408	8,579	9,434	10,198	10,988	11,967
Total Real Property from Existing URD	50,059	35,556	49,946	36,687	20,515	50,125	51,127	34,202	28,734	29,309	37,994	32,662
	153,690	192,320	246,112	287,722	313,991	370,395	428,930	471,711	509,879	549,386	598,368	642,997
Real Property - Added Area Increment												
Inflationary Growth of Added Areas (2)	186	376	570	768	970	1,176	1,385	1,599	1,818	2,040	2,268	2,499
Total Real Property from Added Area	186	376	570	768	970	1,176	1,385	1,599	1,818	2,040	2,268	2,499
Personal Property Increment												
Inflationary Growth (1)	19,660	21,868	23,689	28,921	31,486	32,390	37,401	42,513	45,602	47,252	48,936	52,109
New Development Value (Table 4-A)	393	437	474	578	630	648	748	850	912	945	979	1,042
Total Personal Property	1,815	1,383	4,759	1,986	274	4,364	4,364	2,239	739	739	2,194	794
	21,868	23,689	28,921	31,486	32,390	37,401	42,513	45,602	47,252	48,936	52,109	53,945
II. Total Incremental Value												
Assumed Tax Levy (reduced 1% per year)	175,744	216,385	275,603	319,975	347,350	408,972	472,829	518,912	558,949	600,362	652,744	699,441
	0.017757	0.017579	0.017403	0.017229	0.017057	0.016886	0.016718	0.016550	0.016385	0.016221	0.016059	0.015898
III. Gross Tax Increment Revenue												
Less School District Share at -0.4%	3,121	3,804	4,796	5,513	5,925	6,906	7,905	8,588	9,158	9,738	10,482	11,120
Net Tax Increment Revenue	(703)	(866)	(1,102)	(1,280)	(1,389)	(1,636)	(1,891)	(2,076)	(2,236)	(2,401)	(2,611)	(2,798)
	2,418	2,938	3,694	4,233	4,535	5,270	6,013	6,513	6,923	7,337	7,871	8,322

(1) An annual inflationary increase of 2% is assumed
(2) Reflects the inflationary increase of value from the proposed addition of other parcels to the URD.

Table 3 - A
Tax Increment Projection
River Street /Myrtle Street District
Capital City Development Corporation
of the City of Boise
(000's Omitted)

	FY Ending September 30	2017	2018	2019	2020	2021	2022	2023	TIF Ends 2024	Memo Total
I. Real Property - Existing Increment										
Inflationary Growth of Existing URD (1)		642,997	689,172	743,365	792,893	847,737	904,457	963,106	1,023,740	
New Development Value (Table 4-A)		12,860	13,783	14,867	15,858	16,955	18,089	19,262	20,475	
Total Real Property from Existing URD		33,315	40,410	34,661	38,985	39,765	40,560	41,372	42,199	
		689,172	743,365	792,893	847,737	904,457	963,106	1,023,740	1,086,414	
Real Property - Added Area Increment										
Inflationary Growth of Added Areas (2)		2,736	2,977	3,223	3,473	3,729	3,990	4,256	4,528	
Total Real Property from Added Area		2,736	2,977	3,223	3,473	3,729	3,990	4,256	4,528	
Personal Property Increment										
Inflationary Growth (1)		53,945	55,818	59,148	61,125	63,222	65,361	67,543	69,768	
New Development Value (Table 4-A)		1,079	1,116	1,183	1,222	1,264	1,307	1,351	1,395	
Total Personal Property		794	2,214	794	875	875	875	875	875	
		55,818	59,148	61,125	63,222	65,361	67,543	69,768	72,038	
II. Total Incremental Value										
Assumed Tax Levy (reduced 1% per year)		747,725	805,490	857,241	914,432	973,547	1,034,639	1,097,764	1,162,979	
		0.015739	0.015582	0.015426	0.015272	0.015119	0.014968	0.014818	0.014670	
III. Gross Tax Increment Revenue										
Less School District Share at -0.4%		11,769	12,551	13,224	13,965	14,719	15,486	16,267	17,061	202,098
		(2,991)	(3,222)	(3,429)	(3,658)	(3,894)	(4,139)	(4,391)	(4,652)	(51,364)
Net Tax Increment Revenue		8,778	9,329	9,795	10,307	10,825	11,348	11,876	12,409	150,734

(1) An annual inflationary increase of 2% is assumed
(2) Reflects the inflationary increase of value from the proposed addition of other parcels to the URD.

Table 3 - B
Tax Increment Projection
Old Boise/ Eastside Study Area
Capital City Development Corporation
of the City of Boise
(000's Omitted)

	1	2	3	4	5	6	7	8	9	10	11
FY Ending September 30	Projected										
	2005	2006	2007	2008	2009	2010	2011	2012	2013	2014	2015
I. Real Property											
Inflationary Growth (1)	71,139	72,561	74,012	82,183	90,651	104,837	114,033	129,187	142,833	154,161	160,149
New Development Value (Table 4-B)	1,423	1,451	1,480	1,644	1,813	2,097	2,281	2,584	2,857	3,083	3,203
Total Real Property	0	0	6,690	6,824	12,373	7,100	12,873	11,062	8,472	2,905	2,963
	72,561	74,012	82,183	90,651	104,837	114,033	129,187	142,833	154,161	160,149	166,315
Personal Property											
Inflationary Growth	0	0	0	248	501	759	1,022	1,290	1,564	1,843	2,128
New Development Value (Table 4-B)	0	0	0	5	10	15	20	26	31	37	43
Total Personal Property	0	0	248	248	248	248	248	248	248	248	248
	0	0	248	501	759	1,022	1,290	1,564	1,843	2,128	2,418
II. Total Incremental Value Over Base of	71,139	2,874	11,293	20,013	34,457	43,917	59,338	73,258	84,866	91,139	97,595
Assumed Tax Levy (reduced 1% per year)	0.017757	0.017579	0.017403	0.017229	0.017057	0.016886	0.016718	0.016550	0.016385	0.016221	0.016059
III. Gross Tax Increment Revenue	25	51	197	345	588	742	992	1,212	1,391	1,478	1,567
Less School District Share at -0.4%	(6)	(11)	(45)	(80)	(138)	(176)	(237)	(293)	(339)	(365)	(390)
Net Tax Increment Revenue	20	39	151	265	450	566	755	919	1,051	1,114	1,177

(1) An annual inflationary increase of 2% is assumed

Table 3 - B
Tax Increment Projection
Old Boise/ Eastside Study Area
Capital City Development Corporation
of the City of Boise
(000's Omitted)

FY Ending September 30											TIF Ends		Memo Total
	2016	2017	2018	2019	2020	2021	2022	2023	2024				
I. Real Property													
Inflationary Growth (1)	166,315	169,641	173,034	176,495	180,025	183,625	187,298	191,044	194,865				
New Development Value (Table 4-B)	3,326	3,393	3,461	3,530	3,600	3,673	3,746	3,821	3,897				
Total Real Property	0	0	0	0	0	0	0	0	0				
	169,641	173,034	176,495	180,025	183,625	187,298	191,044	194,865	198,762				
Personal Property													
Inflationary Growth	2,418	2,467	2,516	2,566	2,618	2,670	2,723	2,778	2,833				
New Development Value (Table 4-B)	48	49	50	51	52	53	54	56	57				
Total Personal Property	0	0	0	0	0	0	0	0	0				
	2,467	2,516	2,566	2,618	2,670	2,723	2,778	2,833	2,890				
II. Total Incremental Value Over Base of													
Assumed Tax Levy (reduced 1% per year)	71,139	100,970	104,412	107,923	111,504	115,157	118,883	122,683	126,560	130,514			
	0.015898	0.015739	0.015582	0.015426	0.015272	0.015119	0.014968	0.014818	0.014670				
III. Gross Tax Increment Revenue													
Less School District Share at -0.4%	1,605	1,643	1,682	1,720	1,759	1,797	1,836	1,875	1,915				
Net Tax Increment Revenue	(404)	(418)	(432)	(446)	(461)	(476)	(491)	(506)	(522)				
	1,201	1,226	1,250	1,274	1,298	1,322	1,346	1,369	1,393				

(1) An annual inflationary increase of 2% is assumed

Table 4 - A

New Development Value Added
River Street /Myrtle Street District
Capital City Development Corporation
of the City of Boise
(000's Omitted)

	Scope of Development	Total Value Added	0 2005	1 2006	2 2007	3 2008	4 2009	5 2010	6 2011	7 2012
1	Housing - Parcel 3	152 DU	7,064	7,064	0	0	0	0	0	0
2	Retail - Parcel 3		2,566	2,566	0	0	0	0	0	0
3	Retail - Parcel 3 Per Propty	17,448 sq ft	349	349	0	0	0	0	0	0
4	USFS - Parcel 1		2,526	2,526	0	0	0	0	0	0
5	Water Center Retail - Pcl 1		1,281	1,281	0	0	0	0	0	0
6	Water Center Retail - Pcl 1 Per Prop	8,709 sq ft	174	174	0	0	0	0	0	0
7	Bldg 2 Commercial - Pcl 1		3,154	3,154	0	0	0	0	0	0
8	Bldg 2 Commercial - Pcl 1 Per Prop	21,447 sq ft	214	214	0	0	0	0	0	0
9	Housing - Parcel 2	145 DU	6,742	6,742	0	0	0	0	0	0
10	Shop Space - Parcel 2		2,133	2,133	0	0	0	0	0	0
11	Shop Space - Parcel 2 Per Prop	14,502 sq ft	290	290	0	0	0	0	0	0
12	Idaho Place Retail - Ph I		1,900	1,900	0	0	0	0	0	0
13	Idaho Place Retail - Ph I Per Prop	12,920 sq ft	258	258	0	0	0	0	0	0
14	Idaho Place CommI - Ph I		3,741	3,741	0	0	0	0	0	0
15	Idaho Place CommI - Ph I Per Prop	25,439 sq ft	254	254	0	0	0	0	0	0
16	Idaho Place Retail - Ph 2		9,900	0	0	9,900	0	0	0	0
17	Idaho Place Retail - Ph 2 Per Prop	67,320 sq ft	1,346	0	0	1,346	0	0	0	0
18	Idaho Place Residential - Ph 2		2,280	0	0	2,280	0	0	0	0
19	2D Commercial & Parking		15,501	0	15,501	0	0	0	0	0
20	2D Commercial Per Prop	105,408 sq ft	1,054	0	1,054	0	0	0	0	0
21	BoDo Bldg 2 - Retail		405	0	405	0	0	0	0	0
22	BoDo Bldg 2 - Retail Per Prop	2,754 sq ft	55	0	55	0	0	0	0	0
23	BoDo Bldg 4 - Retail		3,086	0	0	3,086	0	0	0	0
24	BoDo Bldg 4 - Retail Per Prop	20,983 sq ft	420	0	0	420	0	0	0	0
25	BoDo Bldg 4 - Office		3,000	0	0	3,000	0	0	0	0
26	BoDo Bldg 4 - Office Per Prop	20,400 sq ft	204	0	0	204	0	0	0	0
27	BoDo Bldg 7 - Retail		4,099	0	0	4,099	0	0	0	0
28	BoDo Bldg 7 - Retail Per Prop	27,876 sq ft	558	0	0	558	0	0	0	0
29	BoDo Bldg 7 - Cinema		1,777	0	0	1,777	0	0	0	0
30	BoDo Bldg 7 - Cinema Per Prop	14,213 sq ft	622	0	0	622	0	0	0	0
31	BoDo Bldg 8 - Retail		1,189	0	0	1,189	0	0	0	0
32	BoDo Bldg 8 - Retail Per Prop	8,086 sq ft	162	0	0	162	0	0	0	0
33	BoDo Bldg 8 - Hotel	168 rooms	15,903	0	0	15,903	0	0	0	0
34	BoDo Bldg 8 - Hotel Per Prop	168 rooms	2,520	0	0	2,520	0	0	0	0
35	BoDo Bldg 6 - Retail		1,934	0	0	0	1,934	0	0	0
36	BoDo Bldg 6 - Retail Per Prop	13,154 sq ft	263	0	0	0	263	0	0	0
37	BoDo Bldg 8 - Office		1,505	0	0	0	1,505	0	0	0
38	BoDo Bldg 8 - Office Per Prop	10,231 sq ft	102	0	0	0	102	0	0	0
39	Retail - New Destination	250,000 sq ft	31,250	0	0	0	0	15,625	15,625	0
40	Retail - Personal Property	212,500 sq ft	4,250	0	0	0	0	2,125	2,125	0
41	Housing - Condominiums	988 DU	177,840	8,951	8,951	8,951	8,951	8,197	8,197	8,197
42	Housing - Apartments	1,482 DU	118,560	5,968	5,968	5,968	5,968	5,465	5,465	5,465
43	Office (non-governmental)	1,577,286 sq ft	197,161	4,033	4,033	4,033	4,033	10,862	10,862	10,862
44	Office Personal Property	1,340,693 sq ft	13,407	274	274	274	274	739	739	739
45	Retail - Local Serving	0 sq ft	0	0	0	0	0	0	0	0
46	Retail - Personal Property	0 sq ft	0	0	0	0	0	0	0	0
47	Hotels	732 rooms	25,620	0	0	0	0	5,250	5,250	5,250
48	Hotels Personal Property	732 rooms	7,320	0	0	0	0	1,500	1,500	1,500
Total Real Property Value Added			642,117	50,059	34,859	48,006	34,571	18,952	45,400	29,775
Real Property Adjusted for Inflation at 2%				50,059	35,556	49,946	36,687	20,515	50,125	34,202
Total Personal Property Value Added			33,823	1,815	1,383	4,759	1,986	274	4,364	2,239

Source: CCDC staff assumptions as of July 30, 2004

Table 4 - A

New Development Value Added
River Street /Myrtle Street District
Capital City Development Corporation
of the City of Boise
(000's Omitted)

	Scope of Development	Total Value Added	8 2013	9 2014	10 2015	11 2016	12 2017	13 2018	14 2019	15 2020
1	Housing - Parcel 3	152 DU	7,064	0	0	0	0	0	0	0
2	Retail - Parcel 3		2,566	0	0	0	0	0	0	0
3	Retail - Parcel 3 Per Propty	17,448 sq ft	349	0	0	0	0	0	0	0
4	USFS - Parcel 1		2,526	0	0	0	0	0	0	0
5	Water Center Retail - Pcl 1		1,281	0	0	0	0	0	0	0
6	Water Center Retail - Pcl 1 Per Prop	8,709 sq ft	174	0	0	0	0	0	0	0
7	Bldg 2 Commercial - Pcl 1		3,154	0	0	0	0	0	0	0
8	Bldg 2 Commercial - Pcl 1 Per Prop	21,447 sq ft	214	0	0	0	0	0	0	0
9	Housing - Parcel 2	145 DU	6,742	0	0	0	0	0	0	0
10	Shop Space - Parcel 2		2,133	0	0	0	0	0	0	0
11	Shop Space - Parcel 2 Per Prop	14,502 sq ft	290	0	0	0	0	0	0	0
12	Idaho Place Retail - Ph I		1,900	0	0	0	0	0	0	0
13	Idaho Place Retail - Ph I Per Prop	12,920 sq ft	258	0	0	0	0	0	0	0
14	Idaho Place CommI - Ph I		3,741	0	0	0	0	0	0	0
15	Idaho Place CommI - Ph I Per Prop	25,439 sq ft	254	0	0	0	0	0	0	0
16	Idaho Place Retail - Ph 2		9,900	0	0	0	0	0	0	0
17	Idaho Place Retail - Ph 2 Per Prop	67,320 sq ft	1,346	0	0	0	0	0	0	0
18	Idaho Place Residential - Ph 2		2,280	0	0	0	0	0	0	0
19	2D Commercial & Parking		15,501	0	0	0	0	0	0	0
20	2D Commercial Per Prop	105,408 sq ft	1,054	0	0	0	0	0	0	0
21	BoDo Bldg 2 - Retail		405	0	0	0	0	0	0	0
22	BoDo Bldg 2 - Retail Per Prop	2,754 sq ft	55	0	0	0	0	0	0	0
23	BoDo Bldg 4 - Retail		3,086	0	0	0	0	0	0	0
24	BoDo Bldg 4 - Retail Per Prop	20,983 sq ft	420	0	0	0	0	0	0	0
25	BoDo Bldg 4 - Office		3,000	0	0	0	0	0	0	0
26	BoDo Bldg 4 - Office Per Prop	20,400 sq ft	204	0	0	0	0	0	0	0
27	BoDo Bldg 7 - Retail		4,099	0	0	0	0	0	0	0
28	BoDo Bldg 7 - Retail Per Prop	27,876 sq ft	558	0	0	0	0	0	0	0
29	BoDo Bldg 7 - Cinema		1,777	0	0	0	0	0	0	0
30	BoDo Bldg 7 - Cinema Per Prop	14,213 sq ft	622	0	0	0	0	0	0	0
31	BoDo Bldg 8 - Retail		1,189	0	0	0	0	0	0	0
32	BoDo Bldg 8 - Retail Per Prop	8,086 sq ft	162	0	0	0	0	0	0	0
33	BoDo Bldg 8 - Hotel	168 rooms	15,903	0	0	0	0	0	0	0
34	BoDo Bldg 8 - Hotel Per Prop	168 rooms	2,520	0	0	0	0	0	0	0
35	BoDo Bldg 6 - Retail		1,934	0	0	0	0	0	0	0
36	BoDo Bldg 6 - Retail Per Prop	13,154 sq ft	263	0	0	0	0	0	0	0
37	BoDo Bldg 8 - Office		1,505	0	0	0	0	0	0	0
38	BoDo Bldg 8 - Office Per Prop	10,231 sq ft	102	0	0	0	0	0	0	0
39	Retail - New Destination	250,000 sq ft	31,250	0	0	0	0	0	0	0
40	Retail - Personal Property	212,500 sq ft	4,250	0	0	0	0	0	0	0
41	Housing - Condominiums	988 DU	177,840	8,197	8,197	8,756	8,756	8,756	8,756	9,664
42	Housing - Apartments	1,482 DU	118,560	5,465	5,465	5,837	5,837	5,837	5,837	6,442
43	Office (non-governmental)	1,577,286 sq ft	197,161	10,862	10,862	11,675	11,675	11,675	11,675	12,861
44	Office Personal Property	1,340,693 sq ft	13,407	739	739	794	794	794	794	875
45	Retail - Local Serving	0 sq ft	0	0	0	0	0	0	0	0
46	Retail - Personal Property	0 sq ft	0	0	0	0	0	0	0	0
47	Hotels	732 rooms	25,620	0	0	4,900	0	0	4,970	0
48	Hotels Personal Property	732 rooms	7,320	0	0	1,400	0	0	1,420	0
Total Real Property Value Added			642,117	24,525	24,525	31,169	26,269	26,269	31,239	28,967
Real Property Adjusted for Inflation at 2%				28,734	29,309	37,994	32,662	33,315	40,410	38,985
Total Personal Property Value Added			33,823	739	739	2,194	794	794	2,214	875

Source: CCDC staff assumptions as of July 30, 2004

Table 4 - A

New Development Value Added
River Street /Myrtle Street District
Capital City Development Corporation
of the City of Boise
(000's Omitted)

			Total Value Added	16 2021	17 2022	18 2023	TIF Ends 19 2024
Scope of Development							
1	Housing - Parcel 3	152 DU	7,064	0	0	0	0
2	Retail - Parcel 3		2,566	0	0	0	0
3	Retail - Parcel 3 Per Propty	17,448 sq ft	349	0	0	0	0
4	USFS - Parcel 1		2,526	0	0	0	0
5	Water Center Retail - Pcl 1		1,281	0	0	0	0
6	Water Center Retail - Pcl 1 Per Prop	8,709 sq ft	174	0	0	0	0
7	Bldg 2 Commercial - Pcl 1		3,154	0	0	0	0
8	Bldg 2 Commercial - Pcl 1 Per Prop	21,447 sq ft	214	0	0	0	0
9	Housing - Parcel 2	145 DU	6,742	0	0	0	0
10	Shop Space - Parcel 2		2,133	0	0	0	0
11	Shop Space - Parcel 2 Per Prop	14,502 sq ft	290	0	0	0	0
12	Idaho Place Retail - Ph I		1,900	0	0	0	0
13	Idaho Place Retail - Ph I Per Prop	12,920 sq ft	258	0	0	0	0
14	Idaho Place Commml - Ph I		3,741	0	0	0	0
15	Idaho Place Commml - Ph I Per Prop	25,439 sq ft	254	0	0	0	0
16	Idaho Place Retail - Ph 2		9,900	0	0	0	0
17	Idaho Place Retail - Ph 2 Per Prop	67,320 sq ft	1,346	0	0	0	0
18	Idaho Place Residential - Ph 2		2,280	0	0	0	0
19	2D Commercial & Parking		15,501	0	0	0	0
20	2D Commercial Per Prop	105,408 sq ft	1,054	0	0	0	0
21	BoDo Bldg 2 - Retail		405	0	0	0	0
22	BoDo Bldg 2 - Retail Per Prop	2,754 sq ft	55	0	0	0	0
23	BoDo Bldg 4 - Retail		3,086	0	0	0	0
24	BoDo Bldg 4 - Retail Per Prop	20,983 sq ft	420	0	0	0	0
25	BoDo Bldg 4 - Office		3,000	0	0	0	0
26	BoDo Bldg 4 - Office Per Prop	20,400 sq ft	204	0	0	0	0
27	BoDo Bldg 7 - Retail		4,099	0	0	0	0
28	BoDo Bldg 7 - Retail Per Prop	27,876 sq ft	558	0	0	0	0
29	BoDo Bldg 7 - Cinema		1,777	0	0	0	0
30	BoDo Bldg 7 - Cinema Per Prop	14,213 sq ft	622	0	0	0	0
31	BoDo Bldg 8 - Retail		1,189	0	0	0	0
32	BoDo Bldg 8 - Retail Per Prop	8,086 sq ft	162	0	0	0	0
33	BoDo Bldg 8 - Hotel	168 rooms	15,903	0	0	0	0
34	BoDo Bldg 8 - Hotel Per Prop	168 rooms	2,520	0	0	0	0
35	BoDo Bldg 6 - Retail		1,934	0	0	0	0
36	BoDo Bldg 6 - Retail Per Prop	13,154 sq ft	263	0	0	0	0
37	BoDo Bldg 8 - Office		1,505	0	0	0	0
38	BoDo Bldg 8 - Office Per Prop	10,231 sq ft	102	0	0	0	0
39	Retail - New Destination	250,000 sq ft	31,250	0	0	0	0
40	Retail - Personal Property	212,500 sq ft	4,250	0	0	0	0
41	Housing - Condominiums	988 DU	177,840	9,664	9,664	9,664	9,664
42	Housing - Apartments	1,482 DU	118,560	6,442	6,442	6,442	6,442
43	Office (non-governmental)	1,577,286 sq ft	197,161	12,861	12,861	12,861	12,861
44	Office Personal Property	1,340,693 sq ft	13,407	875	875	875	875
45	Retail - Local Serving	0 sq ft	0	0	0	0	0
46	Retail - Personal Property	0 sq ft	0	0	0	0	0
47	Hotels	732 rooms	25,620	0	0	0	0
48	Hotels Personal Property	732 rooms	7,320	0	0	0	0
Total Real Property Value Added			642,117	28,967	28,967	28,967	28,967
Real Property Adjusted for Inflation at 2%				39,765	40,560	41,372	42,199
Total Personal Property Value Added			33,823	875	875	875	875

Source: CCDC staff assumptions as of July 30, 2004

Table 4 - B

New Development Value Added
Old Boise/ Eastside Study Area
Capital City Development Corporation
of the City of Boise
(000's Omitted)

			Total Value Added	0 2005	1 2006	2 2007	3 2008	4 2009	5 2010	6 2011	7 2012
Scope of Development											
1	Housing - Condominiums	140 DU	25,200	0	0	0	0	9,000	0	9,000	7,200
2	Housing - Apartments	210 DU	16,800	0	0	4,000	4,000	0	4,000	0	0
3	Office/Retail	175,000 sq ft	21,875	0	0	2,431	2,431	2,431	2,431	2,431	2,431
4	Office/Retail Per Property	148,750 sq ft	2,231	0	0	248	248	248	248	248	248
Total Real Property Value Added			63,875	0	0	6,431	6,431	11,431	6,431	11,431	9,631
Real Property Adjusted for Inflation at 2%				0	0	6,690	6,824	12,373	7,100	12,873	11,062
Total Personal Property Value Added			2,231	0	0	248	248	248	248	248	248

Source: CCDC staff assumptions as of July 30, 2004

Table 4 - B

New Development Value Added
Old Boise/ Eastside Study Area
Capital City Development Corporation
of the City of Boise
(000's Omitted)

			Total Value Added	8 2013	9 2014	10 2015	11 2016	12 2017	13 2018	14 2019	15 2020
	Scope of Development										
1	Housing - Condominiums	140 DU	25,200	0	0	0	0	0	0	0	0
2	Housing - Apartments	210 DU	16,800	4,800	0	0	0	0	0	0	0
3	Office/Retail	175,000 sq ft	21,875	2,431	2,431	2,431	0	0	0	0	0
4	Office/Retail Per Property	148,750 sq ft	2,231	248	248	248	0	0	0	0	0
Total Real Property Value Added			63,875	7,231	2,431	2,431	0	0	0	0	0
Real Property Adjusted for Inflation at 2%				8,472	2,905	2,963	0	0	0	0	0
Total Personal Property Value Added			2,231	248	248	248	0	0	0	0	0

Source: CCDC staff assumptions as of July 30, 2004